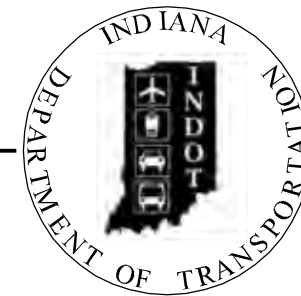


PROJECT	DESIGNATION NO.
1600873	1600873
CONTRACT	
R-39905	

### KIN PROJECT INFORMATION

DESIGNATION	PROJECT DESCRIPTION
1600873	S.R. 56 SMALL TOWN PAVEMENT REPLACEMENT
1600875	S.R. 135 ROAD RESURFACE
1700168	BRIDGE REPLACEMENT S.R. 56 OVER BROCK CREEK

# INDIANA DEPARTMENT OF TRANSPORTATION



## ROAD PLANS

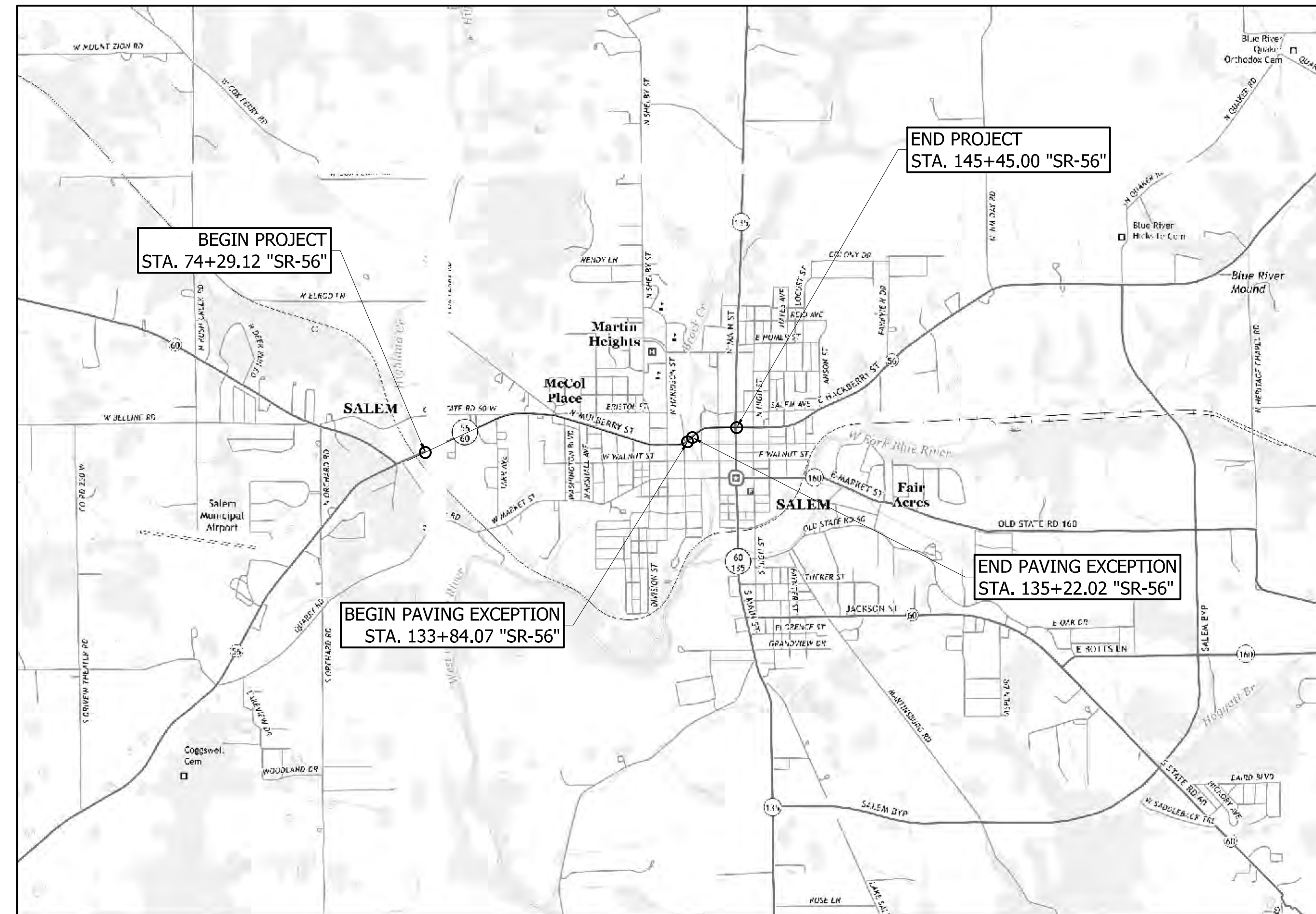
ROUTE: S.R. 56 FROM: RP 97+65 TO RP 99+12

PROJECT NO.

1600873 P.E.  
1600873 R/W  
1600873 CONST.

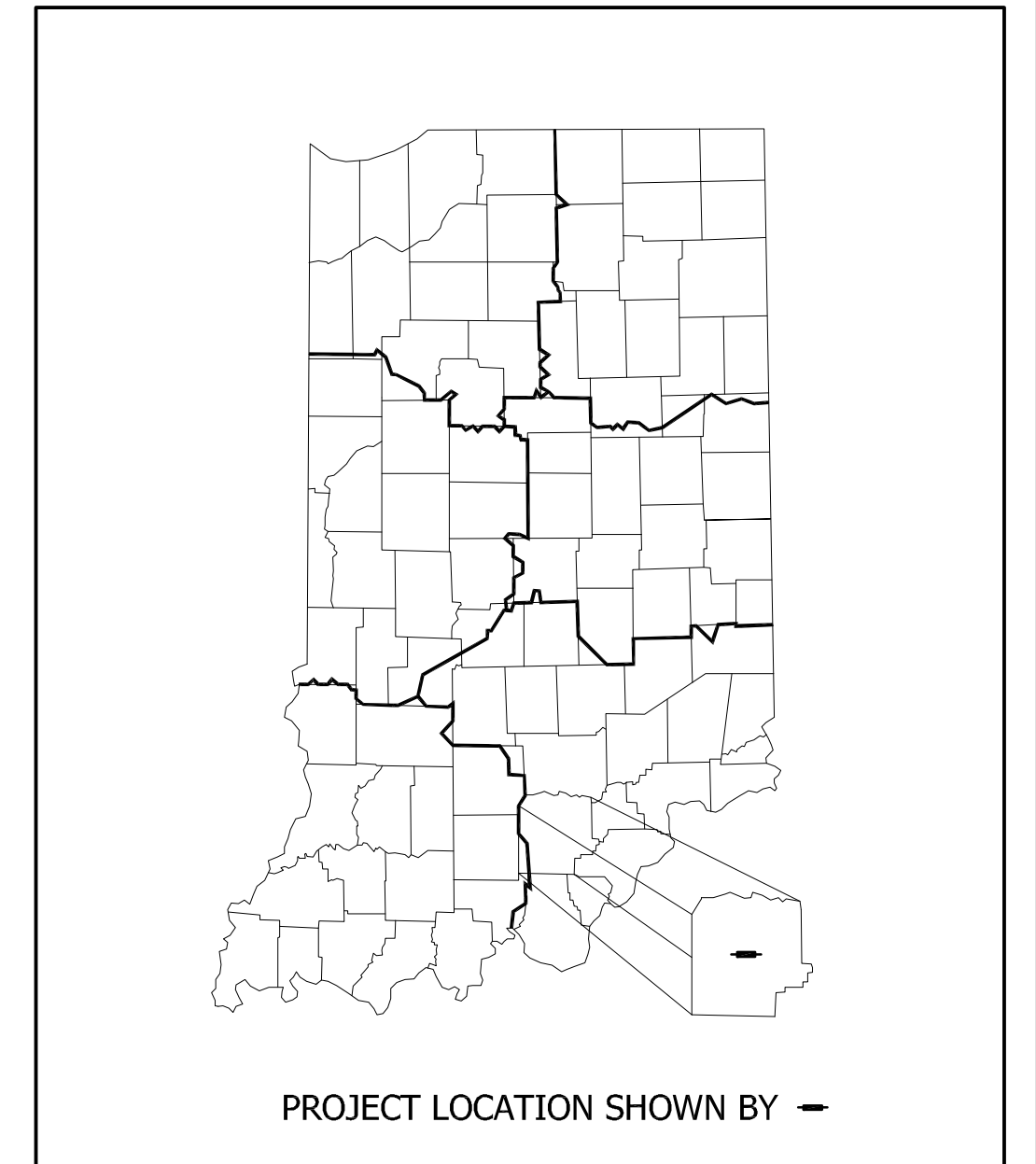
SR 56 Small Town Pavement Replacement  
From 0.09 Mile E of SR 60 to 0.08 Mile E of SR 135  
Through Salem, Indiana

Gross Length: 1.33 Mi.  
Net Length: 1.30 Mi.  
Max. Grade: 5.81%



LOCATION MAP

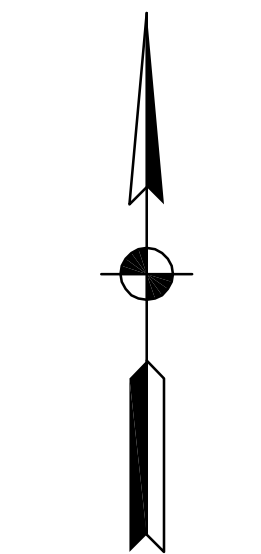
TRAFFIC DATA	S.R. 56
A.A.D.T. 2021	15,120 V.P.D.
A.A.D.T. 2041	16,880 V.P.D.
D.H.V. 2041	1,530 V.P.H.
DIRECTIONAL DISTRIBUTION	49%
TRUCKS	3% D.H.V. 5% A.A.D.T.
DESIGN DATA	S.R. 56
DESIGN SPEED	30, 35, & 45 MPH
PROJECT DESIGN CRITERIA	4R (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Urban Arterial
RURAL/URBAN	Urban (Suburban)
TERRAIN	Rolling
ACCESS CONTROL	None



LATITUDE 37° 57' 00" N LONGITUDE 86° 09' 00" W

HUC: 05140104120030, 05140104120020

CULVERT ASSETS	
DESIGNATION NO.	CULVERT ASSET I.D.
1600873	-
1600873	-
1600873	-



SCALE 1" = 1000'

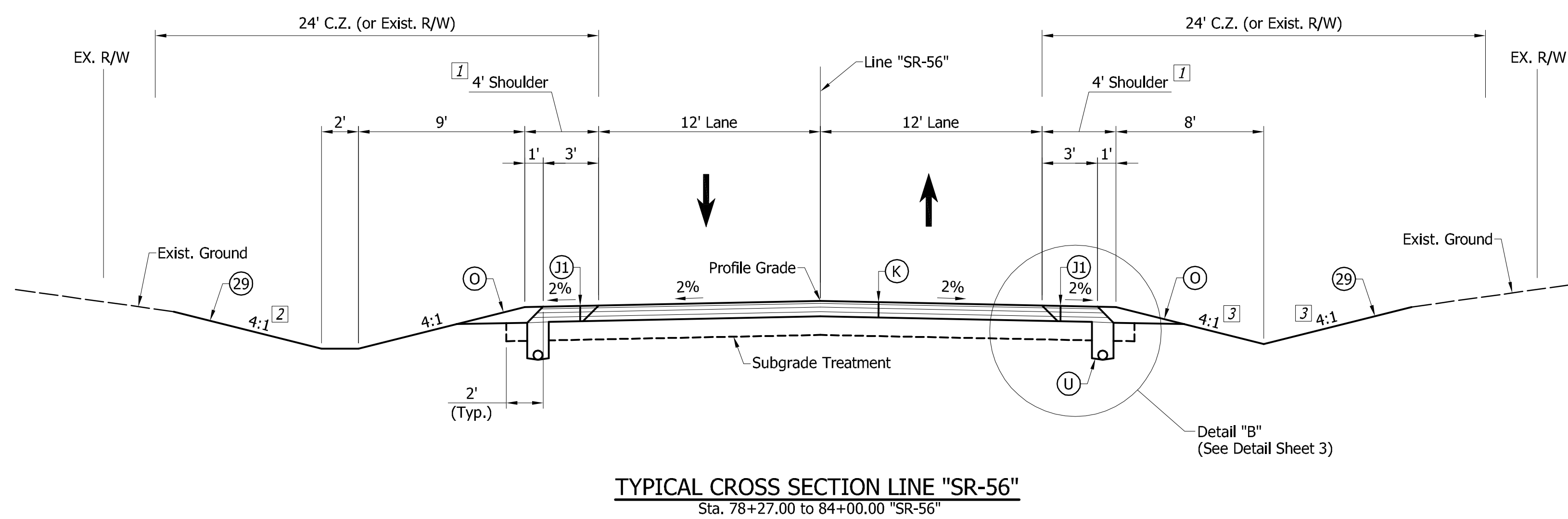
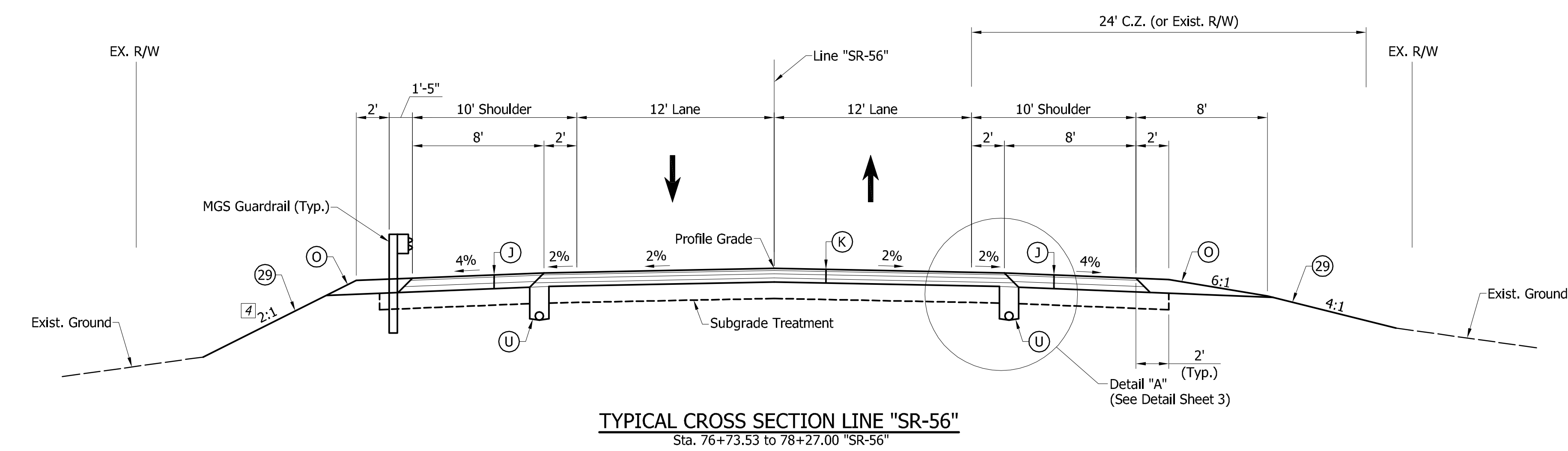
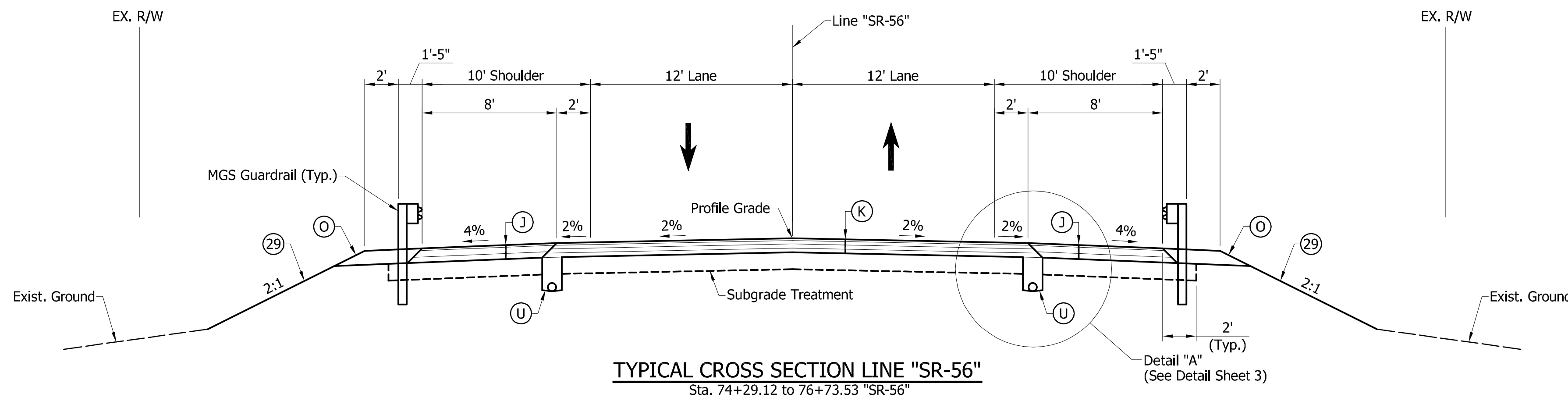
Stage 2 Plans - 05/31/19

[INDIANA DEPARTMENT OF TRANSPORTATION  
STANDARD SPECIFICATIONS DATED 2018  
TO BE USED WITH THESE PLANS]



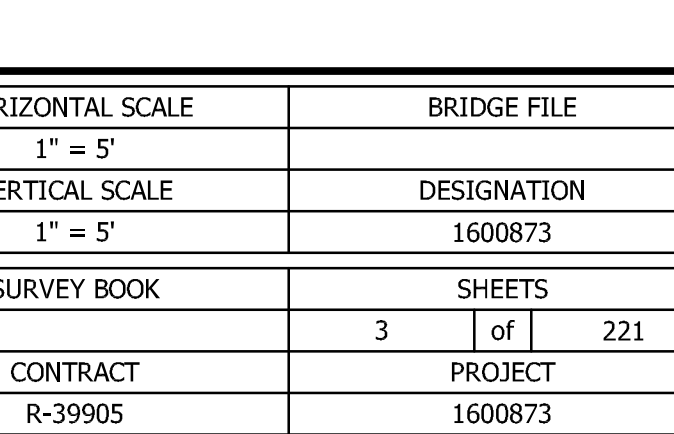
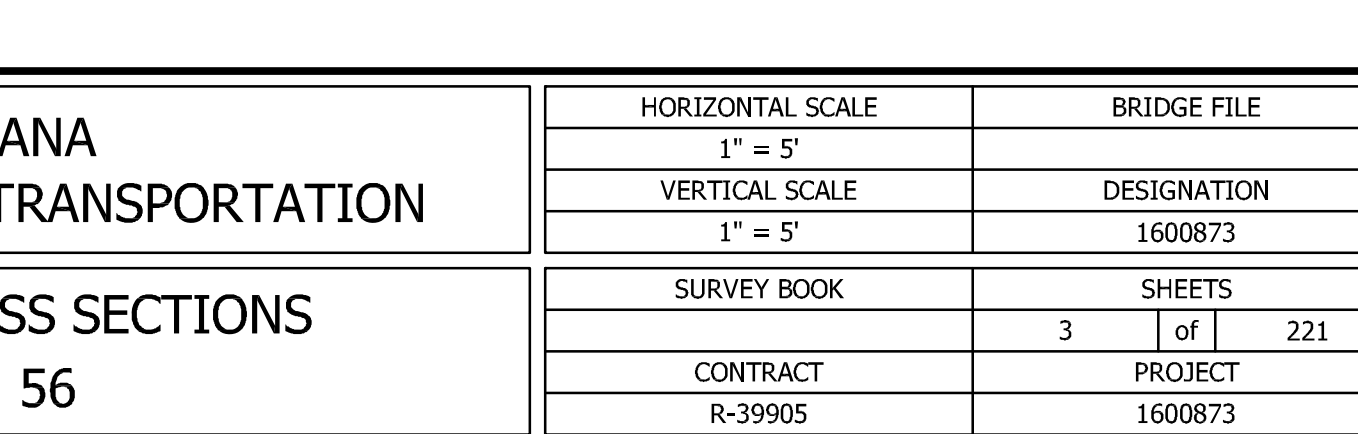
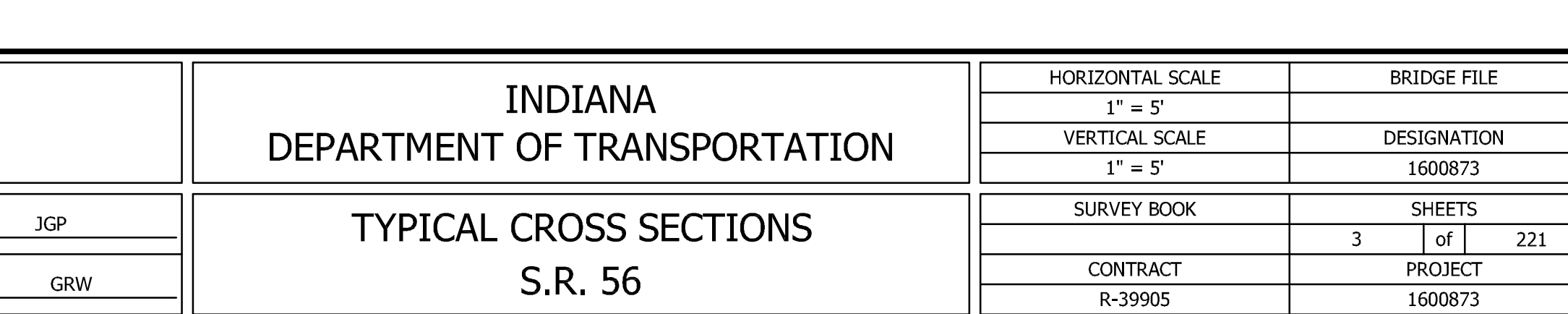
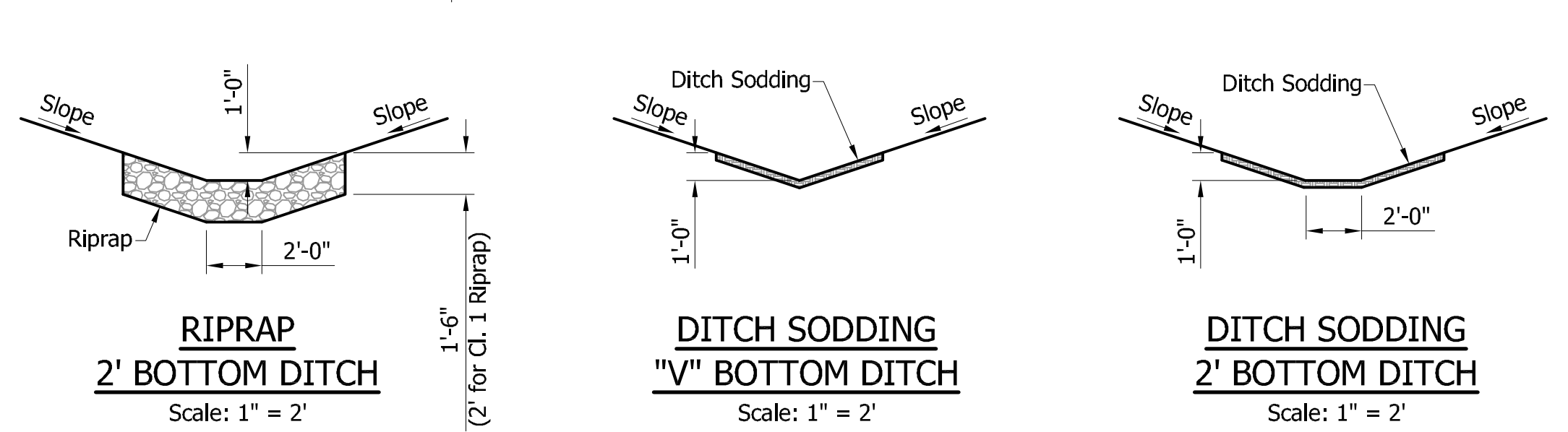
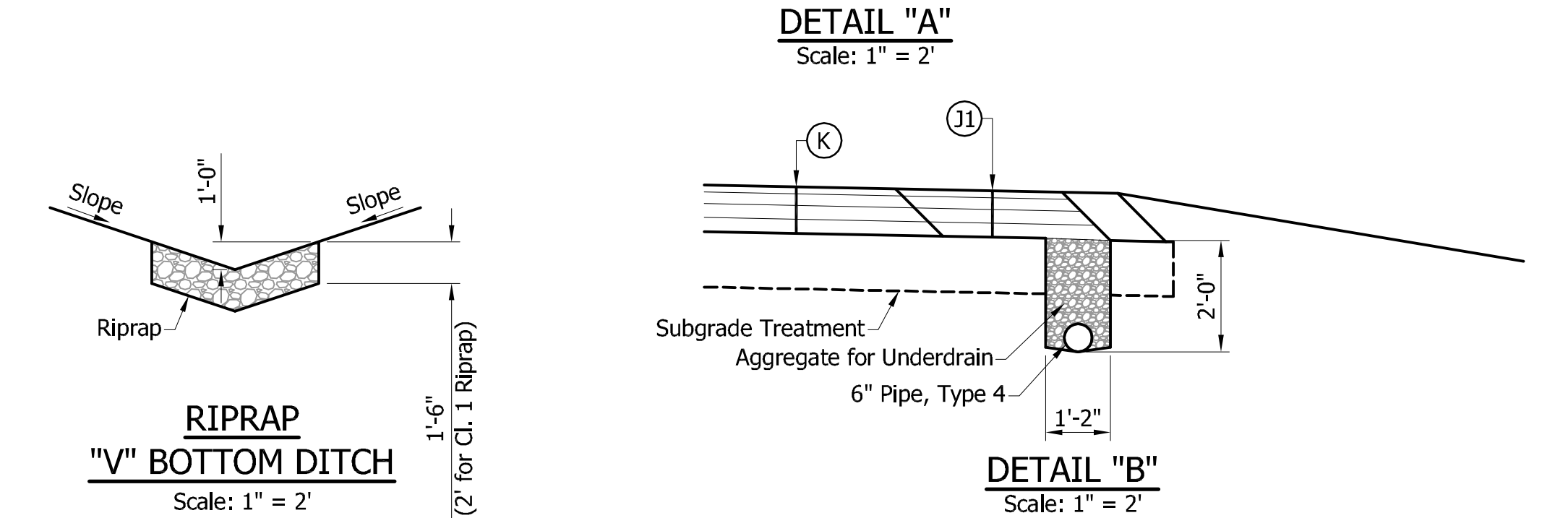
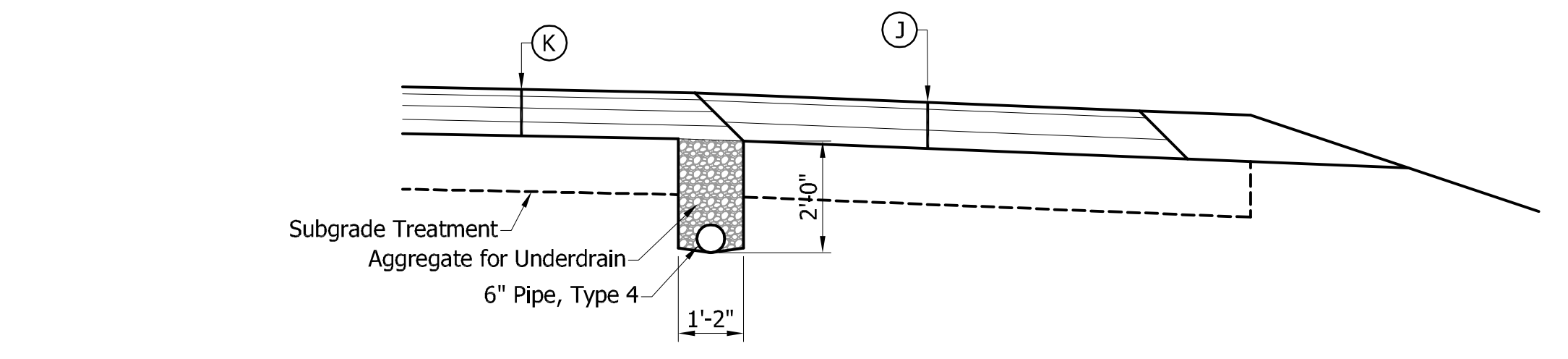
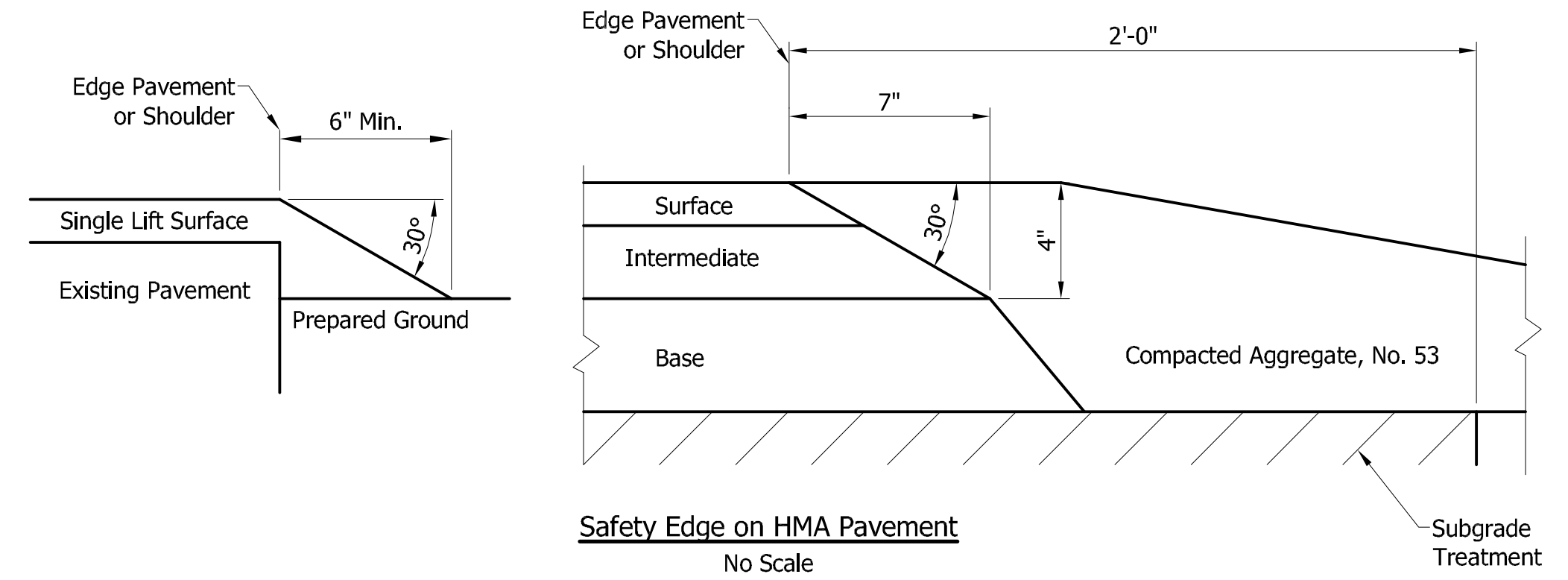
PLANS PREPARED BY:	USI Consultants, Inc.	317-544-4996
		PHONE NUMBER
CERTIFIED BY:		DATE
APPROVED FOR LETTING:		DATE
	INDIANA DEPARTMENT OF TRANSPORTATION	

DESIGNATION NO.	1600873
SHEETS	1 of 221
CONTRACT	R-39905
PROJECT NO.	1600873



- LEGEND**
- (K) HMA Full Depth Pavement  
xxx#/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Intermediate 19.0 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Base 19.0 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Base 19.0 mm
  - (J) HMA Shoulder  
xxx#/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Intermediate 19.0 mm on  
Compacted Aggregate, No. 53, Base, X"
  - (11) HMA Full Depth Shoulder  
xxx#/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Intermediate 19.0 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Base 19.0 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Base 19.0 mm
  - (O) Compacted Aggregate, No. 53
  - (U) Underdrain
  - (29) Mulched Seeding, U  
(Slopes Steeper Than 3:1 Shall Have Erosion Control Blanket)

- Notes:  
C.Z. = Clear Zone
- [1] Varies from 10' @ Sta. 78+27.00 to 4' @ 78+77.00 "SR-56"
  - [2] 2:1 Slope Sta. 80+25 to 83+67 "SR-56"
  - [3] 3:1 Slope Sta. 83+67 to 86+93.75 "SR-56"
  - [3] 3:1 Slope Sta. 79+85 to 80+75 "SR-56"
  - [3] 3:1 Slope Sta. 77+78 to 78+00 "SR-56"
  - [4] 5:1 Slope Sta. 83+18 to 12:1 Slope Sta. 83+23 "SR-56"



**NOT FOR CONSTRUCTION**

DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

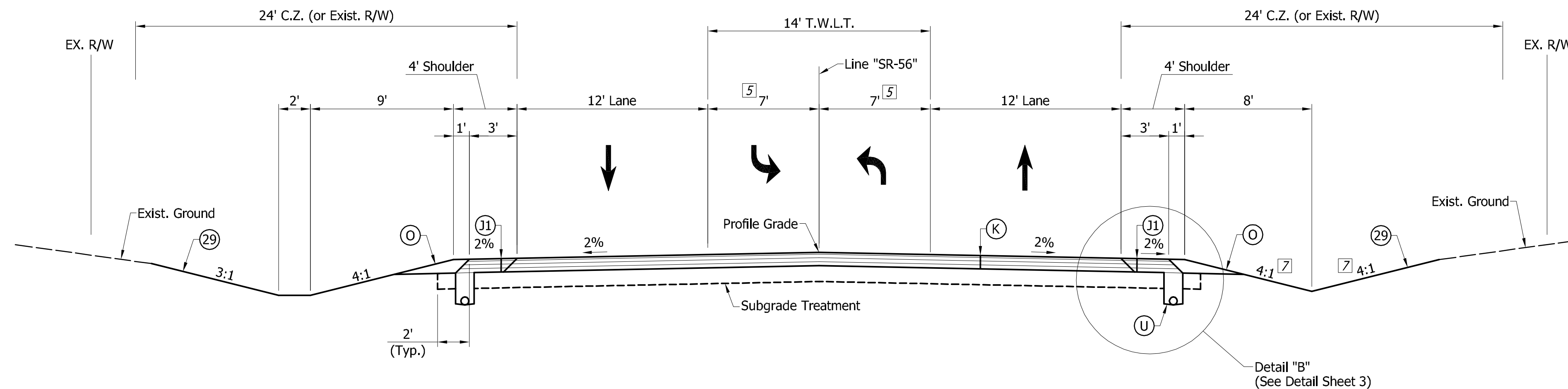
**INDIANA DEPARTMENT OF TRANSPORTATION**

**TYPICAL CROSS SECTIONS**

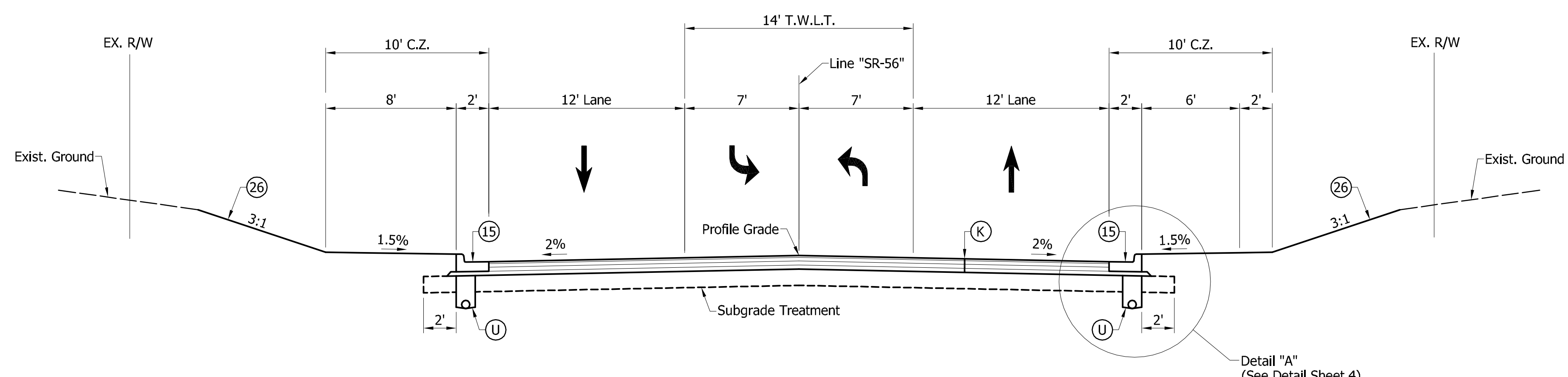
**S.R. 56**

HORIZONTAL SCALE	BRIDGE FILE
1" = 5'	
VERTICAL SCALE	DESIGNATION
1" = 5'	1600873
SURVEY BOOK	SHEETS
	3 of 221
CONTRACT	PROJECT
R-39905	1600873

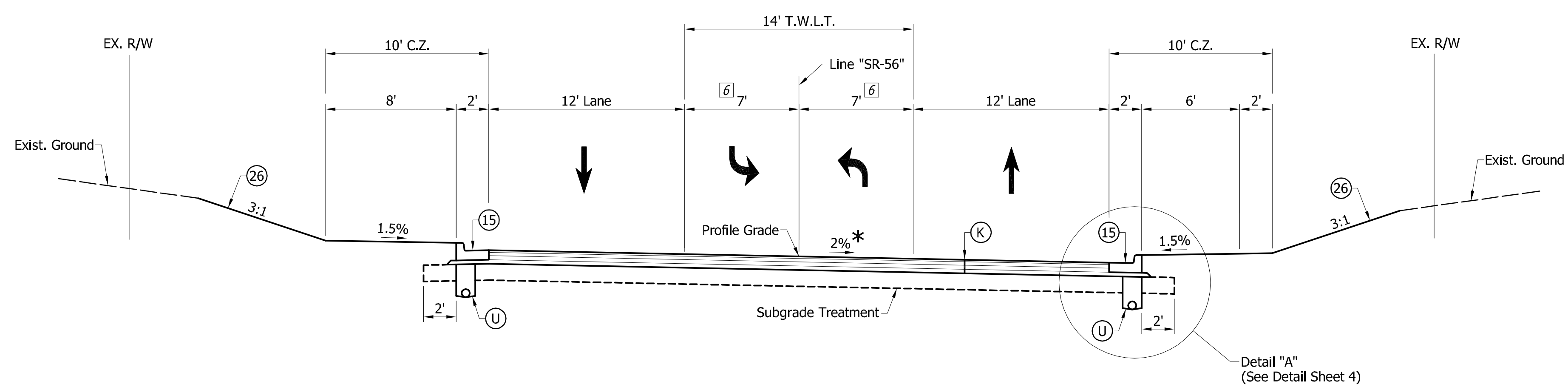
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**TYPICAL CROSS SECTION LINE "SR-56"**  
Sta. 84+00.00 to 86+93.75 "SR-56"



**TYPICAL CROSS SECTION LINE "SR-56"**  
Sta. 86+93.75 to 90+00.00 "SR-56"  
Sta. 98+35.00 to 100+65.00 "SR-56"

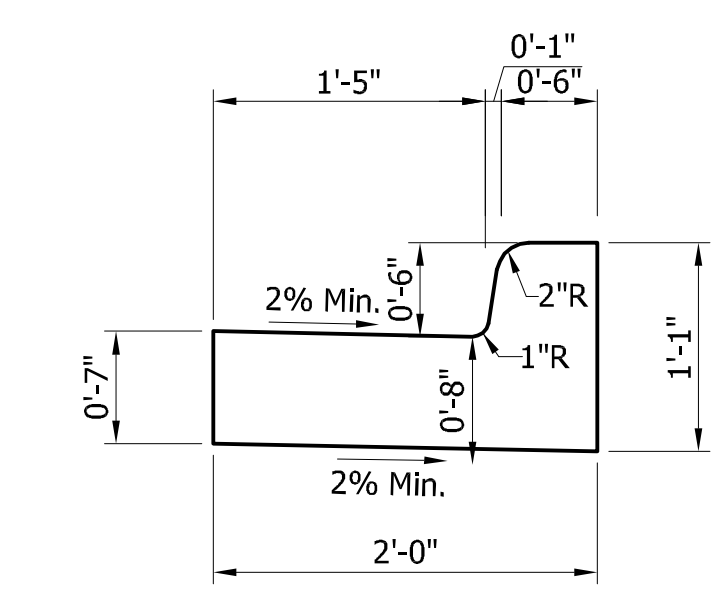


**TYPICAL CROSS SECTION LINE "SR-56"**  
Sta. 90+00.00 to 98+35.00 "SR-56"  
Sta. 100+65.00 to 104+50.00 "SR-56"

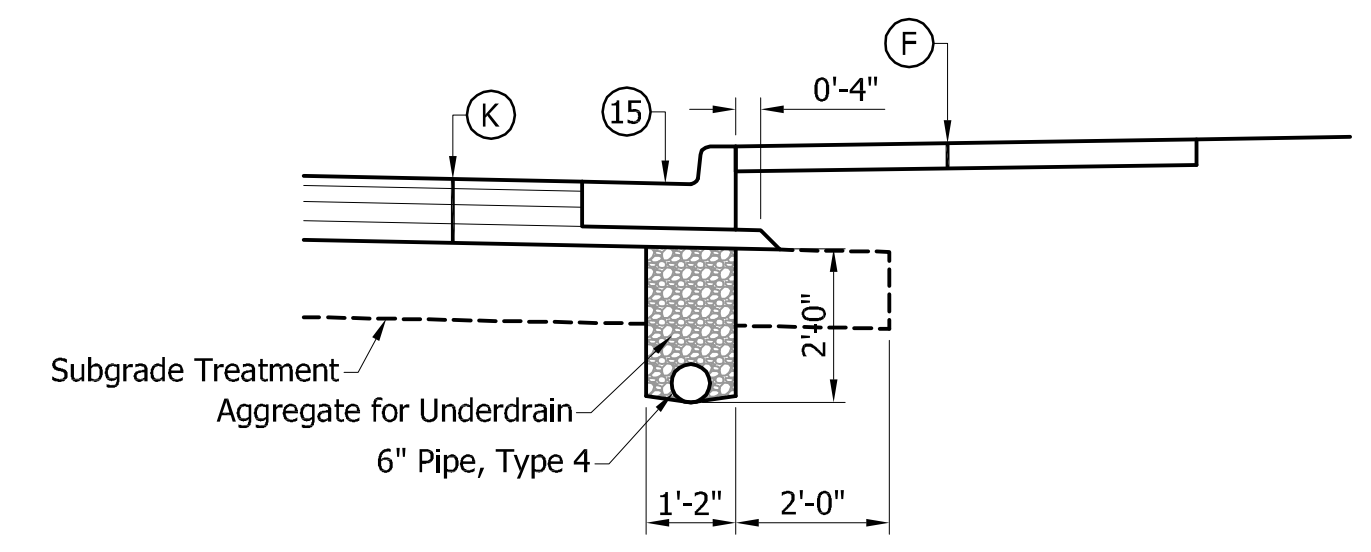
**LEGEND**

- (K) HMA Full Depth Pavement  
xxx#/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Intermediate 19.0 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Base 19.0 mm
- (J) HMA Shoulder  
xxx#/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Intermediate 19.0 mm on  
Compacted Aggregate, No. 53, Base, "X"
- (J1) HMA Full Depth Shoulder  
xxx#/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Intermediate 19.0 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Base 19.0 mm
- (F) Sidewalk, Concrete
- (O) Compacted Aggregate, No. 53
- (U) Underdrain
- (15) Modified Combined Concrete Curb and Gutter (1.5' Gutter)
- (26) Sodding, Nursery
- (29) Mulched Seeding, U  
(Slopes Steeper Than 3:1 Shall Have Erosion Control Blanket)

Notes:  
C.Z. = Clear Zone  
\* See Superelevation Diagram  
[5] Varies from 0' @ Sta. 84+00.00 to 7' @ 85+40.00 "SR-56"  
[6] Varies from 7' @ Sta. 105+14.00 to 0' @ 104+50.00 "SR-56"  
[7] 3:1 Slope Sta. 85+50 to 86+93.75 "SR-56"



**Modified Combined Concrete Curb and Gutter**  
(1.5' Gutter)  
Not to scale



**DETAIL "C"**  
Scale: 1" = 2'

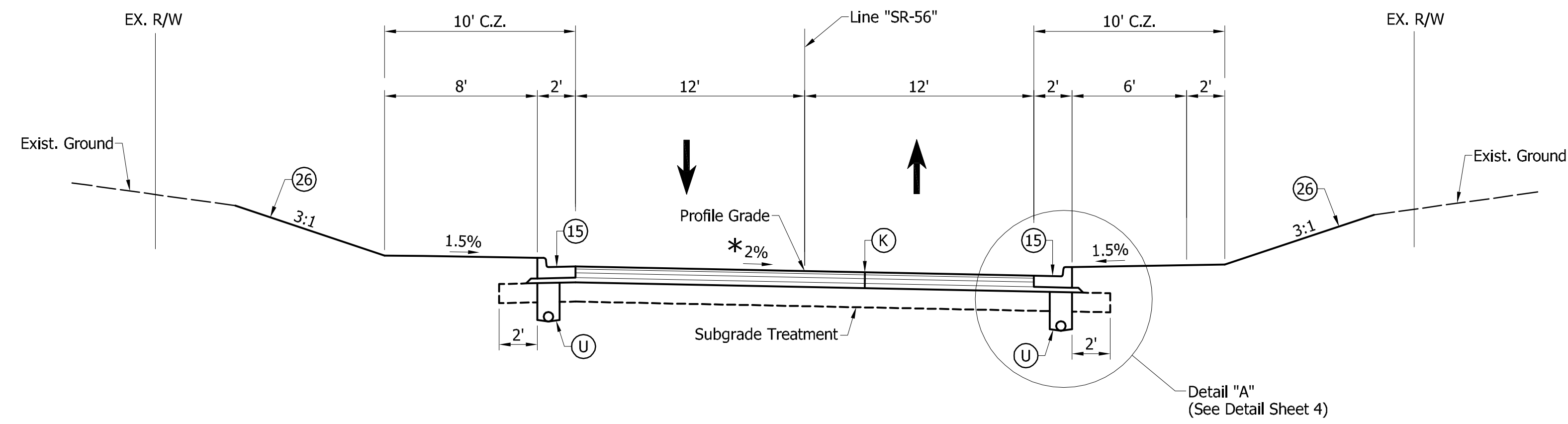
**NOT FOR CONSTRUCTION**

DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

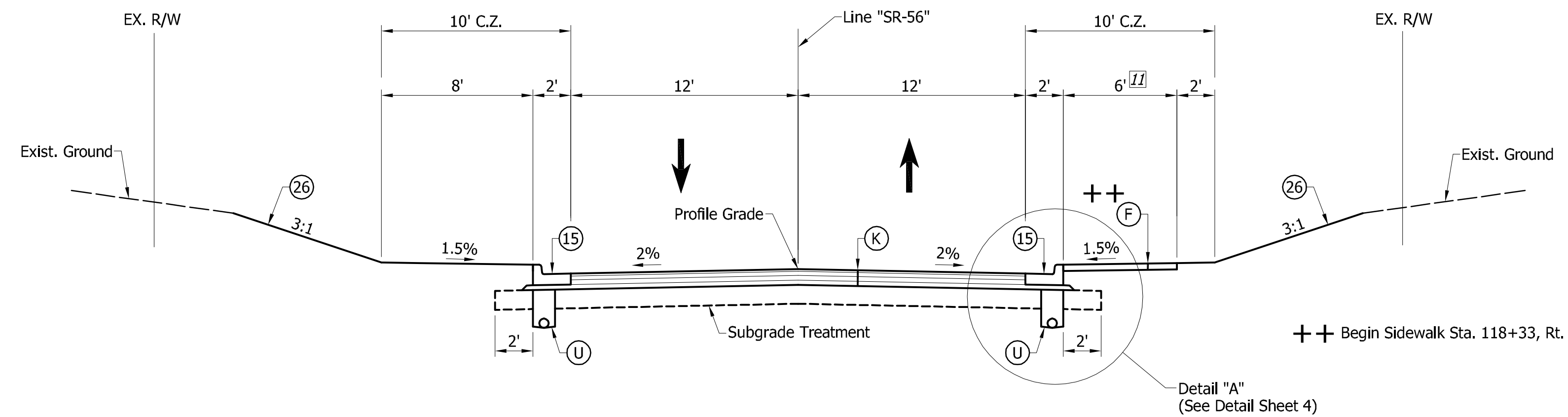
**INDIANA DEPARTMENT OF TRANSPORTATION**

**TYPICAL CROSS SECTIONS  
S.R. 56**

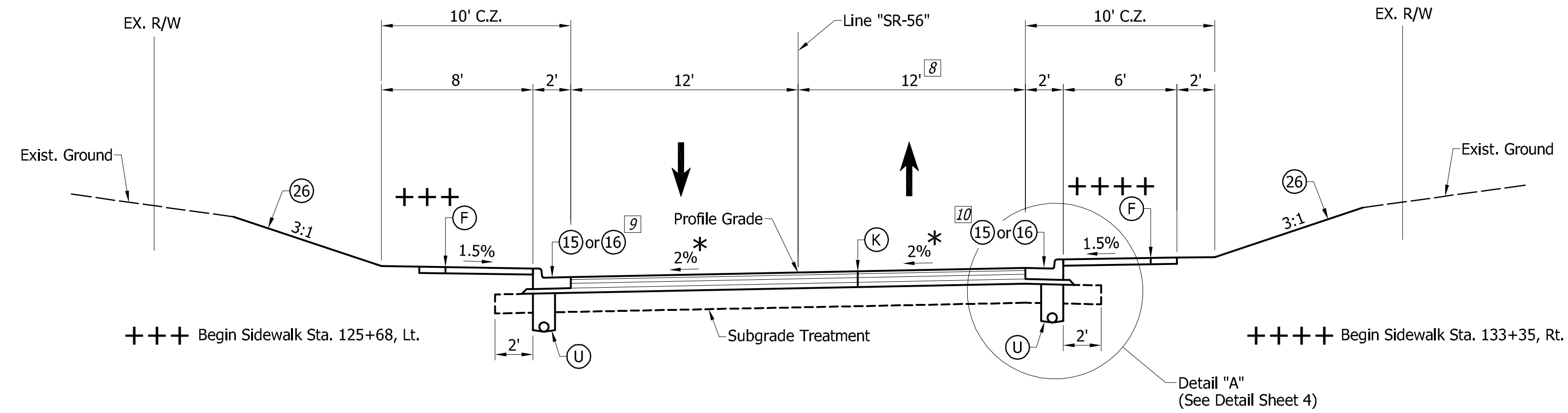
HORIZONTAL SCALE 1" = 5'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS
R-39905	4 of 221
CONTRACT	PROJECT
R-39905	1600873



**TYPICAL CROSS SECTION LINE "SR-56"**  
Sta. 104+50.00 to 106+26.00 "SR-56"



**TYPICAL CROSS SECTION LINE "SR-56"**  
Sta. 106+26.00 to 123+62.00 "SR-56"  
Sta. 127+84.00 to 130+63.00 "SR-56"



**TYPICAL CROSS SECTION LINE "SR-56"**  
Sta. 123+62.00 to 127+84.00 "SR-56"  
Sta. 130+63.00 to 133+84.07 "SR-56"  
Sta. 135+22.02 to 135+63.00 "SR-56"

**LEGEND**

- (K) HMA Full Depth Pavement  
xxx#/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Intermediate 19.0 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Base 19.0 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Base 19.0 mm
- (F) Sidewalk, Concrete
- (U) Underdrain
- (15) Modified Combined Concrete Curb and Gutter (1.5' Gutter)
- (16) Combined Concrete Curb and Gutter, Type "C"
- (26) Sodding, Nursery

Notes:  
C.Z. = Clear Zone  
\* See Superelevation Diagram

- [8] Varies from 12' @ Sta. 132+49.51 to 14' @ Sta. 133+33.37 "SR-56"  
Varies from 14.31' @ Sta. 135+22.02 to 12' @ Sta. 135+71.50 "SR-56"
- [9] Combined Concrete Curb and Gutter, Type "C"  
From Sta. 133+63.96 to 135+46.00 "SR-56" Lt.
- [10] Combined Concrete Curb and Gutter, Type "C"  
From Sta. 133+33.37 "SR-56" Rt. to 133+84.07 "SR-56" Rt.  
From Sta. 135+22.02 "SR-56" Rt. to 136+89.81 "PR-1" Rt.
- [11] 4' From Sta. 110+35 to 110+66 "SR-56" Rt.

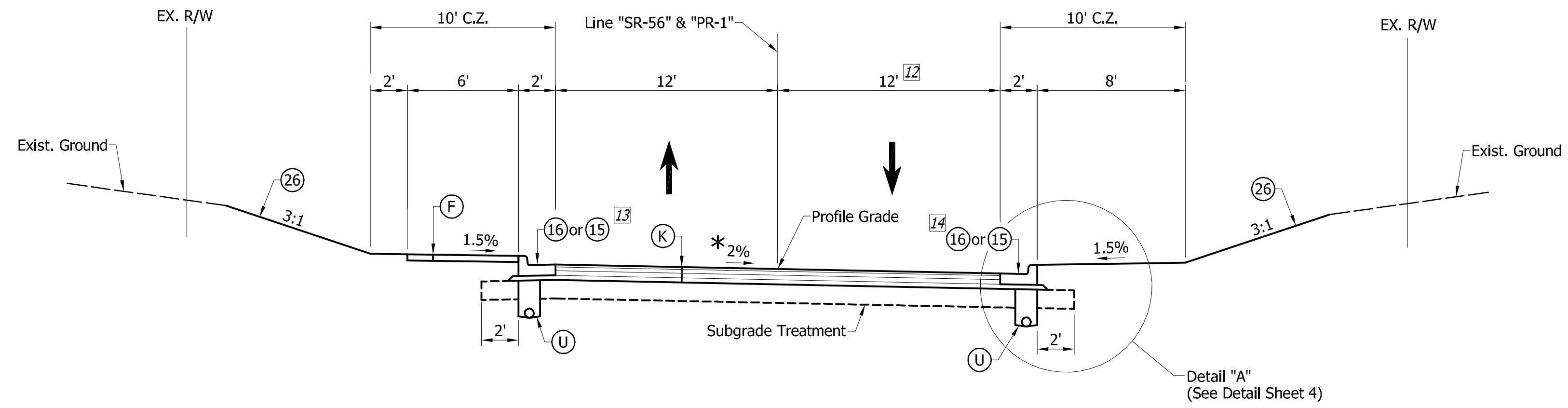
**NOT FOR CONSTRUCTION**

DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

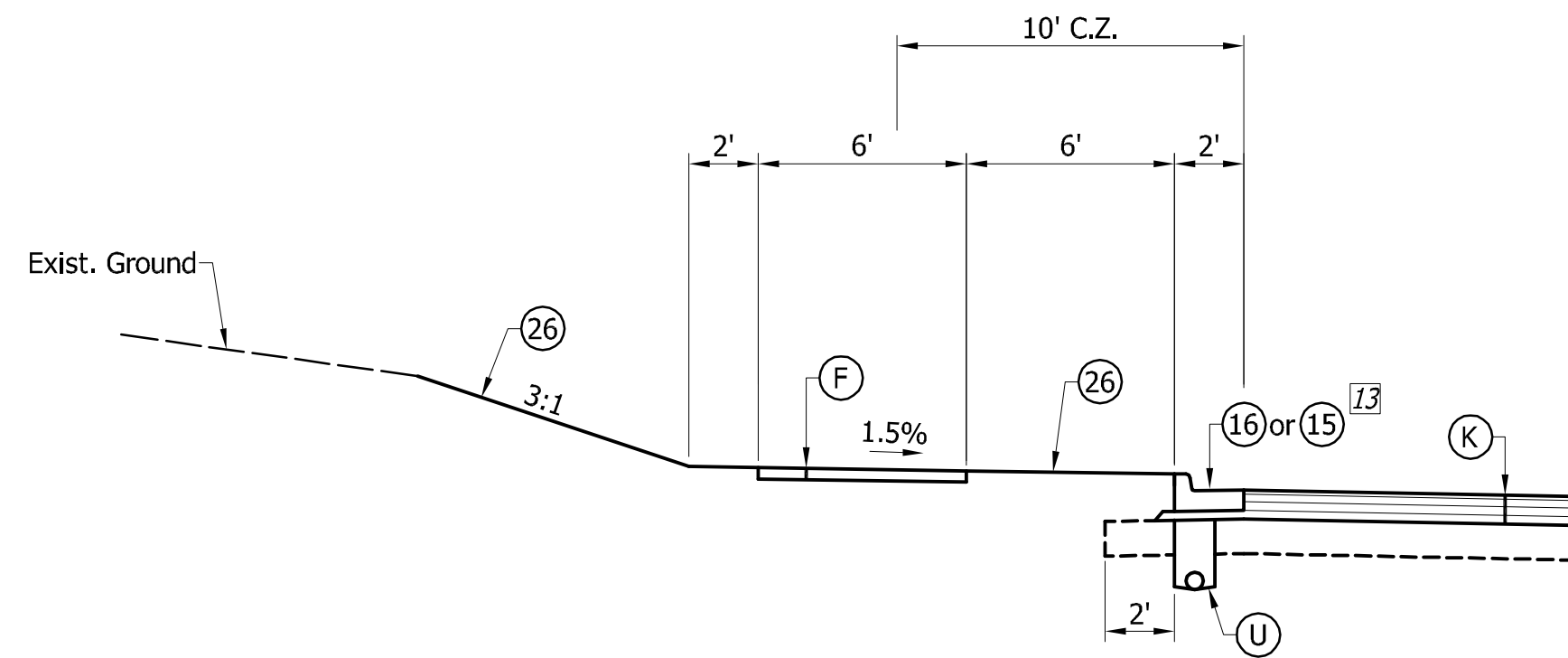
**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL CROSS SECTIONS  
S.R. 56**

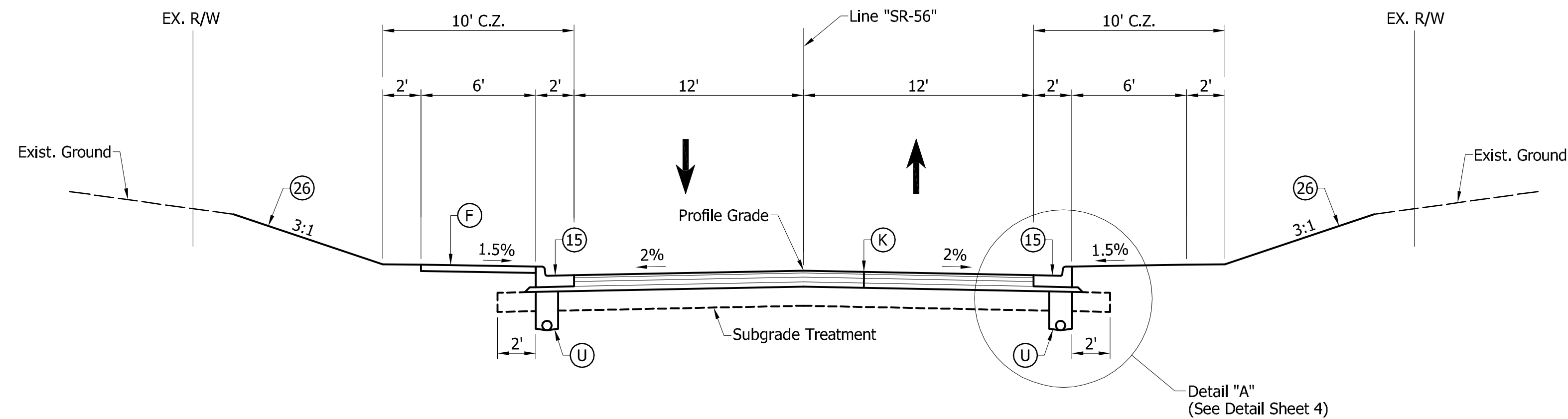
HORIZONTAL SCALE 1" = 5'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 5 of 221
CONTRACT R-39905	PROJECT 1600873



**TYPICAL CROSS SECTION LINE "SR-56" & "PR-1"**  
 Sta. 135+63.00 "SR-56" to 136+48.18 "PR-1"  
 Sta. 135+48.18 "PR-1" to 140+81.00 "SR-56"



**SIDEWALK SECTION LINE "SR-56"**  
 Sta. 138+12.24 to 141+22.47 "SR-56"



**TYPICAL CROSS SECTION LINE "SR-56"**  
 Sta. 140+81.00 to 141+05.64 "SR-56"

**LEGEND**

- (K) HMA Full Depth Pavement  
 xxx#/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on  
 xxx#/Syd. QC/QA-HMA, 2, 64, Intermediate 19.0 mm on  
 xxx#/Syd. QC/QA-HMA, 2, 64, Base 19.0 mm on  
 xxx#/Syd. QC/QA-HMA, 2, 64, Base 19.0 mm
- (F) Sidewalk, Concrete
- (U) Underdrain
- (15) Modified Combined Concrete Curb and Gutter (1.5' Gutter)
- (16) Combined Concrete Curb and Gutter, Type "C"
- (26) Sodding, Nursery

Notes:  
 C.Z. = Clear Zone  
 \* See Superelevation Diagram

I2 Varies from 14.31' @ Sta. 135+22.02 to 12' @ Sta. 135+71.50 "SR-56"

I3 Combined Concrete Curb and Gutter, Type "C"  
 From Sta. 133+63.96 to 135+46.00 "SR-56" Lt.

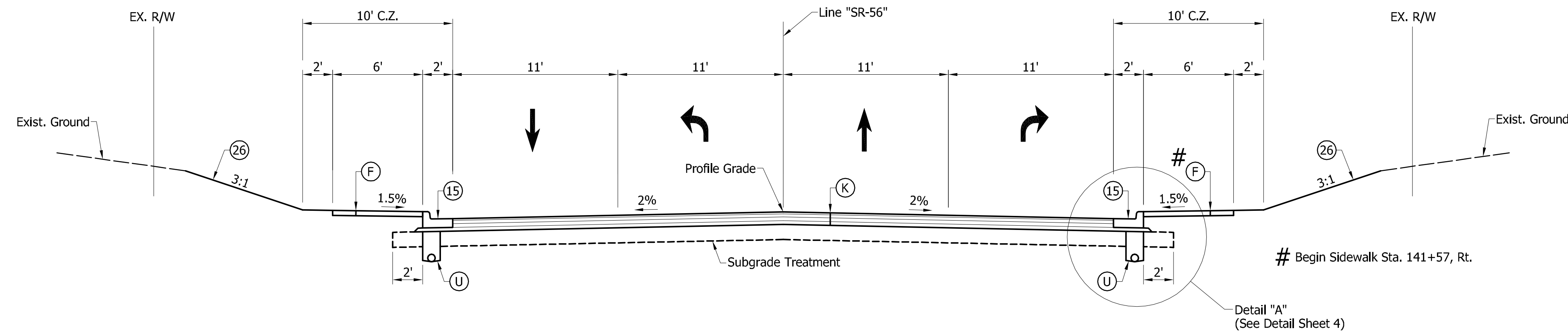
I4 Combined Concrete Curb and Gutter, Type "C"  
 From Sta. 133+33.37 "SR-56" Rt. to 136+89.81 "PR-1" Rt.

S:\2017Proj\2017-401\_SR\_56\_Sullivan\_1608873\Production Drawings\03-Typical Cross Sections\SR-56.dwg, T:\PC\4, 5/24/2015 10:27:58 AM

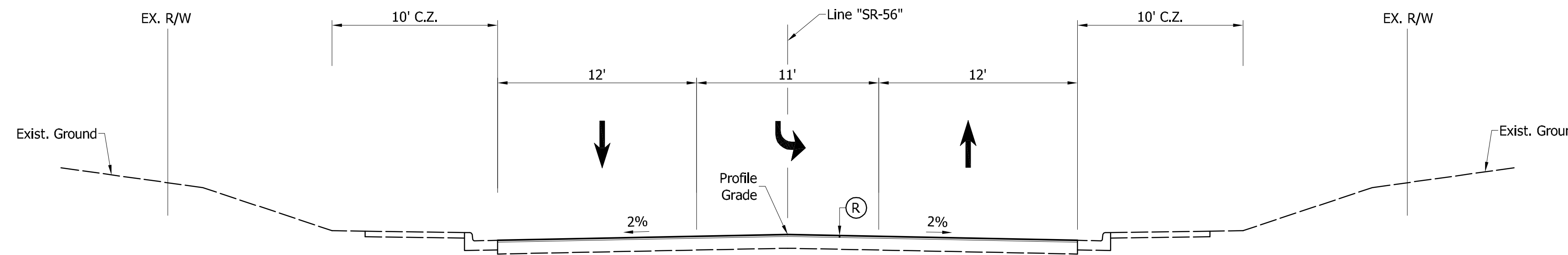
<b>NOT FOR CONSTRUCTION</b>		DESIGNED: JGP	DRAWN: JGP
		CHECKED: GRW	CHECKED: GRW

INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL CROSS SECTIONS S.R. 56	

HORIZONTAL SCALE 1" = 5'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 6 of 221
CONTRACT R-39905	PROJECT 1600873



**TYPICAL CROSS SECTION LINE "SR-56"**  
Sta. 141+05.64 to 144+39.66 "SR-56"



**TYPICAL CROSS SECTION LINE "SR-56"**  
Sta. 144+39.66 to 148+54.00 "SR-56"

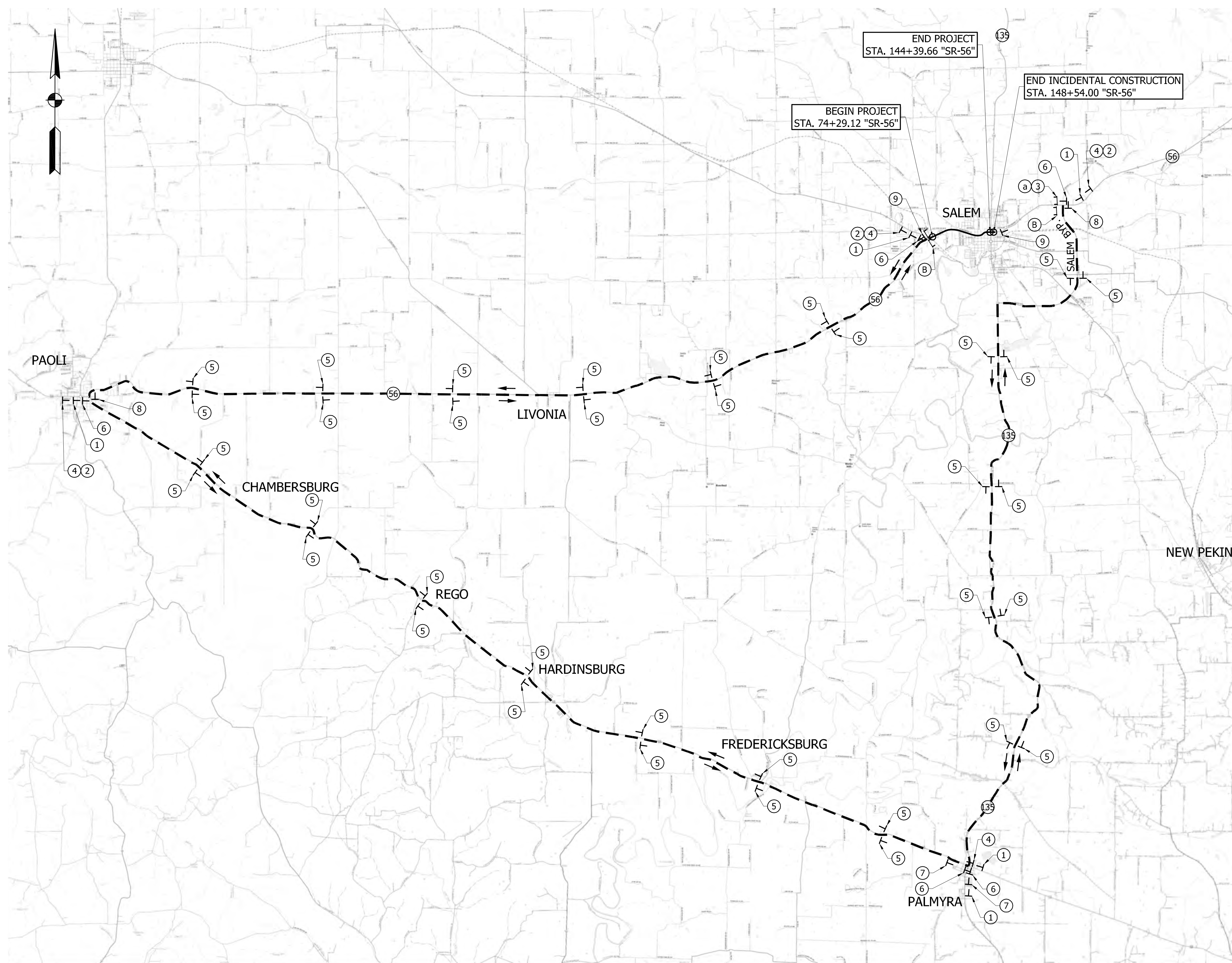
**LEGEND**

- (K) HMA Full Depth Pavement  
xxx#/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Intermediate 19.0 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Base 19.0 mm
- (R) HMA RESURFACE / SURFACE MILLING  
xxx#/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on  
1-1/2" Surface Milling
- (F) Sidewalk, Concrete
- (U) Underdrain
- (15) Modified Combined Concrete Curb and Gutter (1.5' Gutter)
- (26) Sodding, Nursery

Notes:  
C.Z. = Clear Zone

S:\2017proj\2017-401\_SR\_56\_Sullivan\_1608873\Production Drawings\03-Typical Cross Sections\03-401-Typical Cross Sections-S6.dwg, T:\PC\5, 5/24/2019 10:27:59 AM

<b>NOT FOR CONSTRUCTION</b>		<b>INDIANA DEPARTMENT OF TRANSPORTATION</b>	HORIZONTAL SCALE 1" = 5'	BRIDGE FILE	
			VERTICAL SCALE 1" = 5'	DESIGNATION 1600873	
			<b>TYPICAL CROSS SECTIONS S.R. 56</b>	SURVEY BOOK	SHEETS 7 of 221
				CONTRACT R-39905	PROJECT 1600873
	DESIGNED: JGP	DRAWN: JGP			
	CHECKED: GRW	CHECKED: GRW			



LEGEND	DESCRIPTION	SIZE	TYPE	NO.	
①	DETOUR AHEAD	XW20-2	48" X 48"	A	5
②	ROAD CLOSED AHEAD	XW20-3	48" X 48"	A	3
③	ROAD CLOSED TO THRU TRAFFIC	R11-4	60" X 30"	Road Closure Assembly	1
a	DETOUR	XM4-10 (L or R)	48" X 18"	B	1
④	ROAD CLOSED ON OR AFTER X / X / XX	XG20-6	60" X 30"	A	4
⑤	DETOUR SR-56 EAST WEST ↑	XM4-8 M1-5 (56) M3-2 or M3-4 M6-3	30" X 15" 24" X 24" 30" X 15" 21" X 15"	Detour Route Assembly	36
⑥	DETOUR SR-56 EAST WEST →	XM4-8 M1-5 (56) M3-2 or M3-4 M6-1 (L or R)	30" X 15" 24" X 24" 30" X 15" 21" X 15"	Detour Route Assembly	5
⑦	DETOUR SR-56 EAST WEST ↘	XM4-8 M1-5 (56) M3-1 or M3-3 M5-1 (L or R)	30" X 15" 24" X 24" 30" X 15" 21" X 15"	Detour Route Assembly	2
⑧	END DETOUR SR-56 ↘	XM4-6 XM4-8 M1-5 (56) M5-1 (L or R) or M6-3	30" X 15" 30" X 15" 24" X 24" 21" X 15"	Detour Route Assembly	2
⑨	END CONSTRUCTION	XG20-2	60" X 24"	A	2
ⓑ	TYPE III-B BARRICADE		12'		60 ft.

LEGEND	
Detour Route	Indicates Traffic Flow
Construction Sign	Type III Barricade










NOT FOR CONSTRUCTION

DESIGNED: BDC	DRAWN: BDC
CHECKED: GRW	CHECKED: GRW

INDIANA  
DEPARTMENT OF TRANSPORTATION

DETOUR ROUTE  
S.R. 56

HORIZONTAL SCALE 1" = 6000'	BRIDGE FILE
VERTICAL SCALE 1" = 6000'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 11 of 221
CONTRACT R-39905	PROJECT 1600873

PHASE PLAN QUANTITY				
DESCRIPTION		SIZE	TYPE	NO.
	XW20-2	48" X 48"	Road Closure Assembly	.
	XW20-1a	48" X 48"	A	.
	XW20-3	48" X 48"	A	.
	R11-4	60" X 30"	Road Closure Assembly	.
	XG20-2	60" X 24"	A	.
	R9-9	24" X 12"	B	.
	M4-9b	30" X 24"	B	.
TYPE III-A BARRICADE		12'		1ft.
TYPE III-B BARRICADE		12'		1ft.

S:\2017\1600873\Production Drawings\06-Traffic Maintenance Details\2017-04-Road Closure-41-56-56.dwg Traffic Maintenance Quantities 5/24/2019 10:56:46 AM

	<b>NOT FOR CONSTRUCTION</b>		<b>INDIANA DEPARTMENT OF TRANSPORTATION</b>	HORIZONTAL SCALE NO SCALE	BRIDGE FILE
		DESIGNED: _____ BDC	DRAWN: _____ BDC	VERTICAL SCALE NO SCALE	DESIGNATION 1600873
		CHECKED: _____ GRW	CHECKED: _____ GRW	SURVEY BOOK	SHEETS 12 of 221
				CONTRACT R-39905	PROJECT 1600873
<b>TRAFFIC MAINTENANCE QUANTITIES</b>					



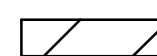




**PHASE 1A CONSTRUCTION SEQUENCE**

1. Install phase 1A signs as shown on plans.
2. Construct proposed storm sewer, main line pavement, street approaches and drives from Sta. 74+29.12 to Sta. 96+30.00 "SR 56".

Notes: 1. Access to all drives must be maintained.

**LEGEND**

-  Phase Construction
-  Construction Sign
-  Type III-B Barricade

**NOT FOR CONSTRUCTION**

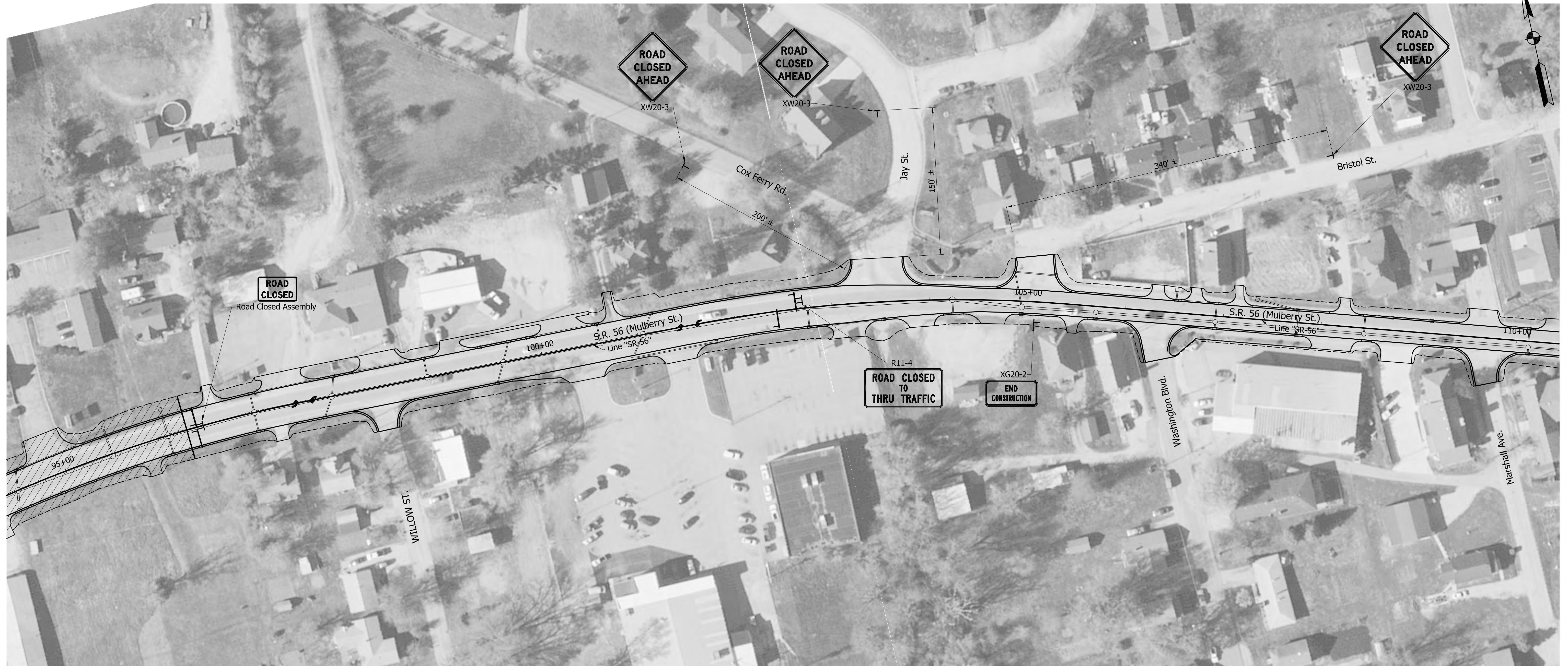
DESIGNED: BDC	DRAWN: BDC
CHECKED: GRW	CHECKED: GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC MAINTENANCE  
PHASE 1A**

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE
VERTICAL SCALE 1" = 50'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 13 of 221
CONTRACT R-39905	PROJECT 1600873

S:\2017\07\2017-401\_S.R. 56\_Sham 1600873\Production Drawings\Traffic Maintenance Details\2017-401-Detour-Seq 1.dwg, SR 56 SEG 1-01, 5/24/2019 10:28:30 AM

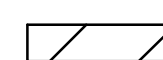




**PHASE 1A CONSTRUCTION SEQUENCE**

1. Install phase 1A signs as shown on plans.
2. Construct proposed storm sewer, main line pavement, street approaches and drives from Sta. 74+29.12 to Sta. 96+30.00 "SR 56".

Notes: 1. Access to all drives must be maintained.

**LEGEND**

-  Phase Construction
-  Construction Sign
-  Type III-B Barricade

**NOT FOR CONSTRUCTION**

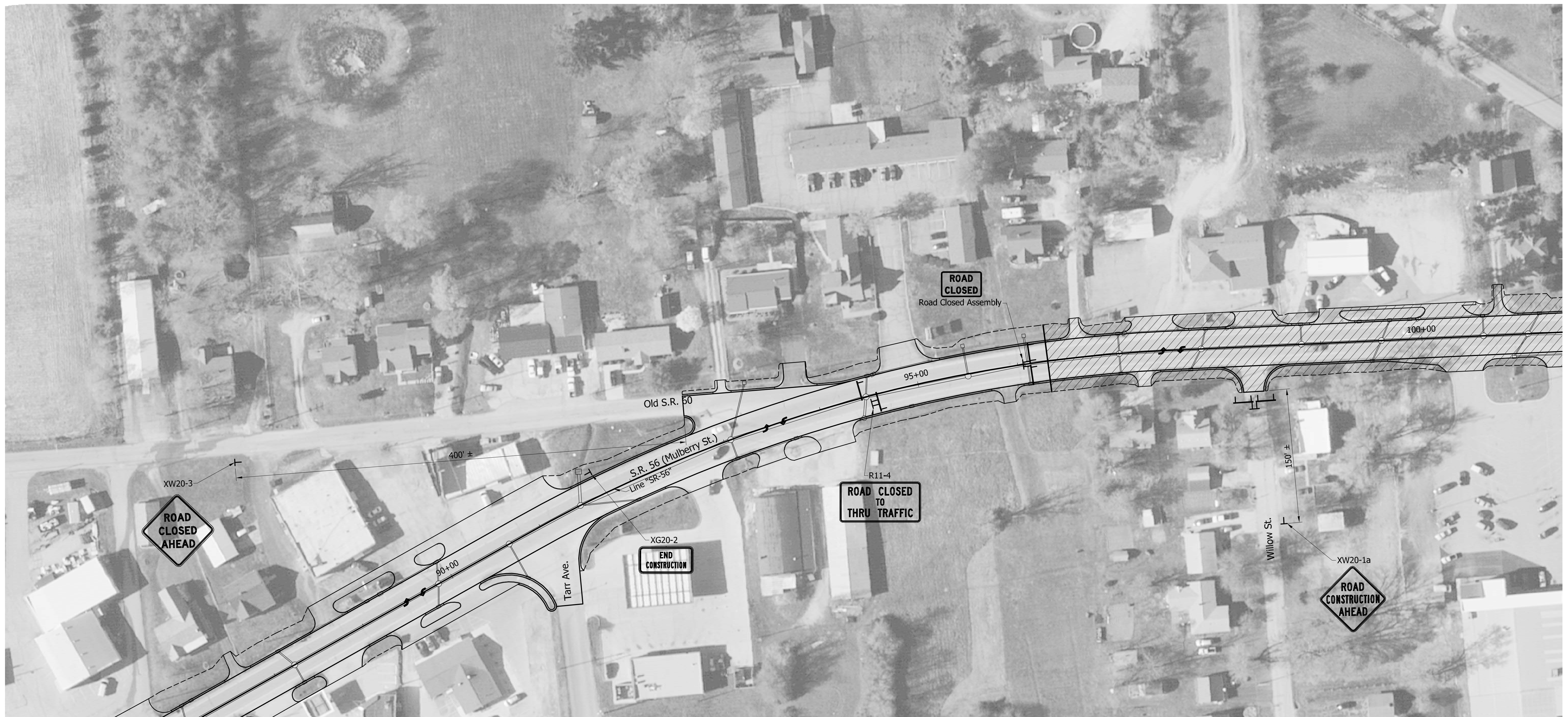
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CHECKED: GRW	CHECKED: GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC MAINTENANCE  
PHASE 1A**

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE
VERTICAL SCALE 1" = 50'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 14 of 221
CONTRACT R-39905	PROJECT 1600873

S:\2017\07\2017-401\_SR\_56\_Salmon\_1600873\Production Drawings\6-Traffic Maintenance Details\2017-401-Detour-Seg 1.dwg, SR 56 SEG 1.dwg, SR 56 SEG 1.dwg, 5/24/2019 10:35:16 AM

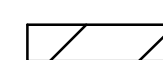




**PHASE 1B CONSTRUCTION SEQUENCE**

1. Install phase 1B signs as shown on plans.
2. Construct proposed storm sewer, main line pavement, street approaches and drives from Sta. 96+30.00 to Sta. 112+80.00 "SR 56".

Notes: 1. Access to all drives must be maintained.

**LEGEND**

-  Phase Construction
-  Construction Sign
-  Type III-B Barricade

**NOT FOR CONSTRUCTION**

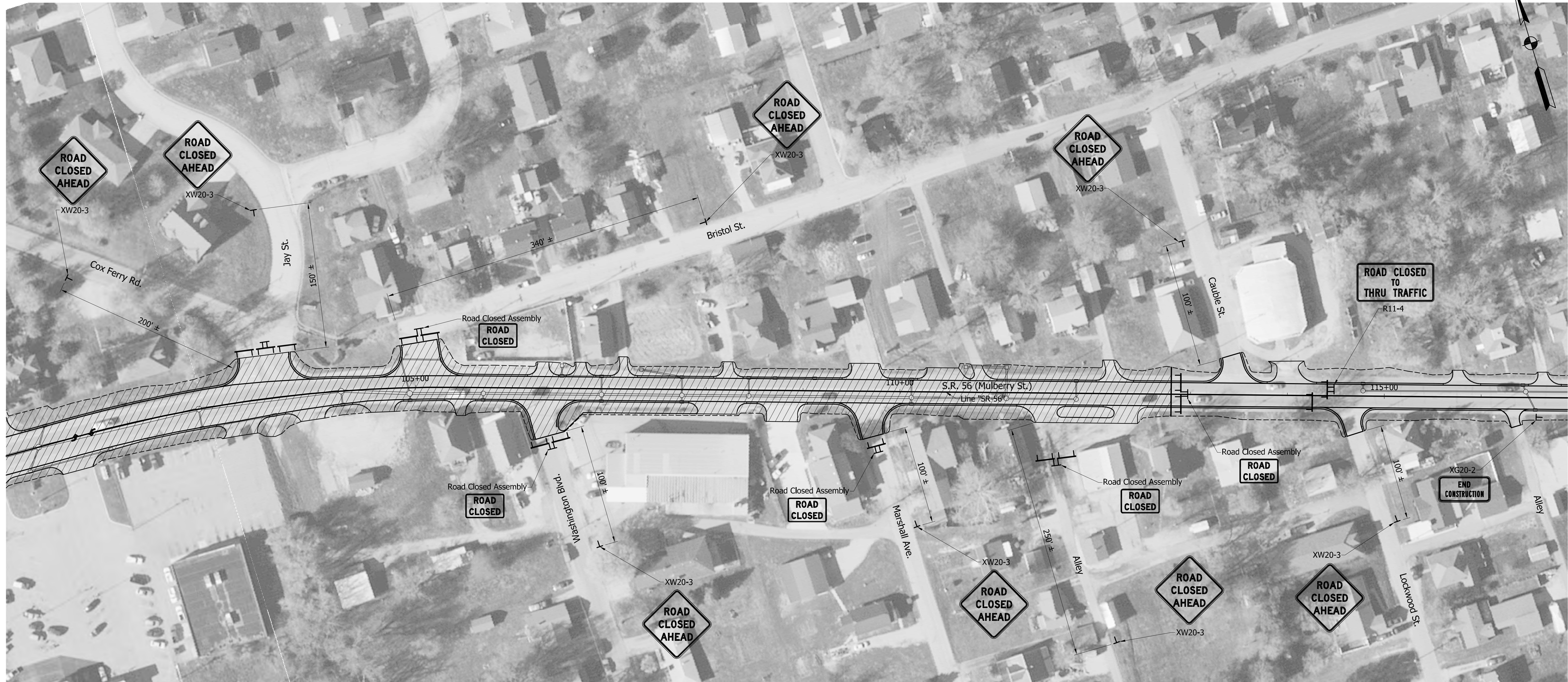
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CHECKED: GRW	CHECKED: GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC MAINTENANCE  
PHASE 1B**

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE
VERTICAL SCALE 1" = 50'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 15 of 221
CONTRACT R-39905	PROJECT 1600873

S:\2017\07\2017-401\_SR\_56\_S&M\_1600873\Production Drawings\6-Traffic Maintenance Details\2017-401-Debus-Seg 2.dwg, SR 56 SEG 2.dwg, SR 56 SEG 2.dwg, 5/24/2019 10:57:56 AM

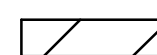




**PHASE 1B CONSTRUCTION SEQUENCE**

1. Install phase 1B signs as shown on plans.
2. Construct proposed storm sewer, main line pavement, street approaches and drives from Sta. 96+30.00 to Sta. 112+80.00 "SR 56".

Notes: 1. Access to all drives must be maintained.

**LEGEND**

-  Phase Construction
-  Construction Sign
-  Type III-B Barricade

**NOT FOR CONSTRUCTION**

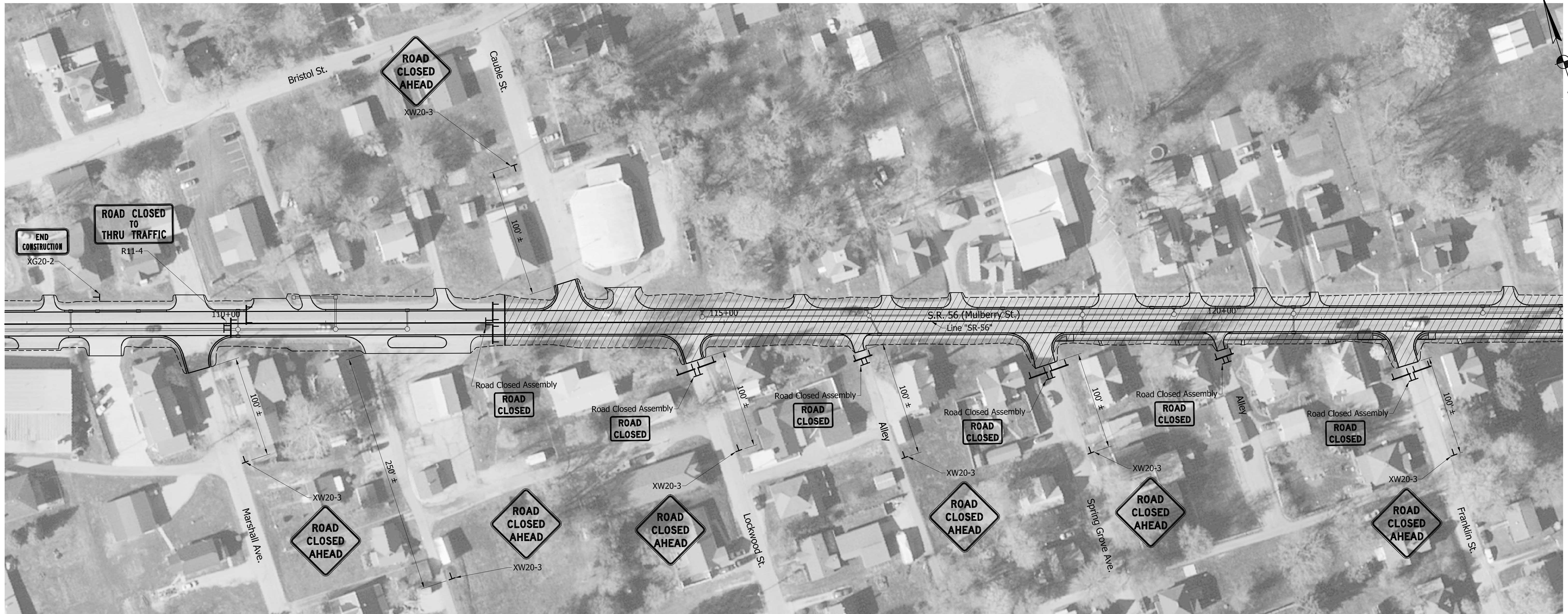
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CHECKED: GRW	CHECKED: GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC MAINTENANCE  
PHASE 1B**

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE
VERTICAL SCALE 1" = 50'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 16 of 221
CONTRACT R-39905	PROJECT 1600873

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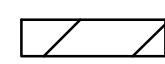




**PHASE 1C CONSTRUCTION SEQUENCE**

1. Install phase 1C signs as shown on plans.
2. Construct proposed storm sewer, main line pavement, street approaches drives, and sidewalks from Sta. 112+80.00 to Sta. 133+84.07 "SR 56".

Notes: 1. Access to all drives must be maintained.

**LEGEND**

-  Phase Construction
-  Construction Sign
-  Type III-B Barricade

**NOT FOR CONSTRUCTION**

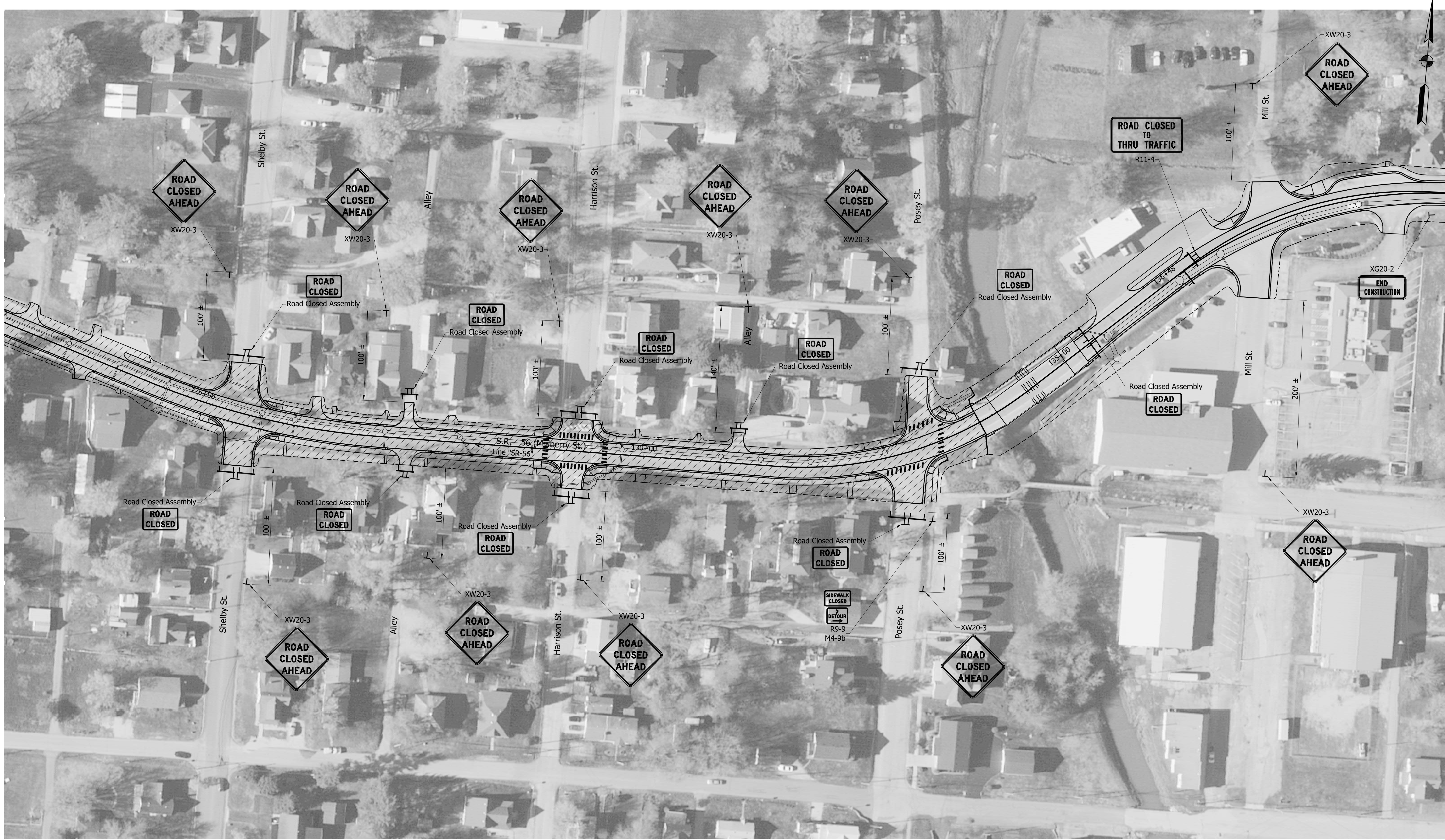
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CHECKED: GRW	CHECKED: GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC MAINTENANCE  
PHASE 1C**

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE
VERTICAL SCALE 1" = 50'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 17 of 221
CONTRACT R-39905	PROJECT 1600873

S:\2017\07\2017-401\_SR\_56\_Salmon\_1600873\Production Drawings\06-Traffic Maintenance Details\2017-401-Detour-Seg 3.dwg, SR 56 SEG 3.dwg, 5/24/2019 10:59:21 AM

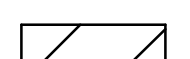




**PHASE 1C CONSTRUCTION SEQUENCE**

1. Install phase 1C signs as shown on plans.
2. Construct proposed storm sewer, main line pavement, street approaches drives, and sidewalks from Sta. 112+80.00 to Sta. 133+84.07 "SR 56".

Notes: 1. Access to all drives must be maintained.

**LEGEND**

-  Phase Construction
-  Construction Sign
-  Type III-B Barricade

**NOT FOR CONSTRUCTION**

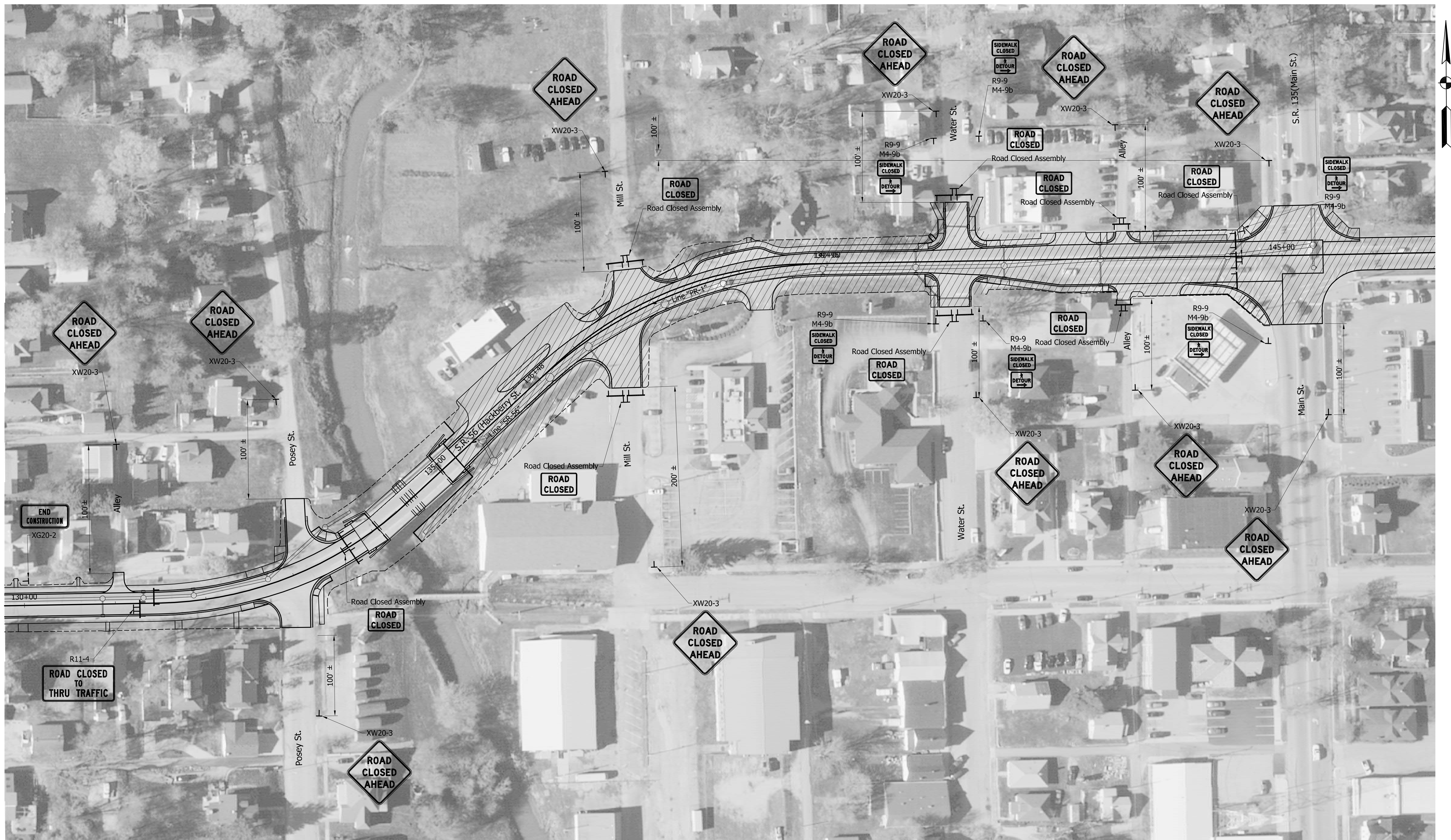
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CHECKED: GRW	CHECKED: GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC MAINTENANCE  
PHASE 1C**

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE
VERTICAL SCALE 1" = 50'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 18 of 221
CONTRACT R-39905	PROJECT 1600873

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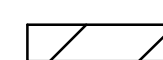




**PHASE 1D CONSTRUCTION SEQUENCE**

1. Install phase 1D signs as shown on plans.
2. Construct proposed storm sewer, main line pavement, street approaches drives, and sidewalks from Sta. 135+22.02 to Sta. 144+39.66 "SR 56". Mill and resurface from Sta. 144+39.66 to Sta. 148+54.00 "SR 56".

Notes: 1. Access to all drives must be maintained.

**LEGEND**

-  Phase Construction
-  Construction Sign
-  Type III-B Barricade

**NOT FOR CONSTRUCTION**

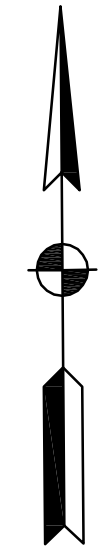
DESIGNED: _____	BDC	DRAWN: _____	BDC
CHECKED: _____	GRW	CHECKED: _____	GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC MAINTENANCE  
PHASE 1D**

HORIZONTAL SCALE	BRIDGE FILE
1" = 50'	
VERTICAL SCALE	DESIGNATION
1" = 50'	1600873
SURVEY BOOK	SHEETS
	19 of 221
CONTRACT	PROJECT
R-39905	1600873

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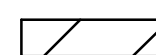




**PHASE 1D CONSTRUCTION SEQUENCE**

1. Install phase 1D signs as shown on plans.
2. Construct proposed storm sewer, main line pavement, street approaches drives, and sidewalks from Sta. 135+22.02 to Sta. 144+39.66 "SR 56". Mill and resurface from Sta. 144+39.66 to Sta. 148+54.00 "SR 56".

Notes: 1. Access to all drives must be maintained.

**LEGEND**

-  Phase Construction
-  Construction Sign
-  Type III-B Barricade

**NOT FOR CONSTRUCTION**

DESIGNED: _____	BDC	DRAWN: _____	BDC
CHECKED: _____	GRW	CHECKED: _____	GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC MAINTENANCE  
PHASE 1D**

HORIZONTAL SCALE	BRIDGE FILE
1" = 50'	
VERTICAL SCALE	DESIGNATION
1" = 50'	1600873
SURVEY BOOK	SHEETS
	20 of 221
CONTRACT	PROJECT
R-39905	1600873

S:\2017\07\2017-401\_SR\_56\_Salmon\_1600873\Production Drawings\06-Traffic Maintenance Details\2017-401-06a-usg-4dwg\_SR\_56\_SEG\_402\_5\20170719\_10282620.dwg



72+00

73+00

74+00

75+00

76+00

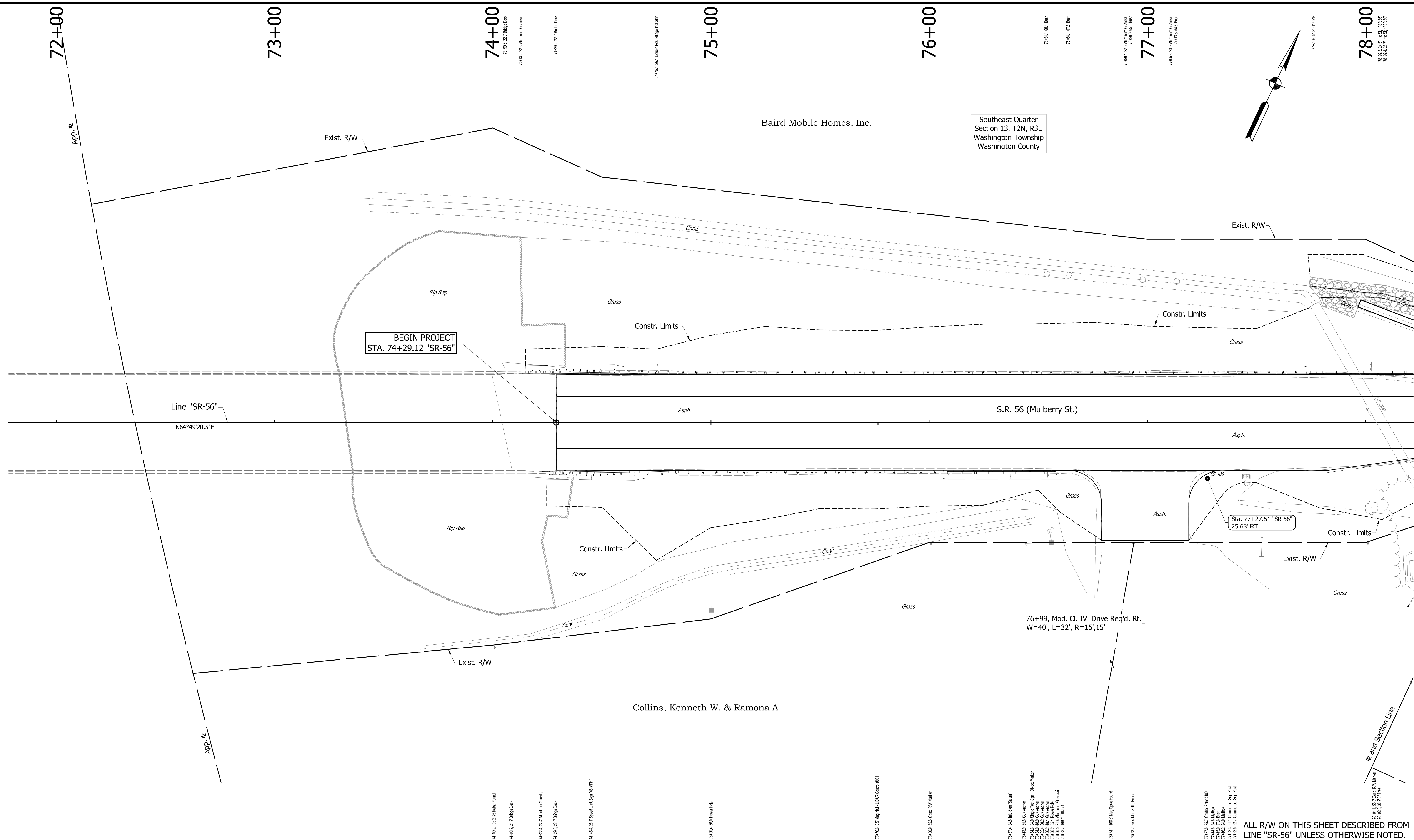
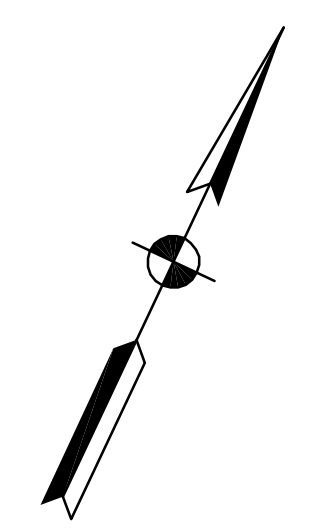
77+00

78+00

Southeast Quarter  
Section 13, T2N, R3E  
Washington Township  
Washington County

Baird Mobile Homes, Inc.

Collins, Kenneth W. & Ramona A



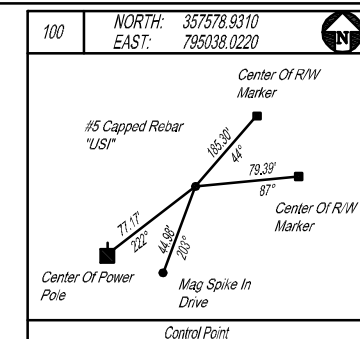
Line "SR-56"  
N64°49'20.5"E

S.R. 56 (Mulberry St.)

Sta. 77+27.51 "SR-56"  
25.68' RT.

76+99, Mod. Cl. IV Drive Req'd. Rt.  
W=40', L=32', R=15',15'

ALL R/W ON THIS SHEET DESCRIBED FROM  
LINE "SR-56" UNLESS OTHERWISE NOTED.



**NOT FOR  
CONSTRUCTION**

DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PLAN  
S.R. 56**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 20'	DESIGNATION 1600873
SURVEY BOOK	SHEETS
CONTRACT R-39905	21 of 221 PROJECT 1600873

74+00.8, 103.26' Rebar Found

74+00.9, 21.9' Bridge Deck

74+02.4, 22.4' Aluminum Guardrail

74+09.0, 20.0' Bridge Deck

74+46.4, 26.1' Speed Limit Sign 45 MPH

75+00.0, 80.0' Power Pole

75+76.6, 0.5' Mag. Nail, UGAR Control 6861

76+00.8, 55.5' Conc. RW Marker

76+27.4, 4.2' 1/2" Sign 'Salem'

76+28.9, 5.0' Sign Anchor

76+30.9, 4.2' 1/2" Sign 'Object Marker'

76+31.9, 4.2' 1/2" Sign 'Object Marker'

76+32.4, 30.2' Sign Anchor

76+32.5, 25.5' Power Pole

76+32.5, 21.8' Aluminum Guardrail

76+37.1, 18.1' TBM #1

76+74.1, 16.6' Mag. Stake Found

76+82.7, 5.6' Mag. Stake Found

77+02.5, 26.7' Control BM #110

77+44.6, 24.8' Mulch

77+46.9, 27.6' Bush

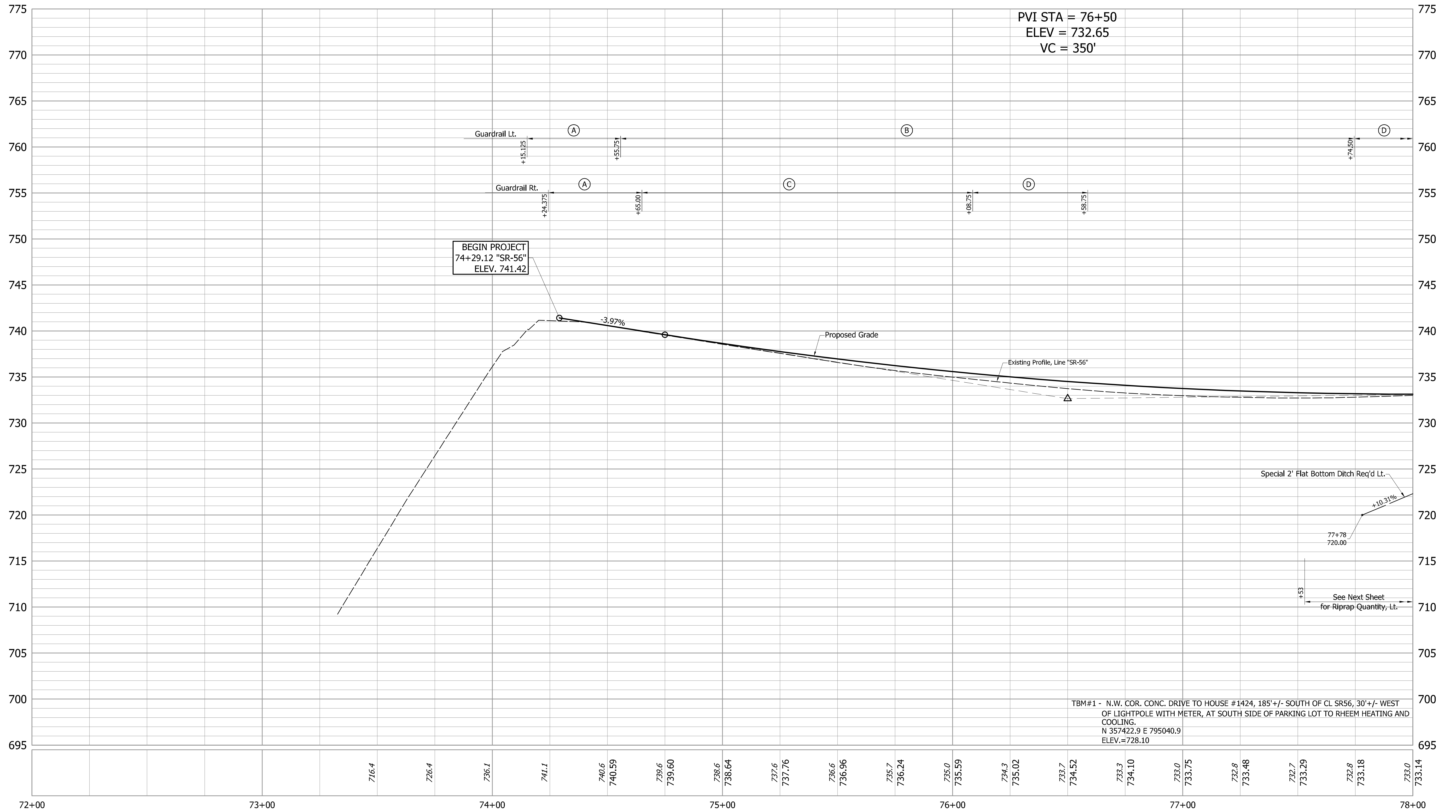
77+52.3, 4.2' 1/2" Sign 'Object Marker'

77+52.5, 52.7' Commercial Sign Post

78+01.1, 55.6' Conc. RW Marker

78+02.8, 38.9' 2" Tree

S:\2017Proj\2017-401 SR 56 Salem 1600873\Production Drawings\Plan and Profile\2017-401-01-01-Plan-Profile-SR-56.dwg, 2017-01-01 11:04:10 AM



- LEGEND**
- (A) MGS Guardrail Transition
  - (B) 318.75' MGS W-Beam Guardrail
  - (C) 143.75' MGS W-Beam Guardrail
  - (D) Guardrail End Treatment Type OS

**NOT FOR CONSTRUCTION**

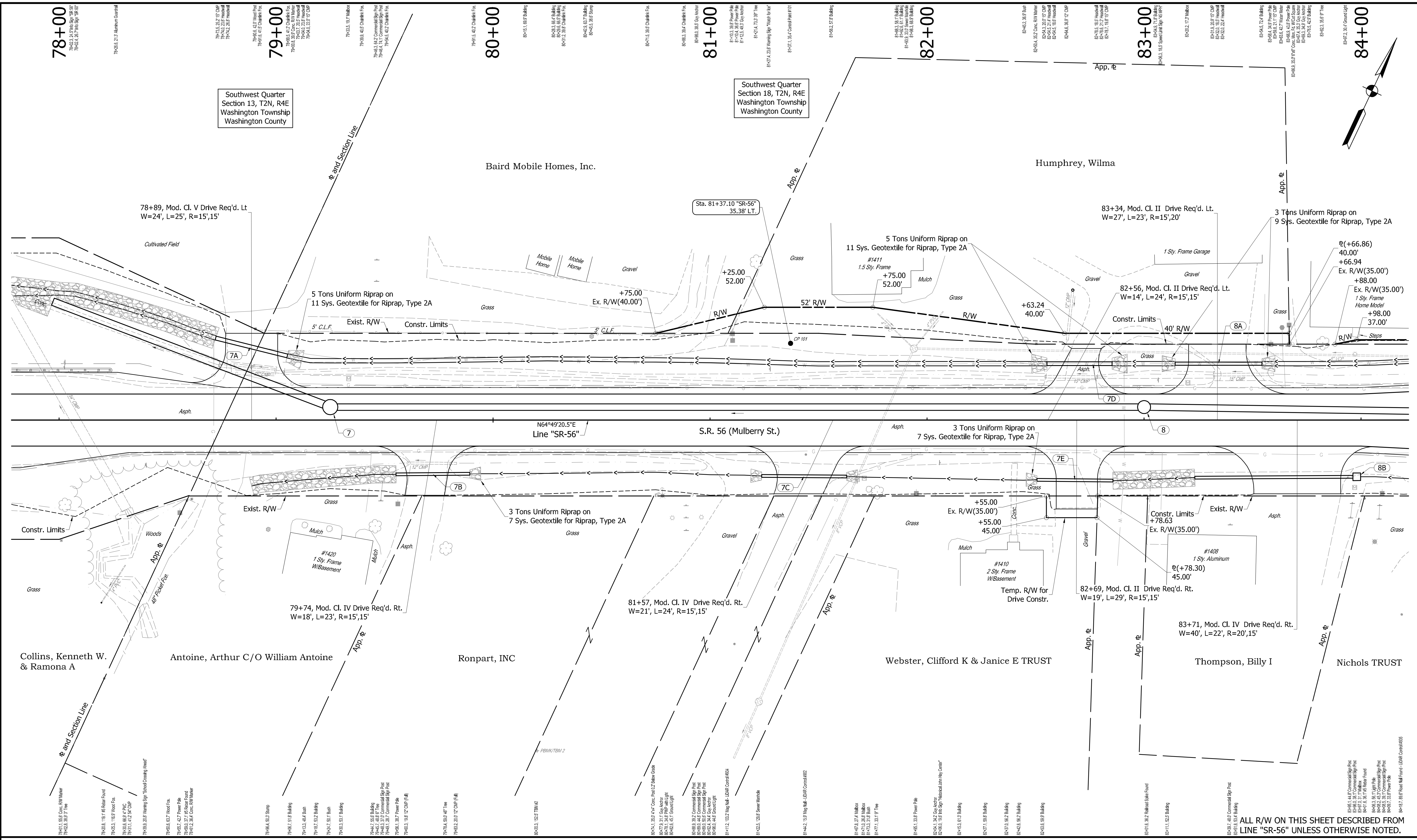
DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PROFILE  
S.R. 56**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS
CONTRACT R-39905	22 of 221 PROJECT 1600873

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Southwest Quarter  
Section 13, T2N, R4E  
Washington Township  
Washington County

Southwest Quarter  
Section 18, T2N, R4E  
Washington Township  
Washington County

Baird Mobile Homes, Inc.

Humphrey, Wilma

Collins, Kenneth W.  
& Ramona A

Antoine, Arthur C/O William Antoine

Ronpart, INC

Webster, Clifford K & Janice E TRUST

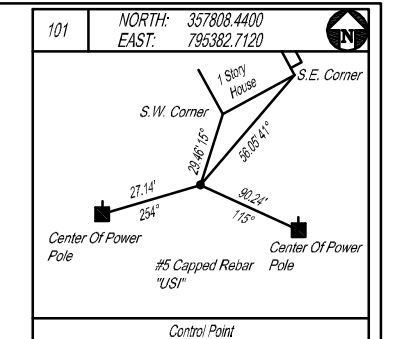
Thompson, Billy I

Nichols TRUST

N64°49'20.5"E  
Line "SR-56"

S.R. 56 (Mulberry St.)

ALL R/W ON THIS SHEET DESCRIBED FROM  
LINE "SR-56" UNLESS OTHERWISE NOTED.



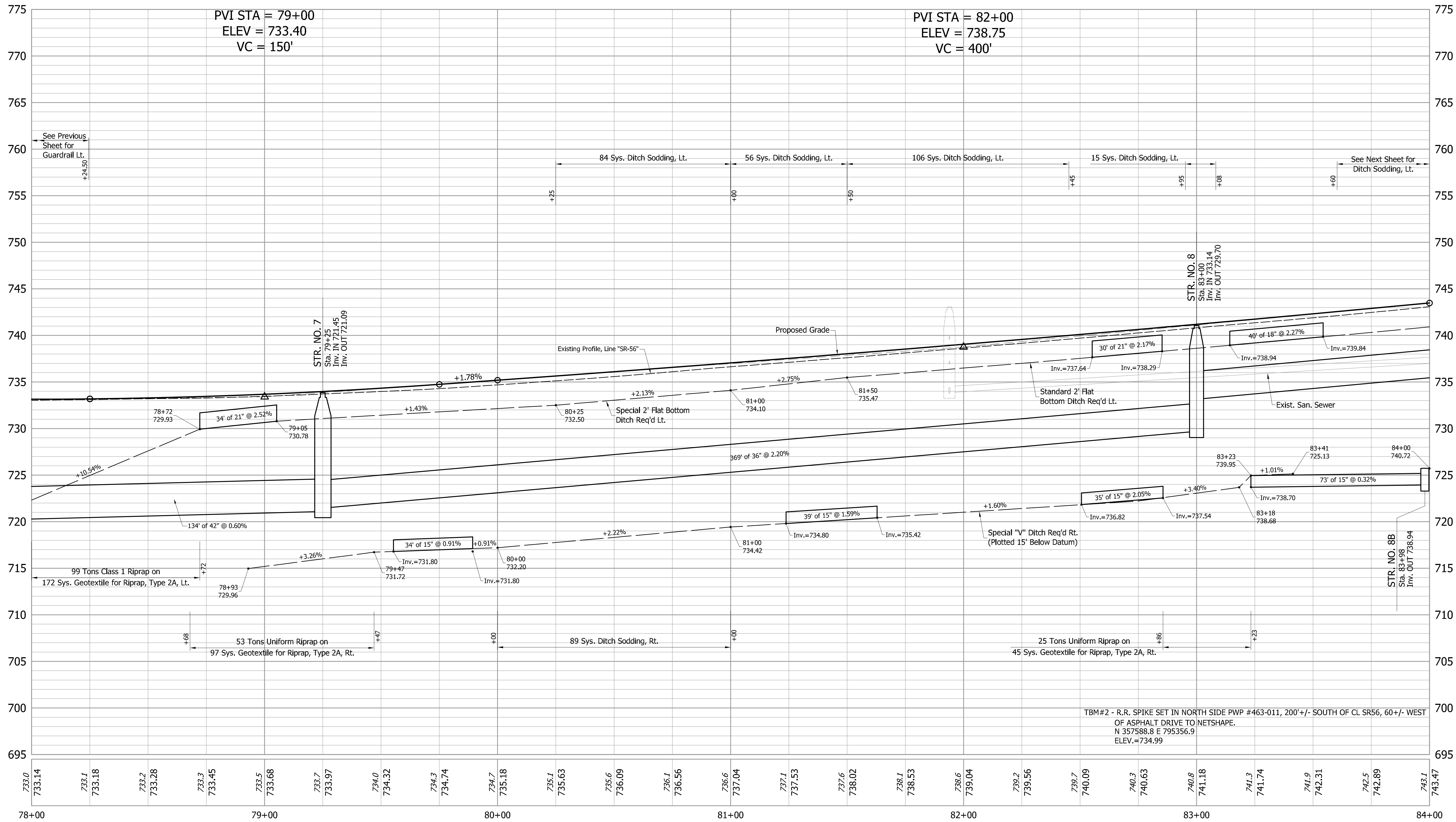
**NOT FOR  
CONSTRUCTION**

DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PLAN  
S.R. 56**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 20'	DESIGNATION 1600873
SURVEY BOOK	SHEETS
CONTRACT R-39905	23 of 221 PROJECT 1600873



**NOT FOR CONSTRUCTION**

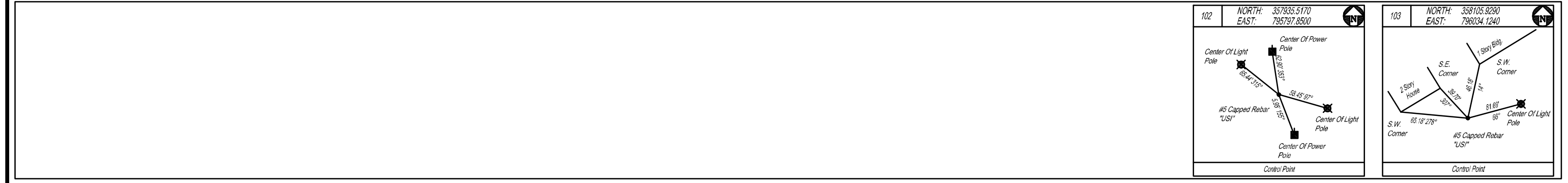
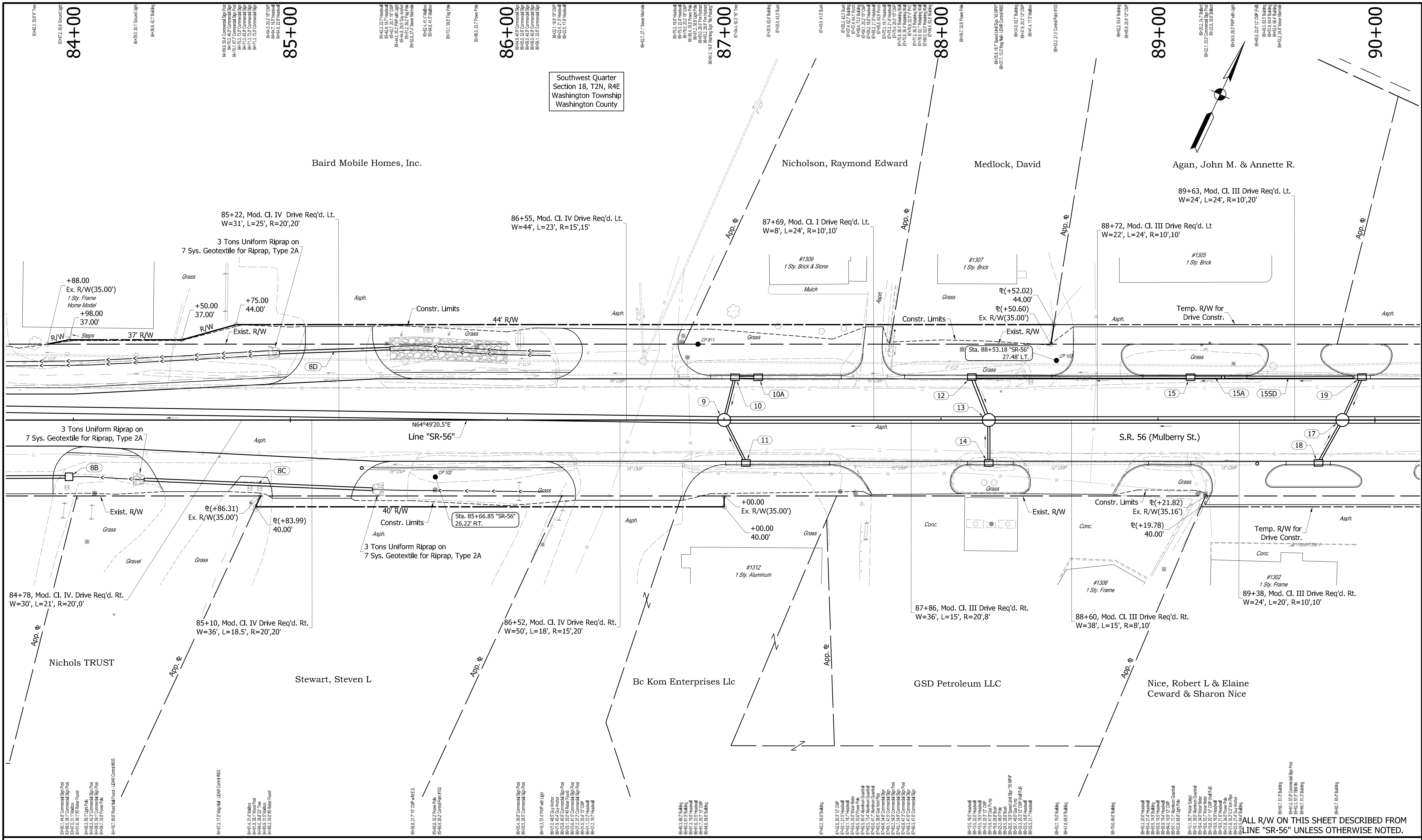
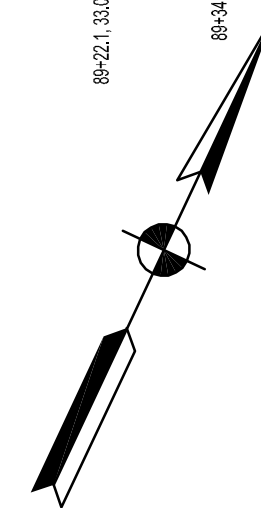
DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**

**PROFILE**  
**S.R. 56**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS
CONTRACT R-39905	24 of 221
	PROJECT 1600873

Southwest Quarter  
Section 18, T2N, R4E  
Washington Township  
Washington County



**NOT FOR CONSTRUCTION**

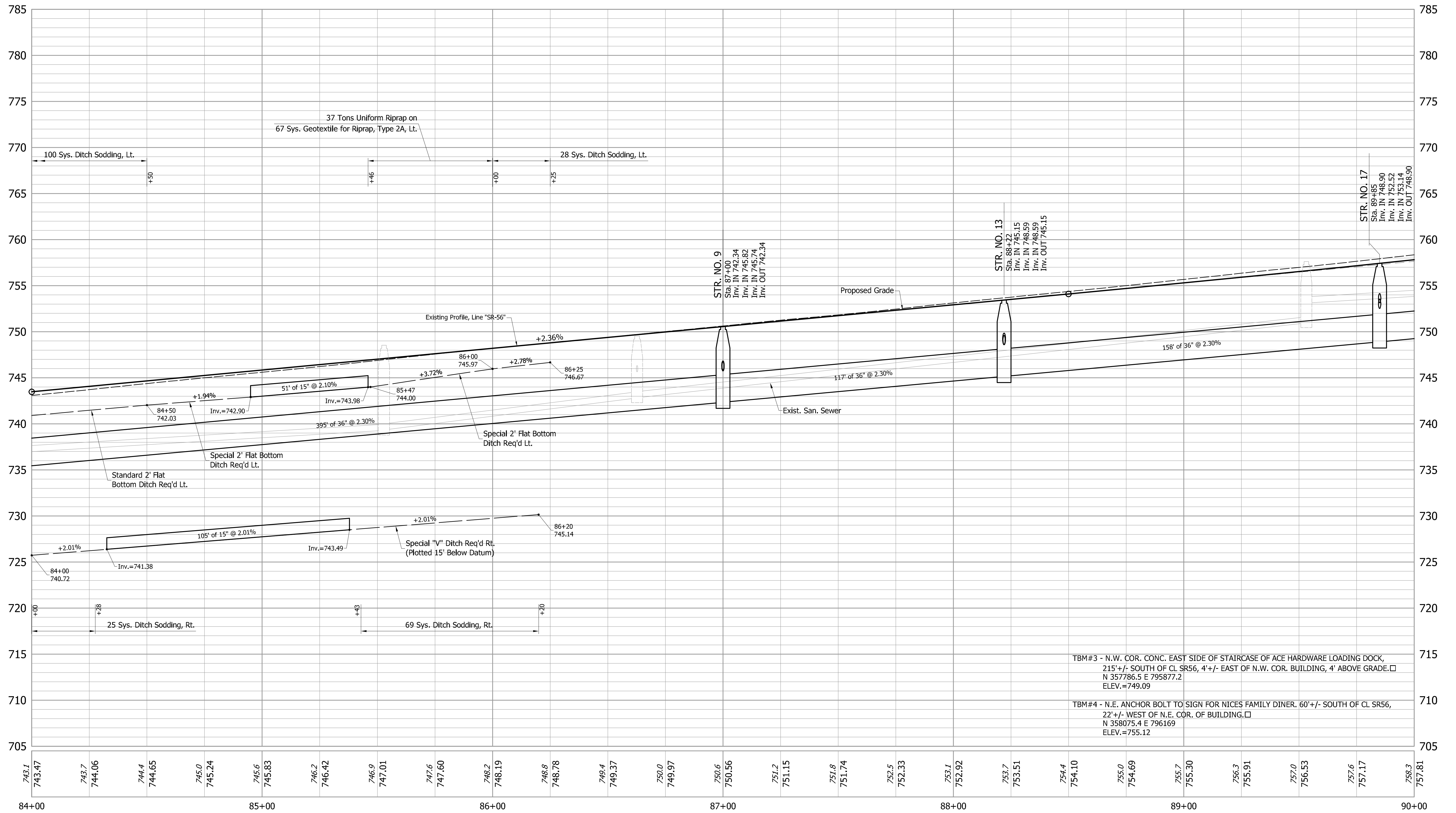
DESIGNED:	JGP	DRAWN:	JGP
CHECKED:	GRW	CHECKED:	GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PLAN  
S.R. 56**

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
1" = 20'	1600873
SURVEY BOOK	SHEETS
	25 of 221
CONTRACT	PROJECT
R-39905	1600873

ALL R/W ON THIS SHEET DESCRIBED FROM  
LINE "SR-56" UNLESS OTHERWISE NOTED.



TBM#3 - N.W. COR. CONC. EAST SIDE OF STAIRCASE OF ACE HARDWARE LOADING DOCK,  
 215'+/- SOUTH OF CL SR56, 4'+/- EAST OF N.W. COR. BUILDING, 4' ABOVE GRADE. □  
 N 357786.5 E 795877.2  
 ELEV.=749.09

TBM#4 - N.E. ANCHOR BOLT TO SIGN FOR NICES FAMILY DINER. 60'+/- SOUTH OF CL SR56,  
 22'+/- WEST OF N.E. COR. OF BUILDING. □  
 N 358075.4 E 796169  
 ELEV.=755.12

NOT FOR  
CONSTRUCTION

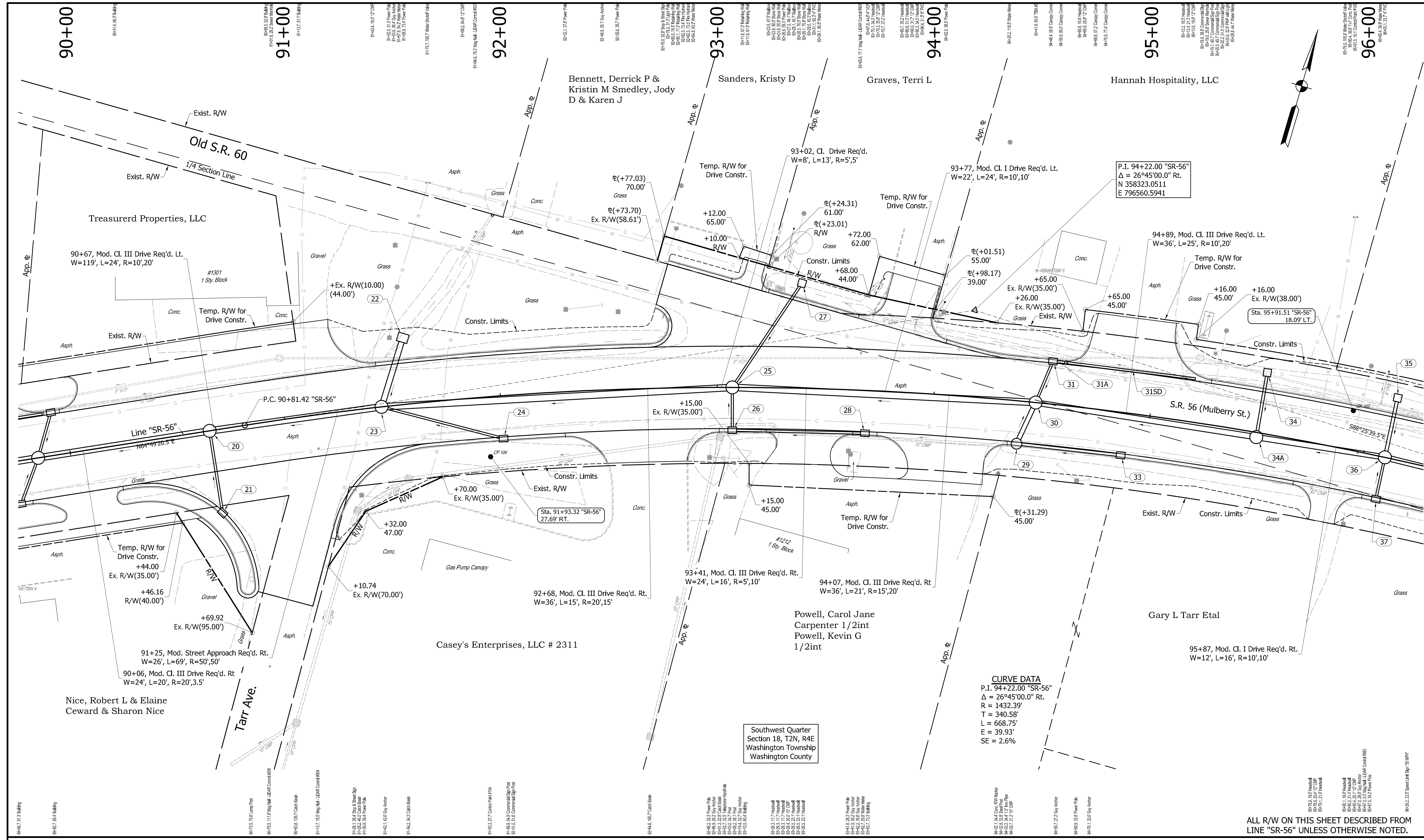
DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PROFILE  
S.R. 56**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS
	26 of 221
CONTRACT R-39905	PROJECT 1600873

S:\2017\Proj\2017-401-56-Salem-1600873\Production Drawings\Profiles and Profiles\2017-401-56-Salem-1600873-Profile-SR-56.dwg, 26/10/2017 11:06:35 AM

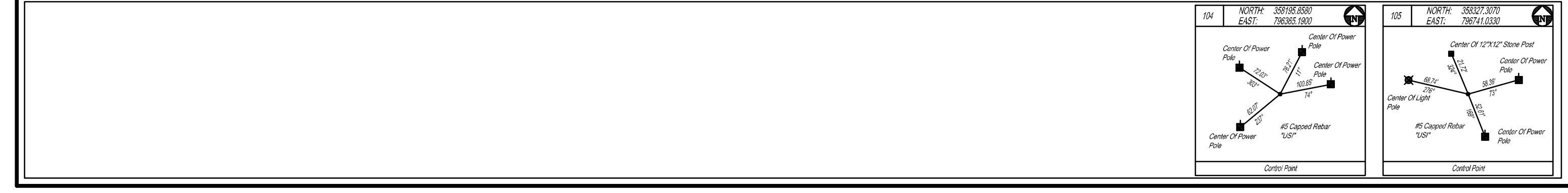


P.I. 94+22.00 "SR-56"  
 $\Delta = 26^\circ 45' 00.0''$  Rt.  
 N 358323.0511  
 E 796560.5941

**CURVE DATA**  
 P.I. 94+22.00 "SR-56"  
 $\Delta = 26^\circ 45' 00.0''$  Rt.  
 R = 1432.39'  
 T = 340.58'  
 L = 668.75'  
 E = 39.93'  
 SE = 2.6%

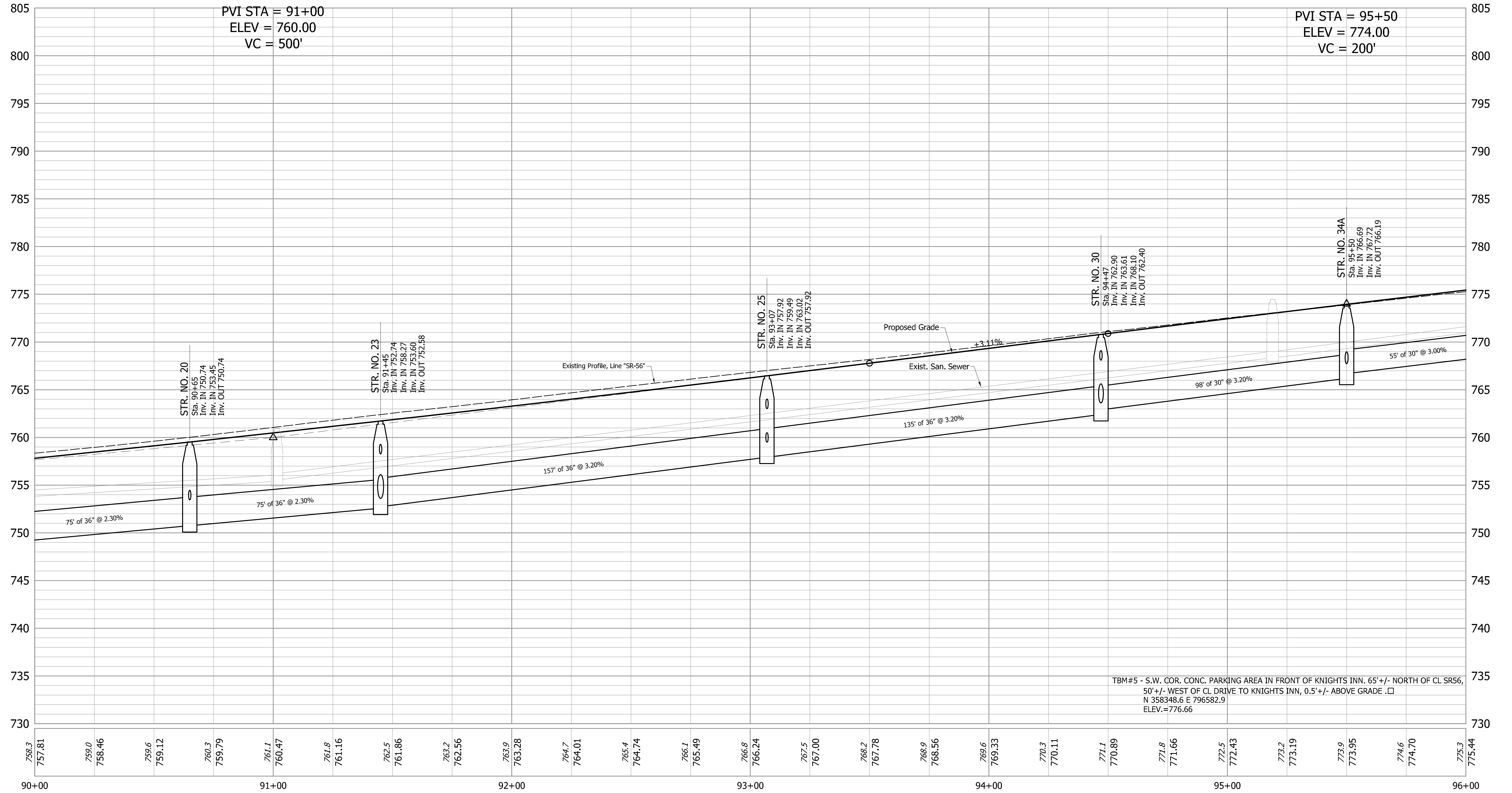
Southwest Quarter  
 Section 18, T2N, R4E  
 Washington Township  
 Washington County

ALL R/W ON THIS SHEET DESCRIBED FROM  
 LINE "SR-56" UNLESS OTHERWISE NOTED.



<b>INDIANA DEPARTMENT OF TRANSPORTATION</b>	
<b>PROFILE S.R. 56</b>	
DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 20'	DESIGNATION 1600873
SURVEY BOOK	SHEETS
R-39905	27 of 221
	PROJECT
	1600873



S:\2017\Proj\2017-401-55-Salem-160873\Production Drawings\Profiles and Profiles\2017-401-55-Salem-160873-Profile-SR-56-Profile-C4\_3/20/2015 11:04:41 AM

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CONSTRUCTION**

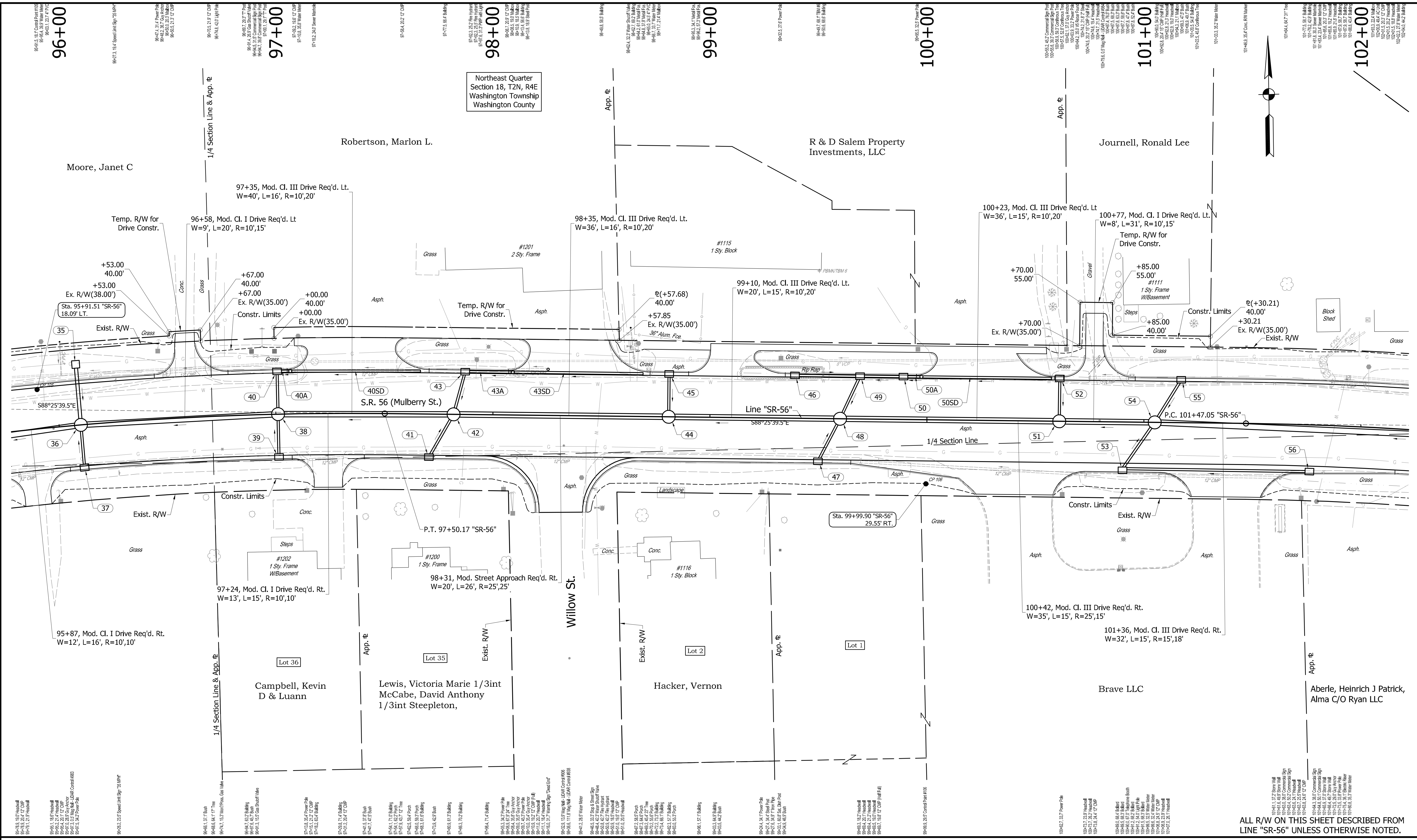
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**INDIANA  
DEPARTMENT OF TRANSPORTATION**

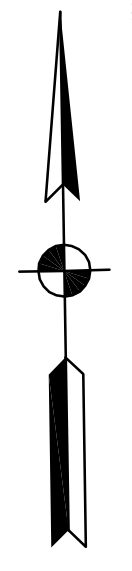
**PROFILE  
S.R. 56**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 28 of 221
CONTRACT R-39905	PROJECT 1600873





Northeast Quarter  
Section 18, T2N, R4E  
Washington Township  
Washington County



**NOT FOR CONSTRUCTION**

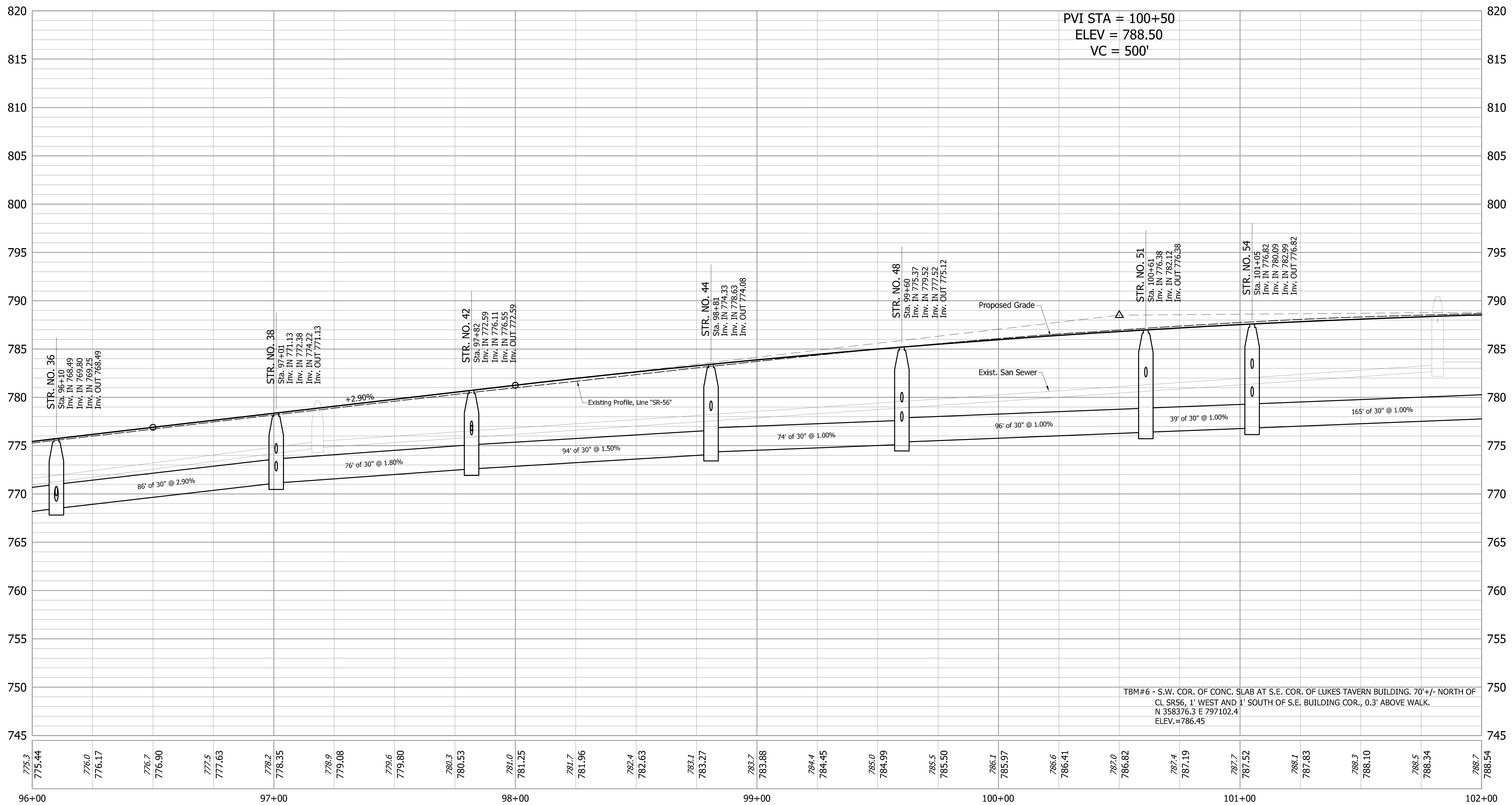
DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PLAN  
S.R. 56**

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
1" = 20'	1600873
SURVEY BOOK	SHEETS
	29 of 221
CONTRACT	PROJECT
R-39905	1600873

ALL R/W ON THIS SHEET DESCRIBED FROM LINE "SR-56" UNLESS OTHERWISE NOTED.

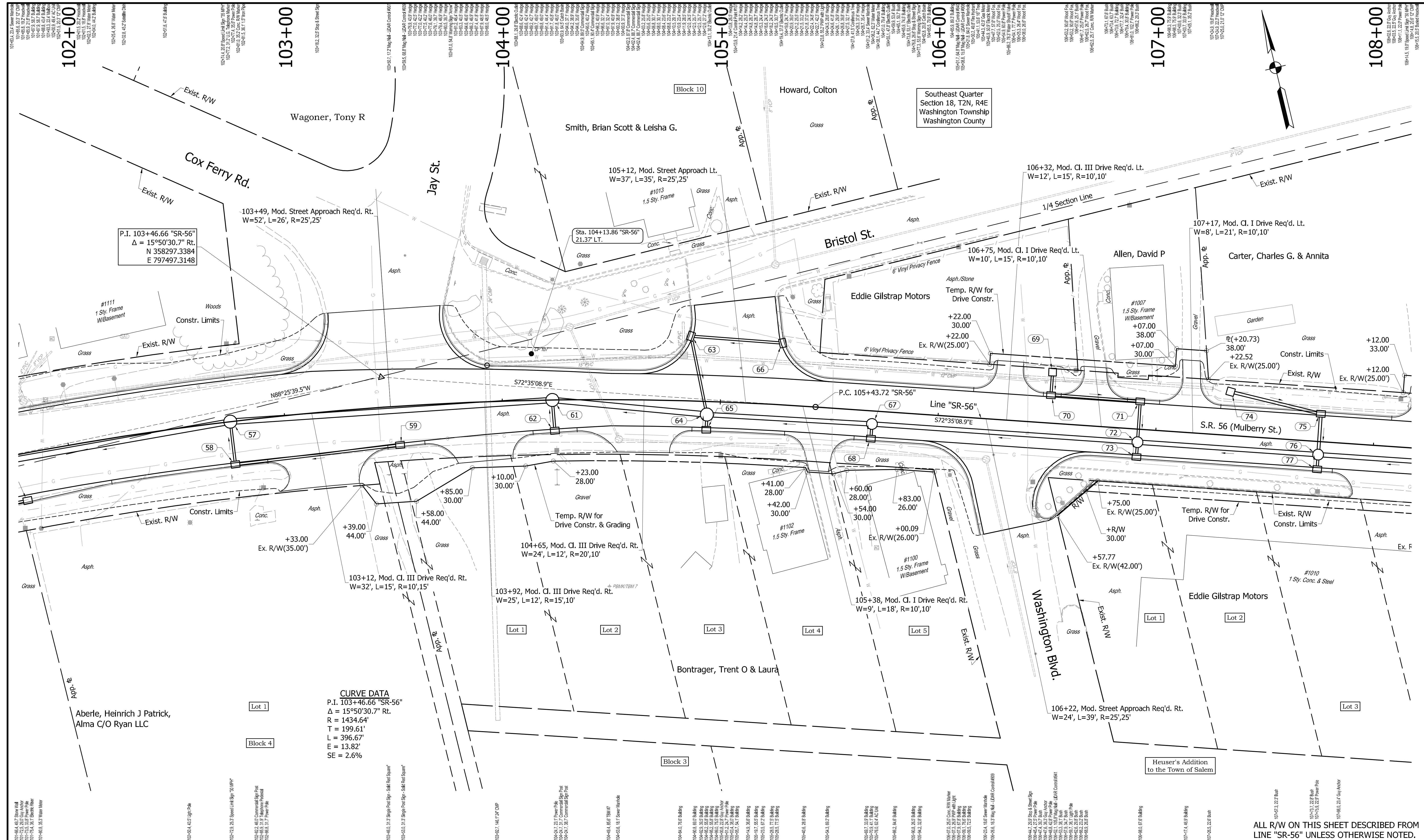


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<b>NOT FOR CONSTRUCTION</b>		DESIGNED: JGP	DRAWN: JGP
		CHECKED: GRW	CHECKED: GRW

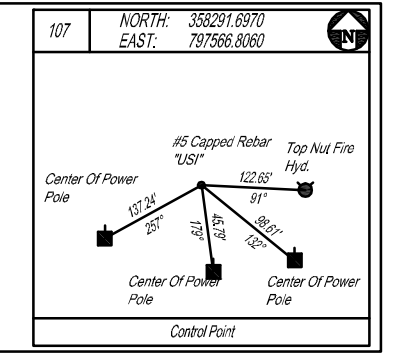
INDIANA DEPARTMENT OF TRANSPORTATION	
PROFILE S.R. 56	

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS
CONTRACT R-39905	30 of 221
	PROJECT 1600873



**CURVE DATA**  
 P.I. 103+46.66 "SR-56"  
 $\Delta = 15^\circ 50' 30.7''$  Rt.  
 R = 1434.64'  
 T = 199.61'  
 L = 396.67'  
 E = 13.82'  
 SE = 2.6%

ALL R/W ON THIS SHEET DESCRIBED FROM LINE "SR-56" UNLESS OTHERWISE NOTED.



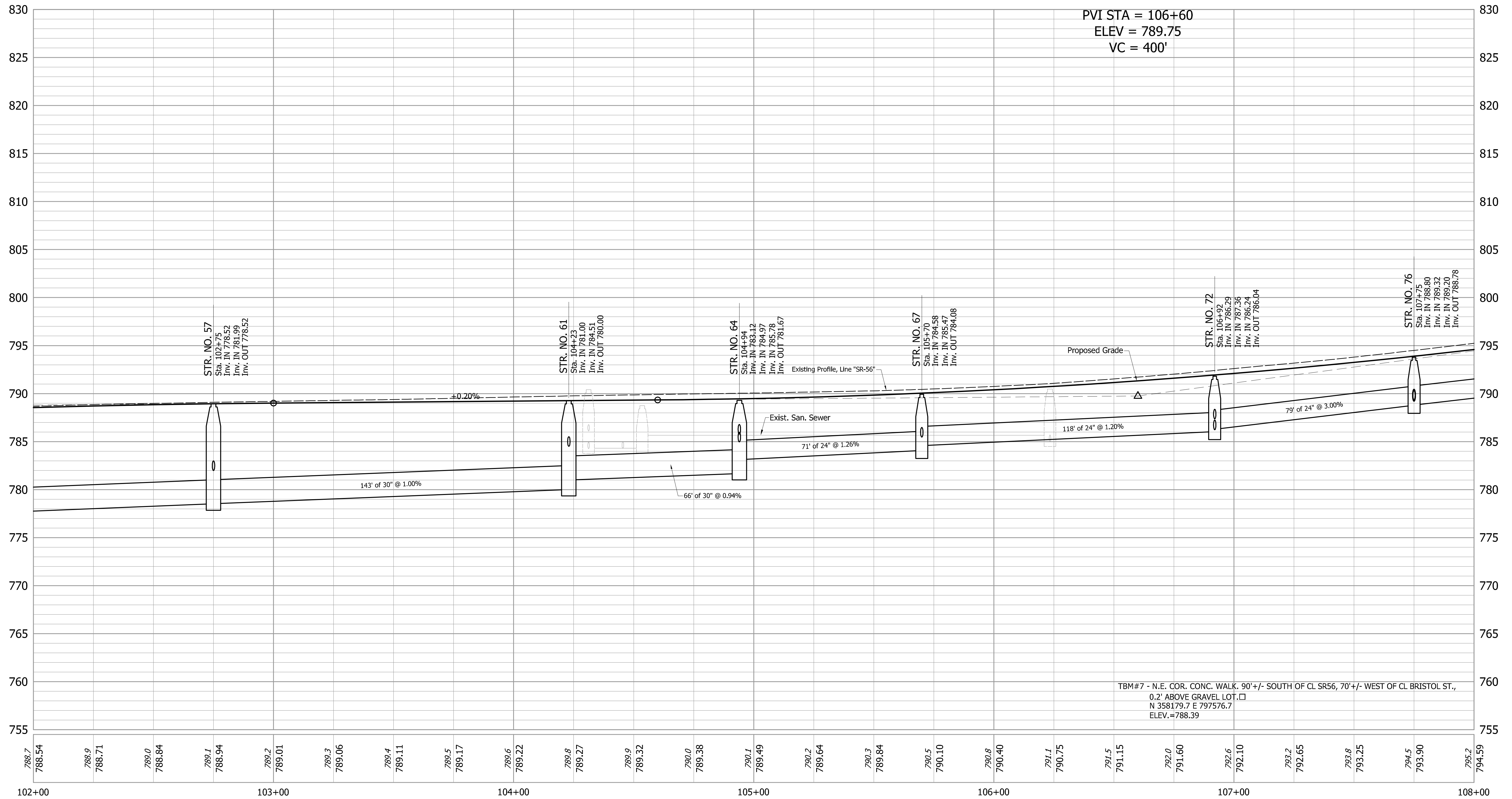
**NOT FOR CONSTRUCTION**

DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

**INDIANA**  
 DEPARTMENT OF TRANSPORTATION

**PLAN**  
 S.R. 56

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
1" = 20'	1600873
SURVEY BOOK	SHEETS
	31 of 221
CONTRACT	PROJECT
R-39905	1600873



S:\2017Proj\2017-401 SR 56 Salem 1608873\Production Drawings\Profiles and Profiles\2017-401 SR 56 Salem 1608873\Profile-SR-56.dwg, 3/2/2017 11:04:52 AM

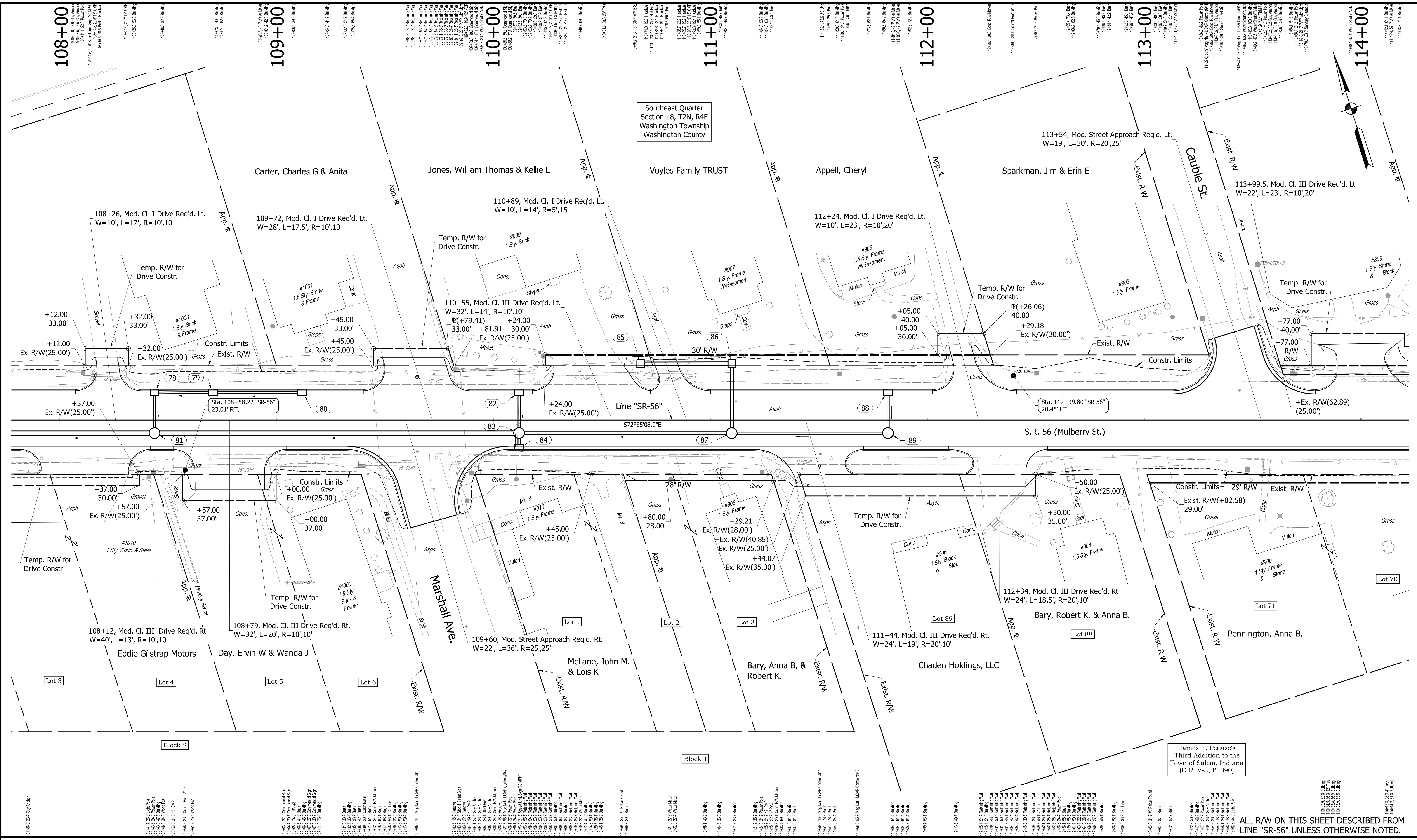
NOT FOR CONSTRUCTION

DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

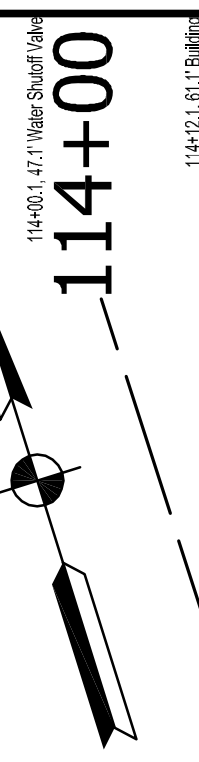
INDIANA  
DEPARTMENT OF TRANSPORTATION

PROFILE  
S.R. 56

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS
CONTRACT R-39905	32 of 221
	PROJECT 1600873

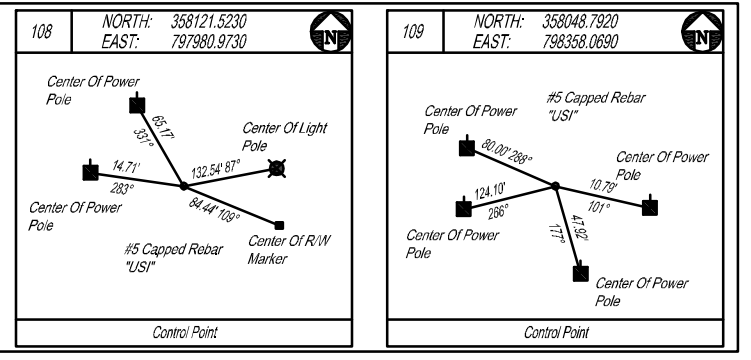


Southeast Quarter  
Section 18, T2N, R4E  
Washington Township  
Washington County



James F. Persie's  
Third Addition to the  
Town of Salem, Indiana  
(D.R. V-3, P. 390)

ALL R/W ON THIS SHEET DESCRIBED FROM  
LINE "SR-56" UNLESS OTHERWISE NOTED.



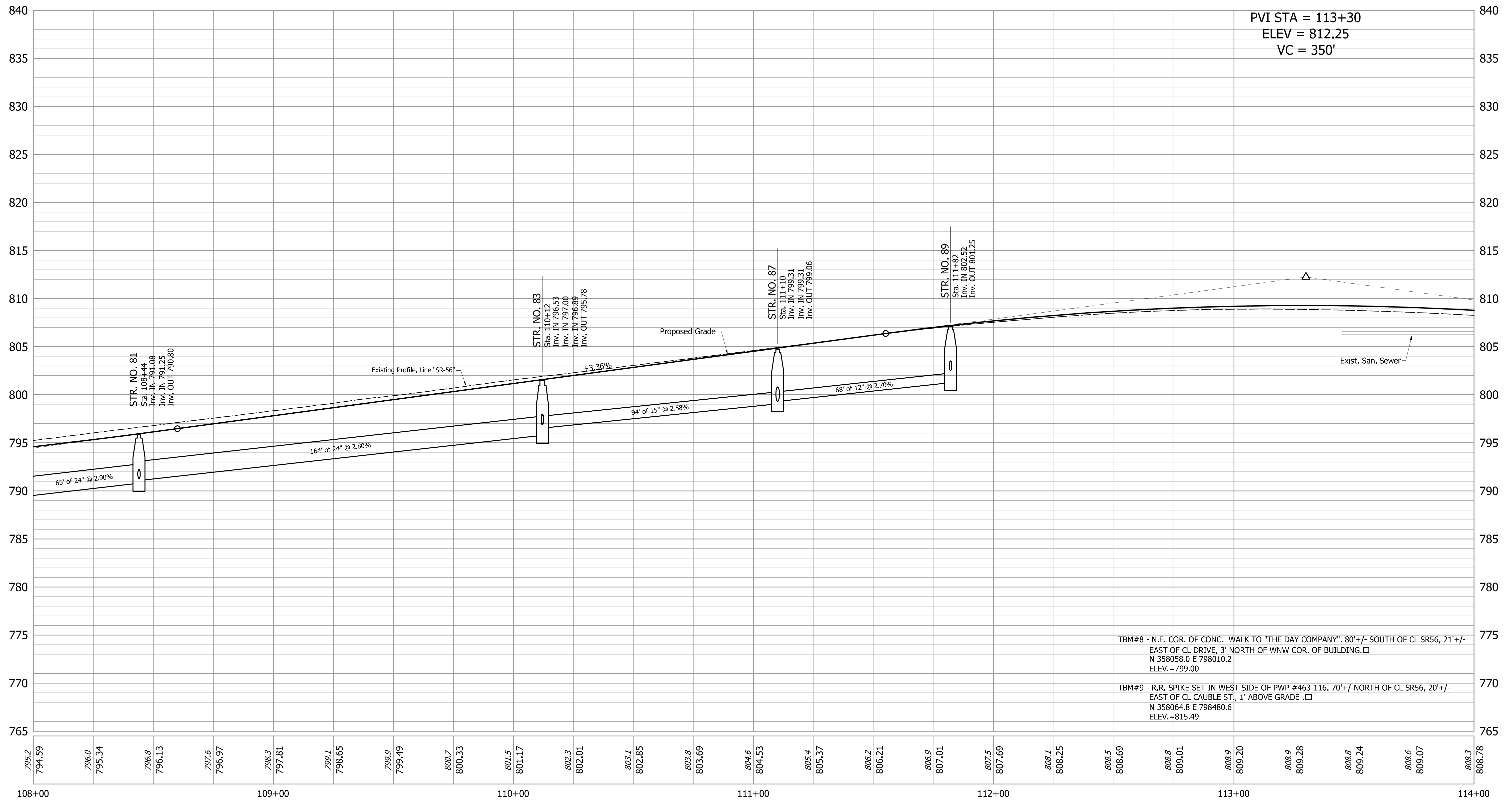
**NOT FOR  
CONSTRUCTION**

DESIGNED:	JGP	DRAWN:	JGP
CHECKED:	GRW	CHECKED:	GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PLAN  
S.R. 56**

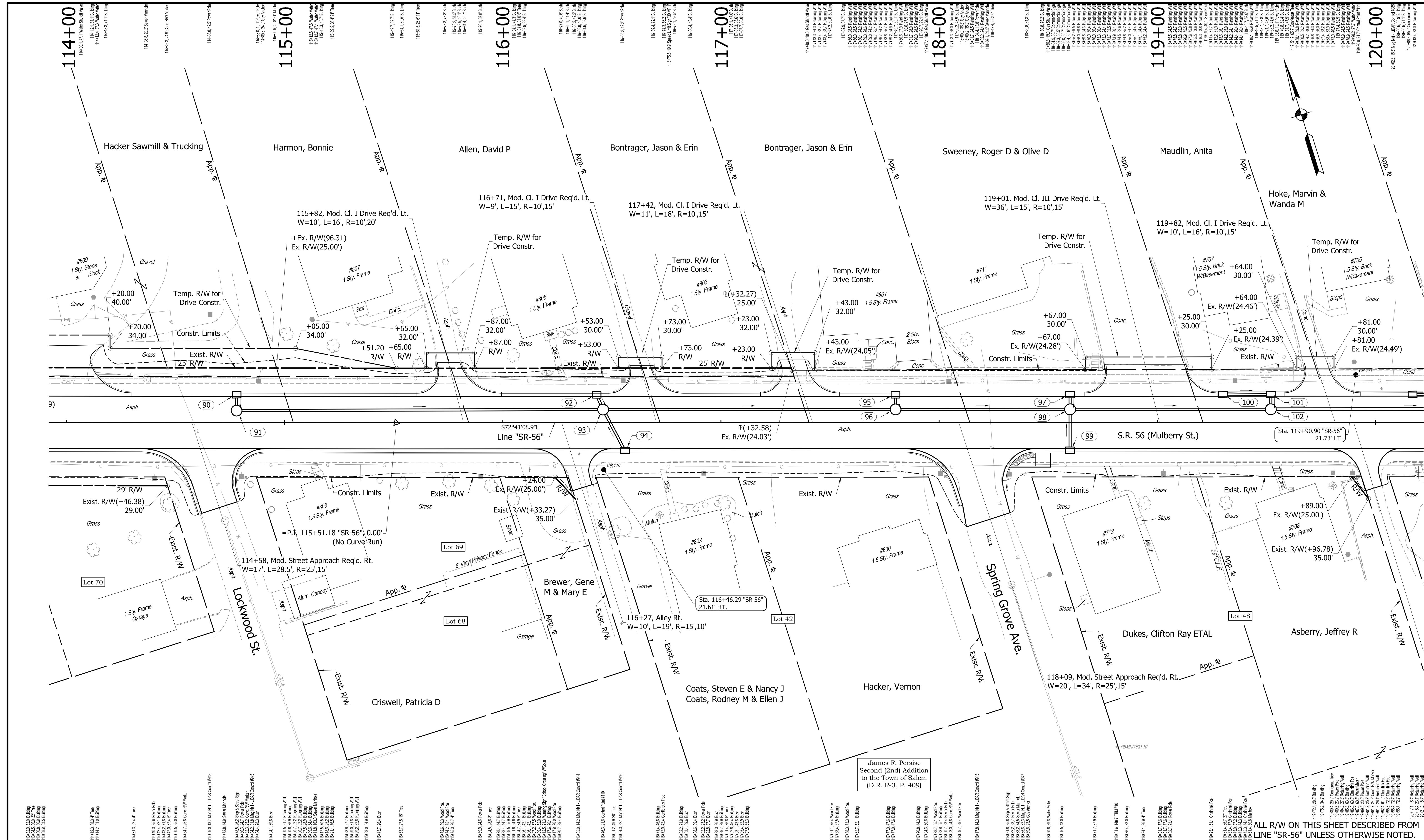
HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
1" = 20'	1600873
SURVEY BOOK	SHEETS
	33 of 221
CONTRACT	PROJECT
R-39905	1600873



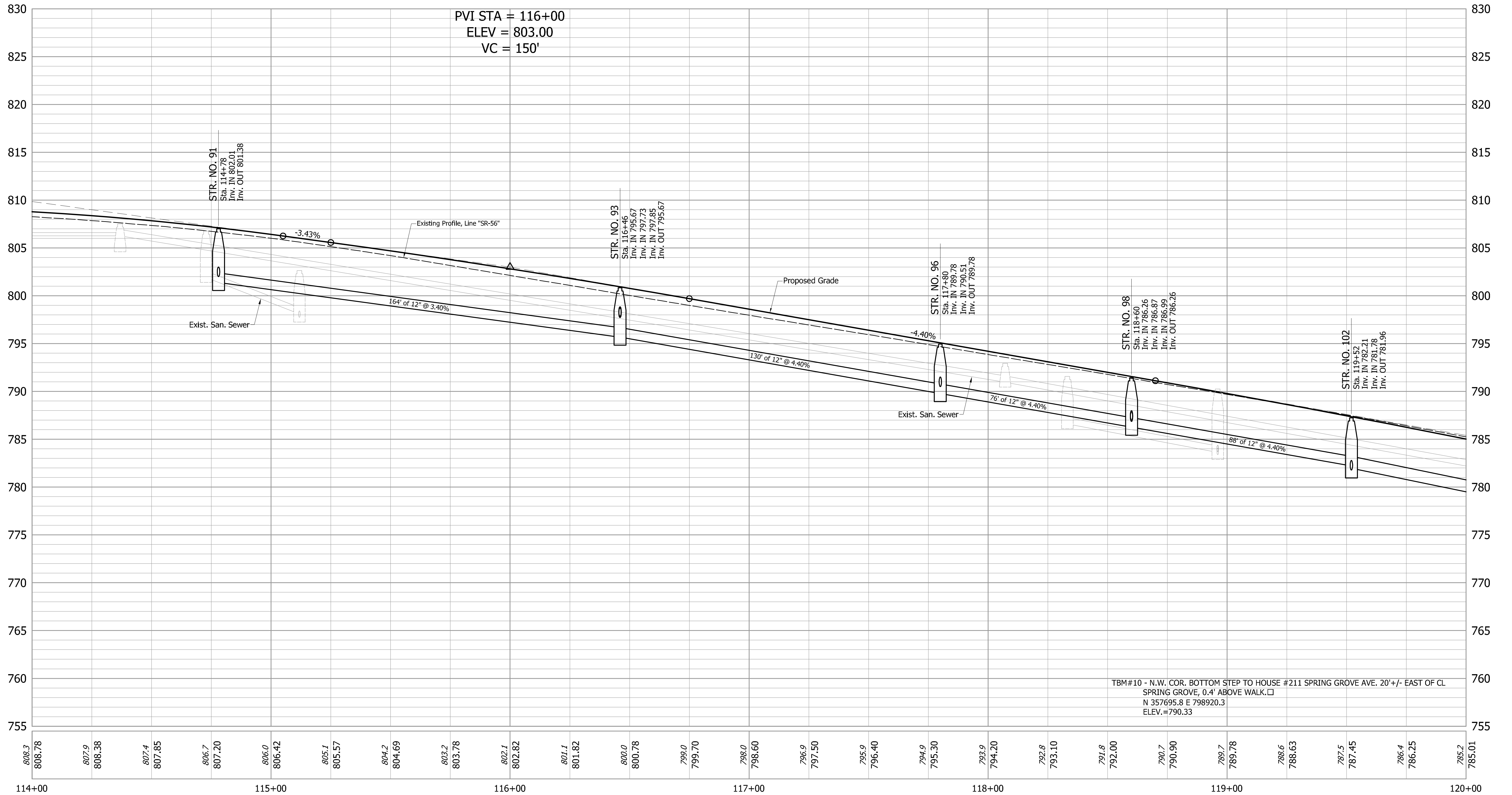
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NOT FOR CONSTRUCTION	DESIGNED: JGP		DRAWN: JGP		<b>INDIANA DEPARTMENT OF TRANSPORTATION</b>  <b>PROFILE S.R. 56</b>	HORIZONTAL SCALE	BRIDGE FILE
	CHECKED: GRW		CHECKED: GRW			1" = 20'	DESIGNATION
	CONTRACT		PROJECT			1" = 5'	1600873
	R-39905		1600873			SURVEY BOOK	SHEETS
						34	of 221

S:\2017\Proj\2017-401\_S.R. 56\_Salem\_160873\Production Drawings\07-Plans and Profiles\2017-401-SR-56-Mulberry-56-56.dwg - Rev=8, 5/29/2019 11:55:01 AM



		<p>ALL R/W ON THIS SHEET DESCRIBED FROM LINE "SR-56" UNLESS OTHERWISE NOTED.</p>	
<p><b>INDIANA</b> DEPARTMENT OF TRANSPORTATION</p>		<p>HORIZONTAL SCALE 1" = 20'</p> <p>VERTICAL SCALE 1" = 20'</p>	
<p>DESIGNED: JGP CHECKED: GRW</p>		<p>DRAWN: JGP CHECKED: GRW</p>	
<p>PLAN S.R. 56</p>		<p>BRIDGE FILE DESIGNATION 1600873</p> <p>SURVEY BOOK R-39905</p> <p>SHEETS 35 of 221 PROJECT 1600873</p>	

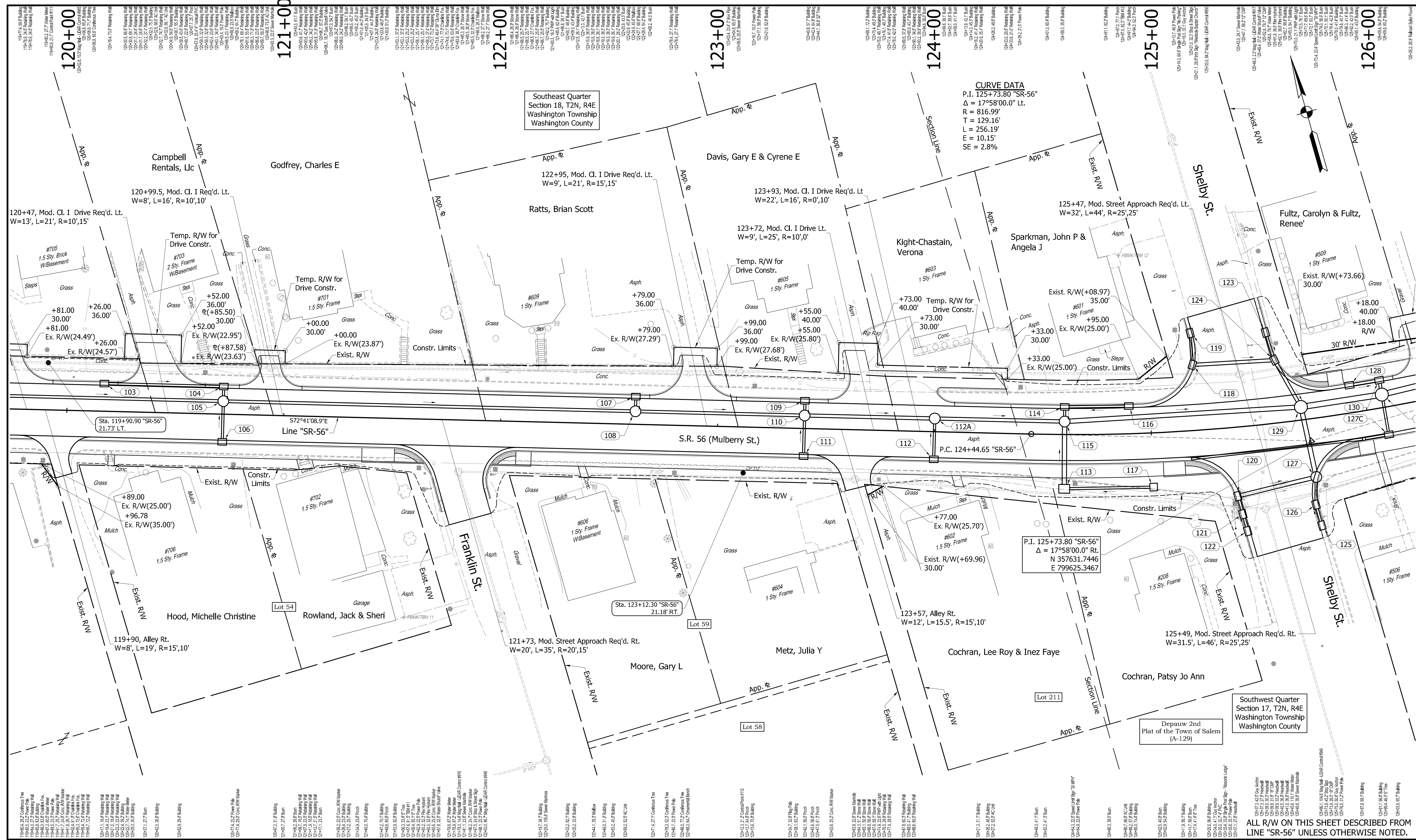


<p style="transform: rotate(-45deg); font-weight: bold;">NOT FOR CONSTRUCTION</p>	DESIGNED: JGP	DRAWN: JGP	<p style="font-weight: bold;">INDIANA DEPARTMENT OF TRANSPORTATION</p> <p style="font-weight: bold;">PROFILE S.R. 56</p>	HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
	CHECKED: GRW	CHECKED: GRW		VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
				SURVEY BOOK	SHEETS
				CONTRACT R-39905	36 of 221 PROJECT 1600873

S:\2017\Proj\2017-401-SR-56-Salem-1608873\Production Drawings\Profiles and Profiles\2017-401-SR-56-Salem-1608873\Profile-SR-56-Profile-8-2017-401-SR-56-Salem-1608873.dwg, 3/20/2015 11:05:44 AM



S:\2017\Proj\2017-401\_SR\_56\_Salem\_1600873\Production Drawings\01-Plans and Profiles\2017-401-01-Plan-Profile-SR-56 - Rev(9) 3/29/2019 11:05:07 AM



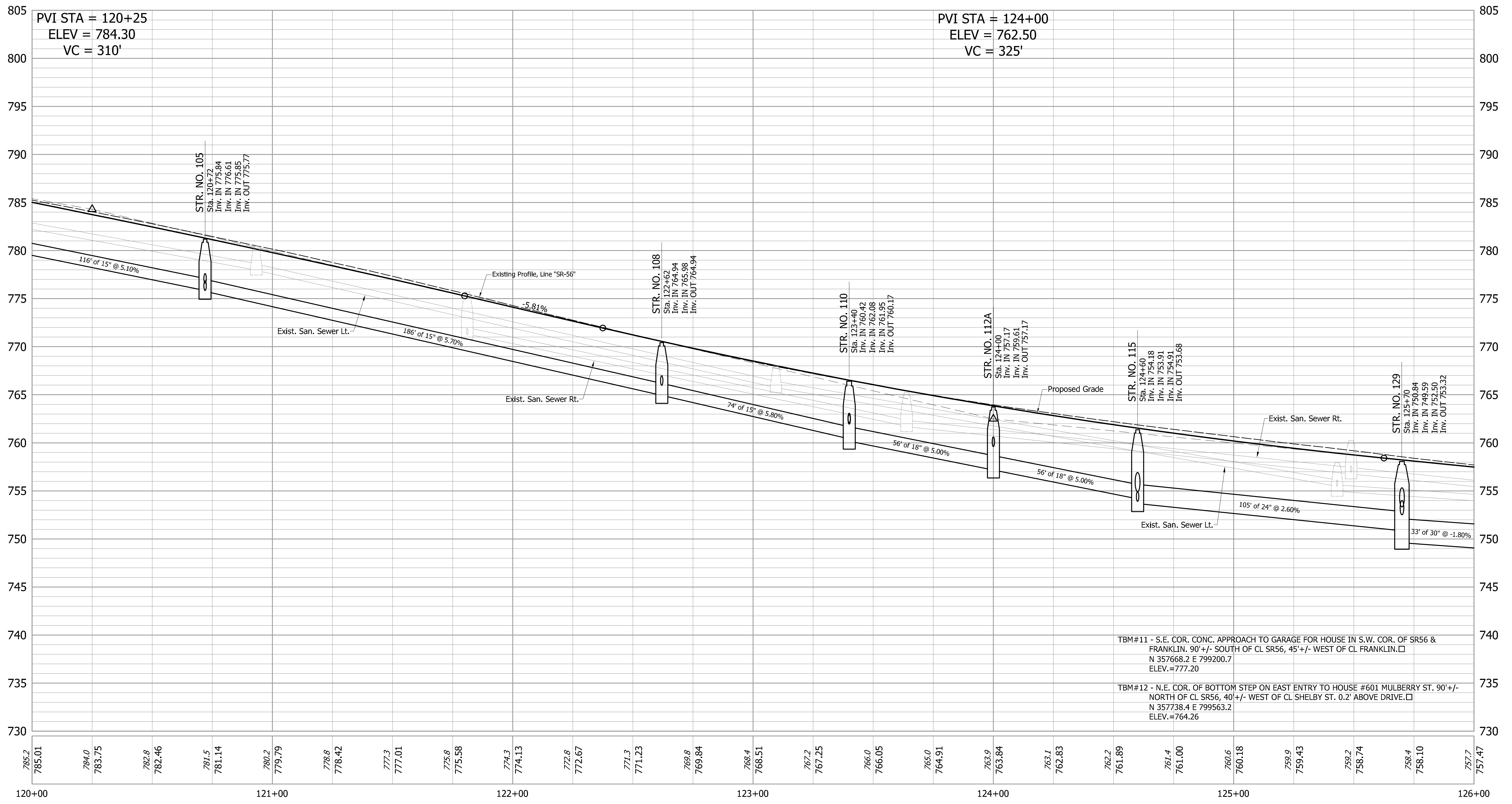
Southeast Quarter  
Section 18, T2N, R4E  
Washington Township  
Washington County

**CURVE DATA**  
P.I. 125+73.80 "SR-56"  
 $\Delta = 17^\circ 58' 00.0''$  Lt.  
R = 816.99'  
T = 129.16'  
L = 256.19'  
E = 10.15'  
SE = 2.8%

ALL R/W ON THIS SHEET DESCRIBED FROM  
LINE "SR-56" UNLESS OTHERWISE NOTED.

				<p><b>INDIANA</b> DEPARTMENT OF TRANSPORTATION</p> <p><b>PLAN</b> S.R. 56</p>		<p>HORIZONTAL SCALE 1" = 20'</p>		<p>BRIDGE FILE</p>	
<p>DESIGNED: JGP</p>		<p>DRAWN: JGP</p>				<p>VERTICAL SCALE 1" = 20'</p>		<p>DESIGNATION 1600873</p>	
<p>CHECKED: GRW</p>		<p>CHECKED: GRW</p>		<p>PLAN S.R. 56</p>		<p>SURVEY BOOK R-39905</p>		<p>SHEETS 37 of 221</p>	
						<p>CONTRACT R-39905</p>		<p>PROJECT 1600873</p>	

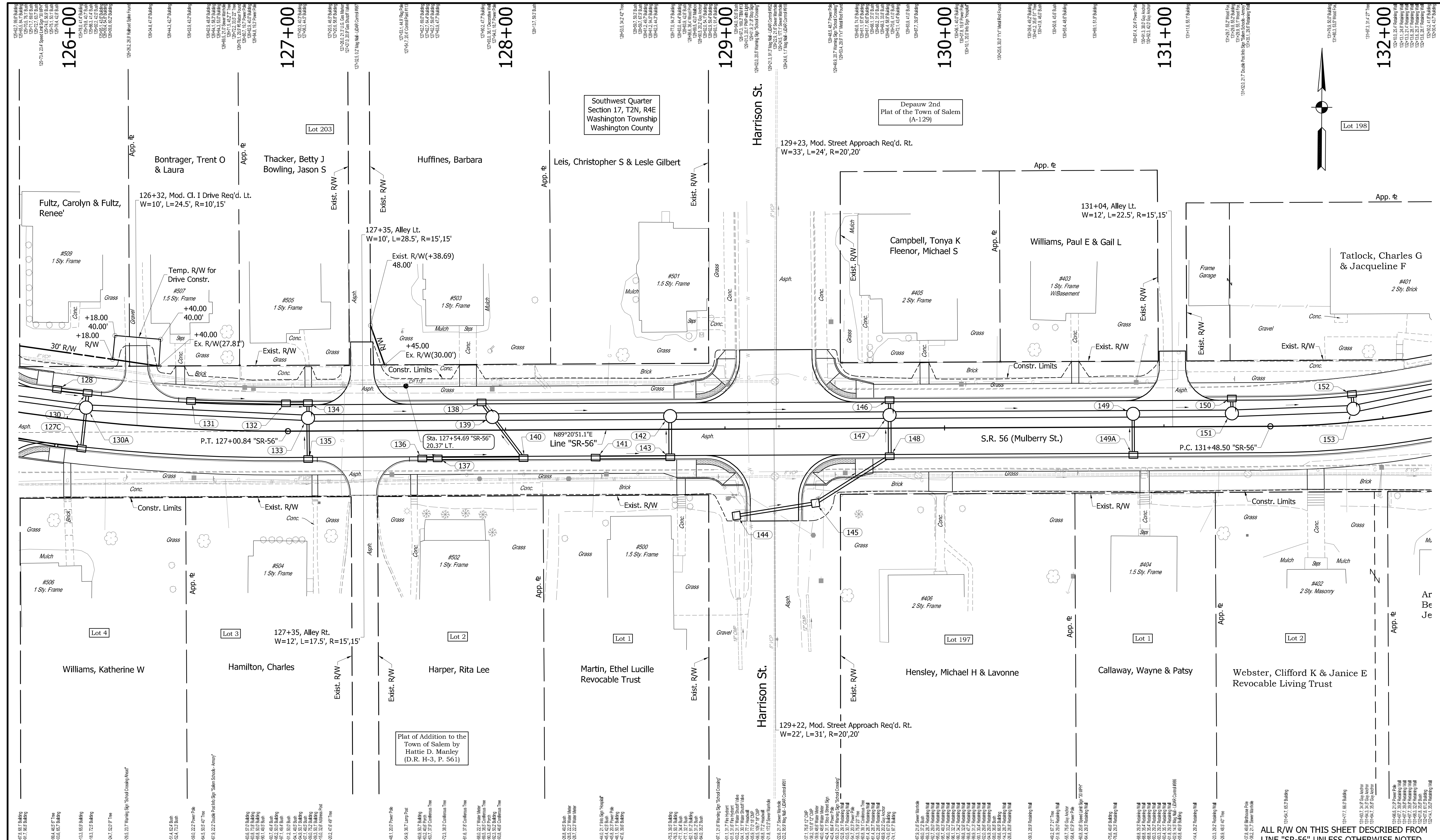
**NOT FOR CONSTRUCTION**



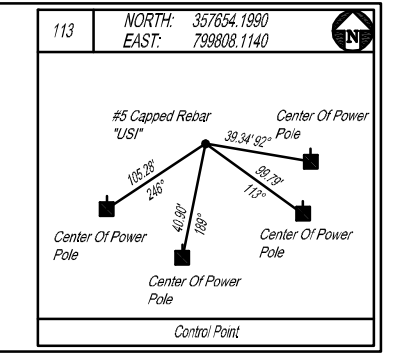
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<p style="font-size: 2em; transform: rotate(-45deg);">NOT FOR CONSTRUCTION</p>		<b>INDIANA DEPARTMENT OF TRANSPORTATION</b>		HORIZONTAL SCALE 1" = 20' VERTICAL SCALE 1" = 5'	BRIDGE FILE DESIGNATION 1600873
	DESIGNED: JGP CHECKED: GRW	DRAWN: JGP CHECKED: GRW	<b>PROFILE S.R. 56</b>		SURVEY BOOK CONTRACT R-39905

S:\2017\Proj\2017-401\_SR\_56\_Salem\_16088\Production Drawings\07-Plans and Profiles\2017-401-01-Plan-Profile-SR-56 - Plan-10\_5/29/2019 11:55:14 AM



ALL R/W ON THIS SHEET DESCRIBED FROM LINE "SR-56" UNLESS OTHERWISE NOTED.



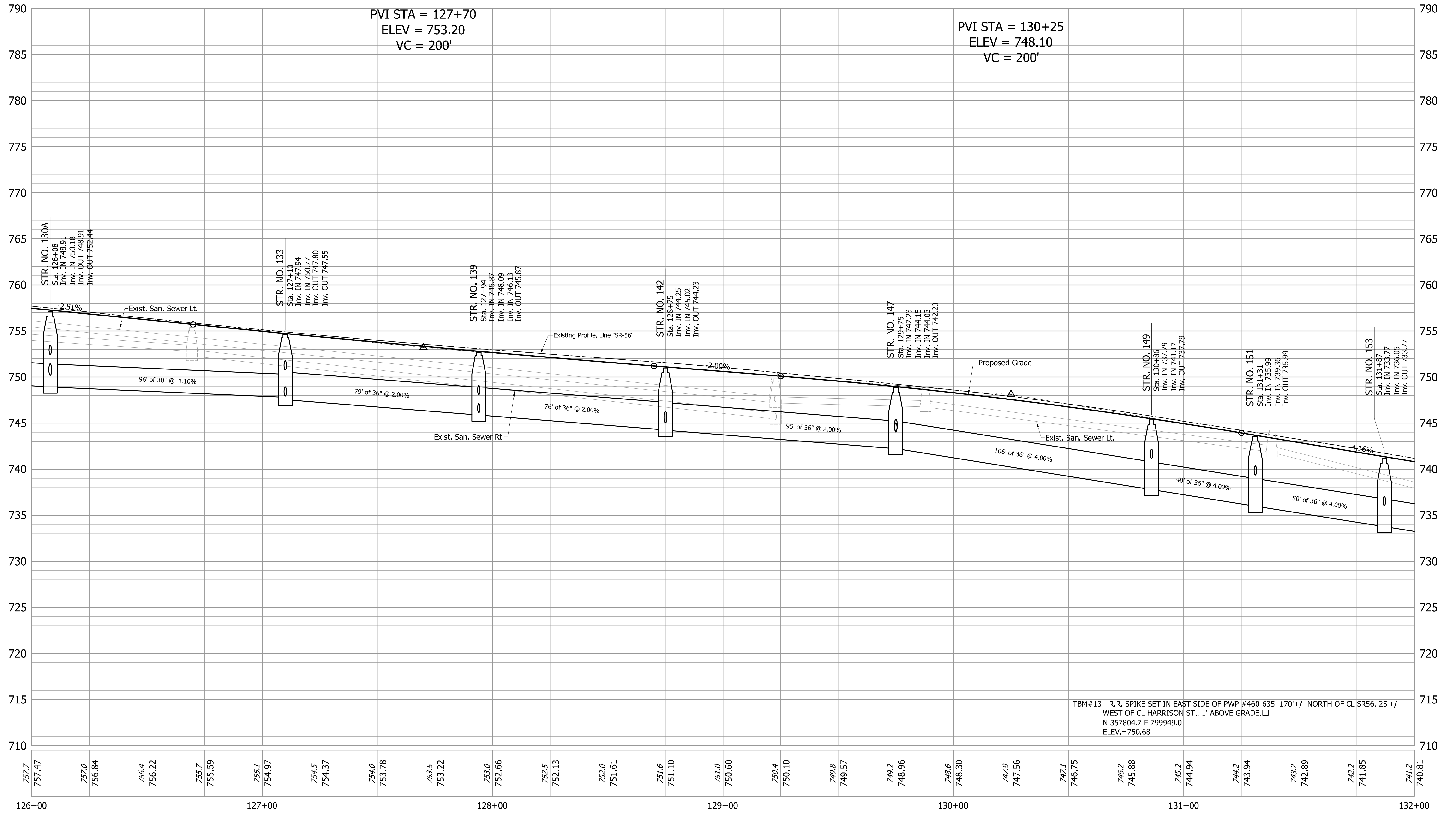
NOT FOR CONSTRUCTION

DESIGNED:	JGP	DRAWN:	JGP
CHECKED:	GRW	CHECKED:	GRW

INDIANA DEPARTMENT OF TRANSPORTATION

PLAN S.R. 56

HORIZONTAL SCALE		BRIDGE FILE	
1" = 20'			
VERTICAL SCALE		DESIGNATION	
1" = 20'		1600873	
SURVEY BOOK		SHEETS	
		39 of 221	
CONTRACT		PROJECT	
R-39905		1600873	



S:\2017Proj\2017-401 SR 56 Salem 1608873\Production Drawings\Profiles and Profiles\2017-401 SR 56 Salem 1608873\Profile-SR-56.dwg, 4/20/2017 11:05:17 AM

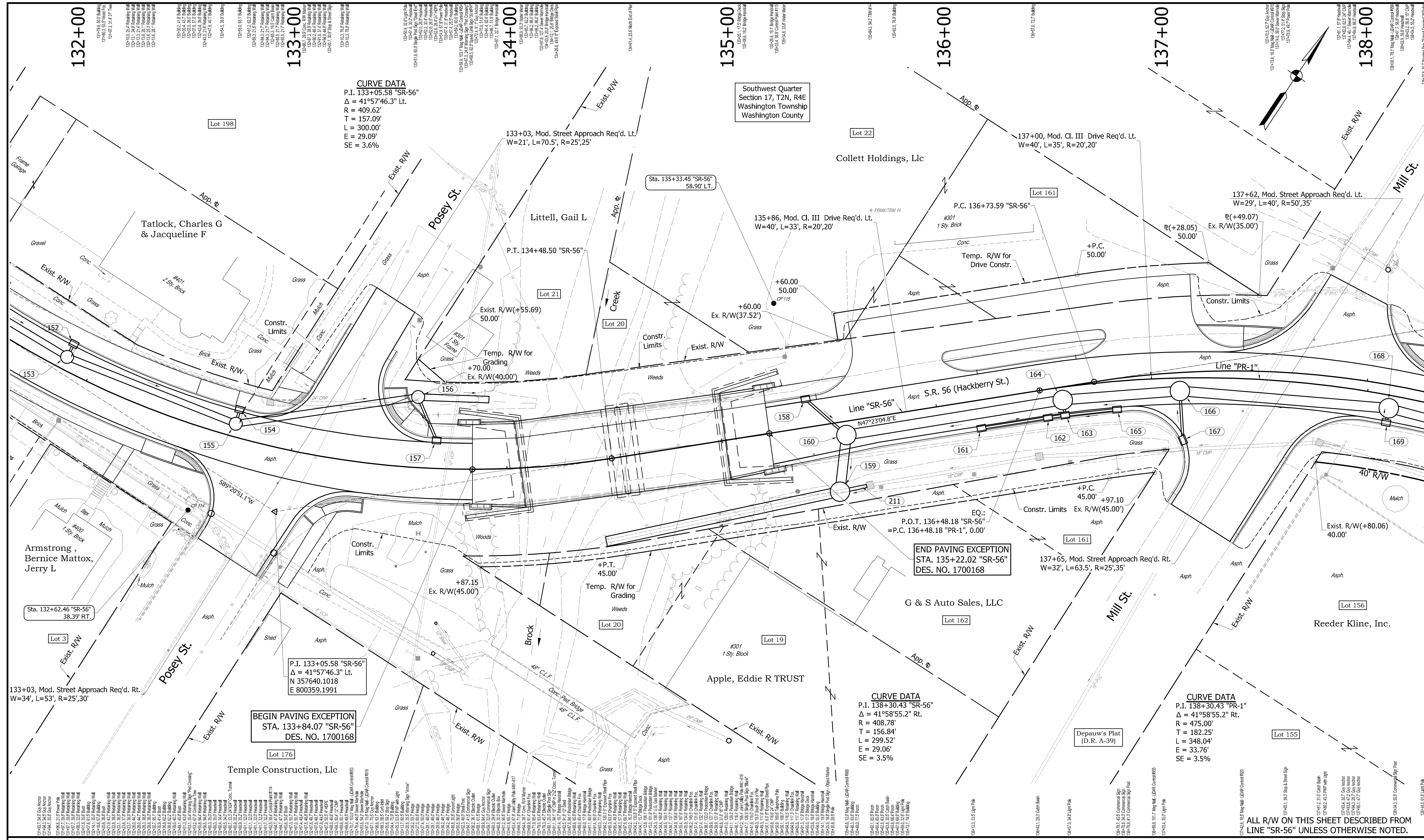
**NOT FOR CONSTRUCTION**

DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

**INDIANA DEPARTMENT OF TRANSPORTATION**

**PROFILE S.R. 56**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS
CONTRACT R-39905	40 of 221 PROJECT 1600873



**CURVE DATA**  
 P.I. 133+05.58 "SR-56"  
 $\Delta = 41^\circ 57' 46.3''$  Lt.  
 R = 409.62'  
 T = 157.09'  
 L = 300.00'  
 E = 29.09'  
 SE = 3.6%

P.I. 133+05.58 "SR-56"  
 $\Delta = 41^\circ 57' 46.3''$  Lt.  
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 E 800359.1991

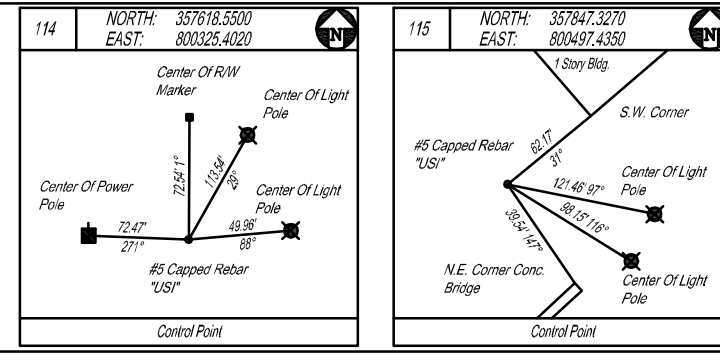
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 STA. 133+84.07 "SR-56"  
 DES. NO. 1700168

**END PAVING EXCEPTION**  
 STA. 135+22.02 "SR-56"  
 DES. NO. 1700168

**CURVE DATA**  
 P.I. 138+30.43 "SR-56"  
 $\Delta = 41^\circ 58' 55.2''$  Rt.  
 R = 408.78'  
 T = 156.84'  
 L = 299.52'  
 E = 29.06'  
 SE = 3.5%

**CURVE DATA**  
 P.I. 138+30.43 "PR-1"  
 $\Delta = 41^\circ 58' 55.2''$  Rt.  
 R = 475.00'  
 T = 182.25'  
 L = 348.04'  
 E = 33.76'  
 SE = 3.5%

ALL R/W ON THIS SHEET DESCRIBED FROM  
 LINE "SR-56" UNLESS OTHERWISE NOTED.



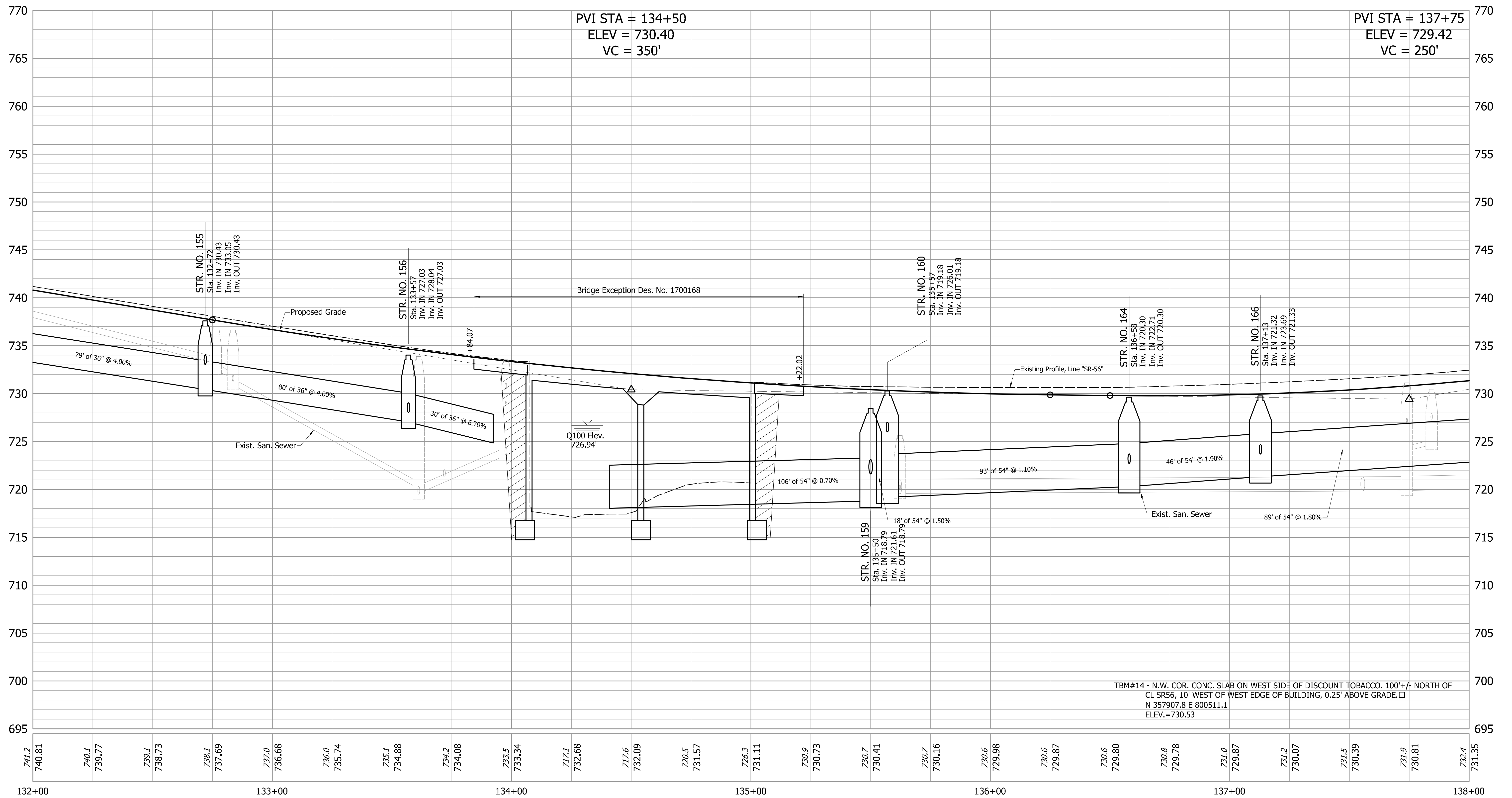
**NOT FOR CONSTRUCTION**

DESIGNED:	JGP	DRAWN:	JGP
CHECKED:	GRW	CHECKED:	GRW

**INDIANA DEPARTMENT OF TRANSPORTATION**

**PLAN S.R. 56**

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
1" = 20'	1600873
SURVEY BOOK	SHEETS
	41 of 221
CONTRACT	PROJECT
R-39905	1600873



S:\2017Proj\2017-401 SR 56 Salem 1608873\Production Drawings\Profiles and Profiles\2017-401 SR 56 Salem 1608873\Profile-11\_S2920105\_110523.dwg, © 2017-401 SR 56 Salem 1608873\Production Drawings\Profiles and Profiles\2017-401 SR 56 Salem 1608873\Profile-11\_S2920105\_110523.dwg

**NOT FOR CONSTRUCTION**

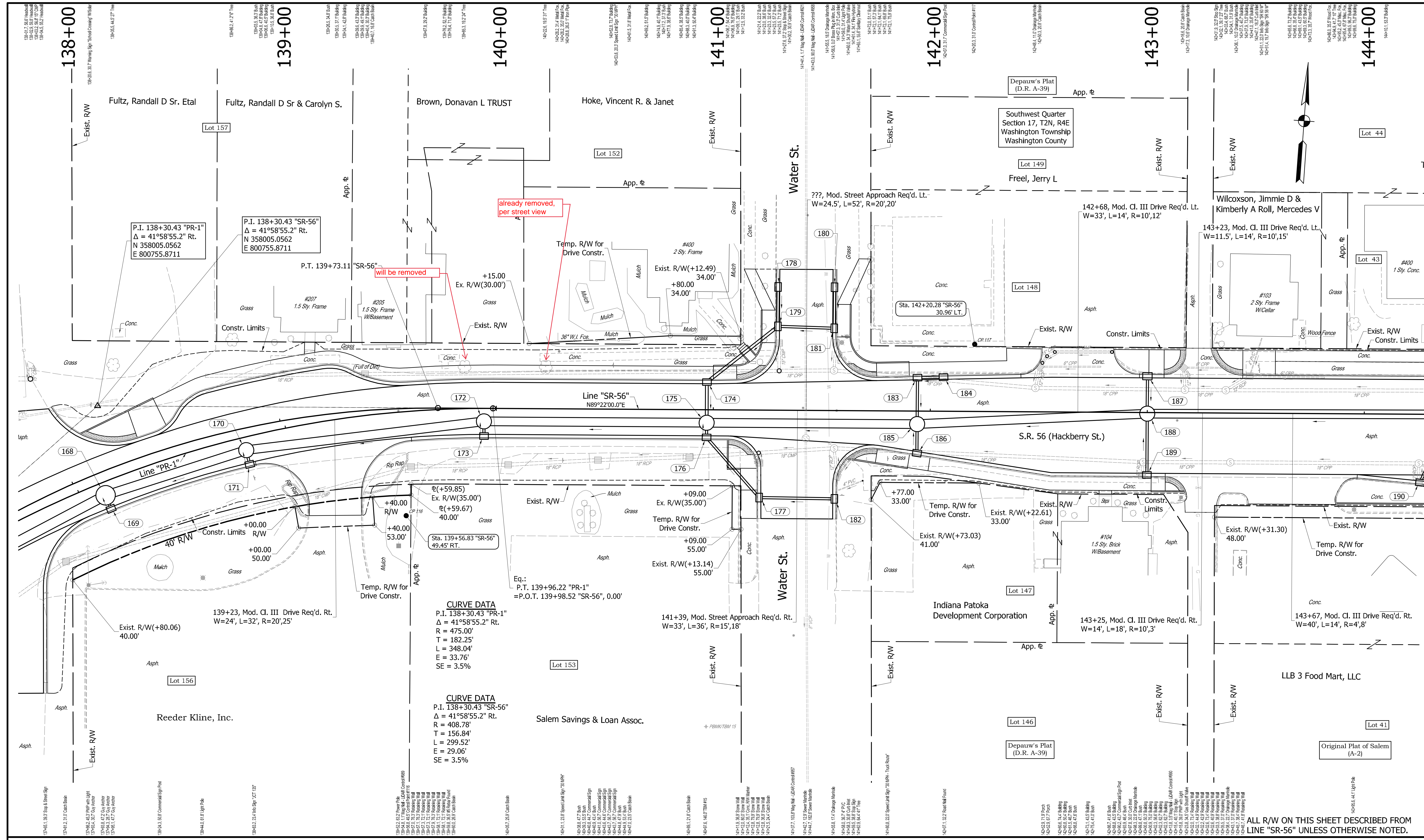
DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

**INDIANA DEPARTMENT OF TRANSPORTATION**

**PROFILE**  
**S.R. 56**

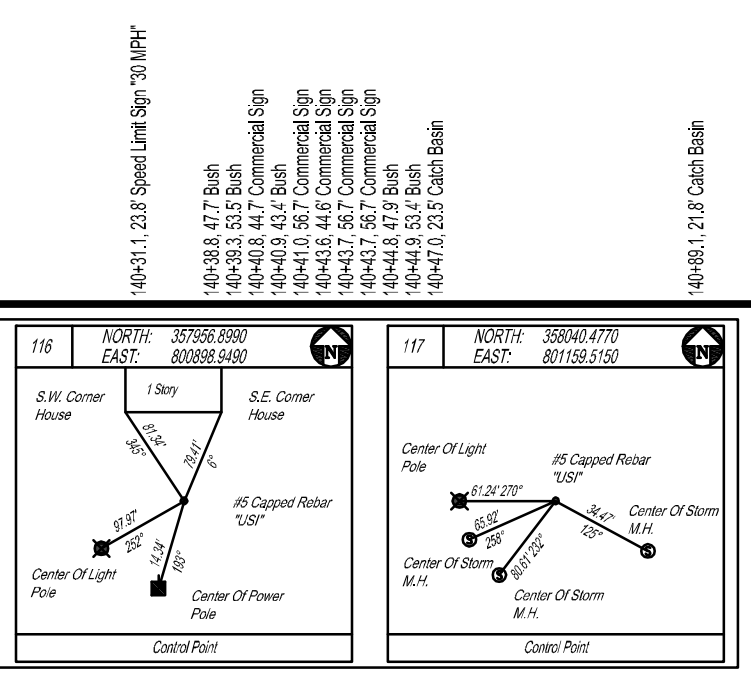
HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS
	42 of 221
CONTRACT R-39905	PROJECT 1600873

S:\2017Proj\2017-401 SR 56 Salem 160883\Production Drawings\Plan and Profile\2017-401-SR-56-Salem-160883-SR-56 - Item 12 - 3/29/2019 11:55:26 AM



**CURVE DATA**  
 P.I. 138+30.43 "PR-1"  
 Δ = 41°58'55.2" Rt.  
 R = 475.00'  
 T = 182.25'  
 L = 348.04'  
 E = 33.76'  
 SE = 3.5%

**CURVE DATA**  
 P.I. 138+30.43 "SR-56"  
 Δ = 41°58'55.2" Rt.  
 R = 408.78'  
 T = 156.84'  
 L = 299.52'  
 E = 29.06'  
 SE = 3.5%



**NOT FOR CONSTRUCTION**

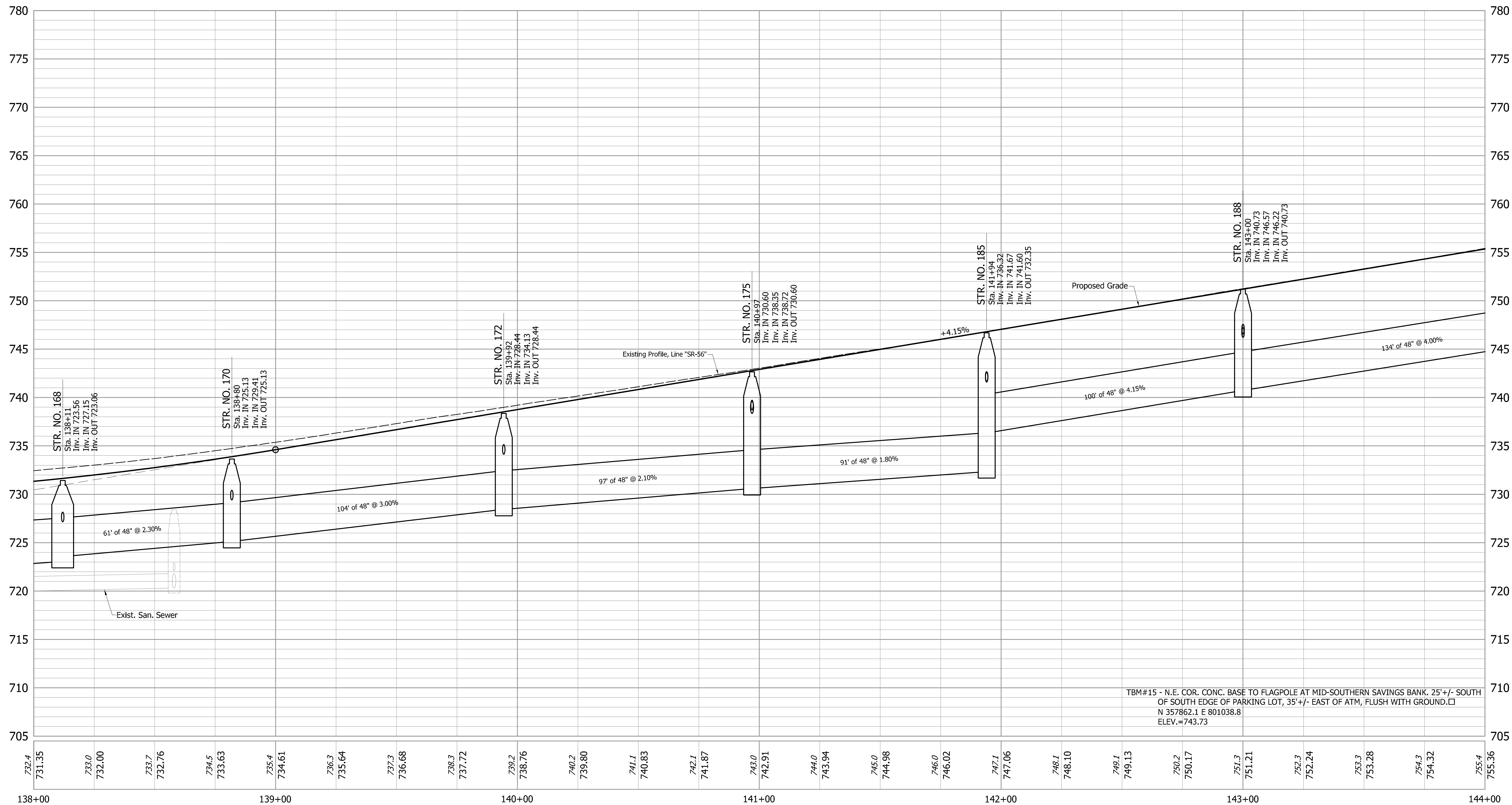
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CHECKED: GRW	CHECKED: GRW

**INDIANA DEPARTMENT OF TRANSPORTATION**

**PLAN S.R. 56**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 20'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 43 of 221
CONTRACT R-39905	PROJECT 1600873

ALL R/W ON THIS SHEET DESCRIBED FROM LINE "SR-56" UNLESS OTHERWISE NOTED.



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<p><b>NOT FOR CONSTRUCTION</b></p>	
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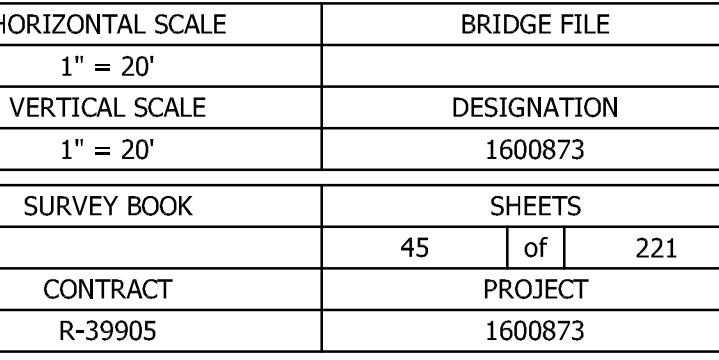
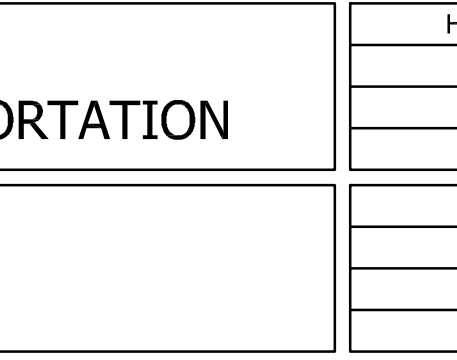
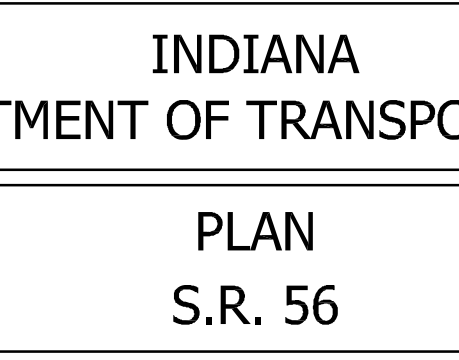
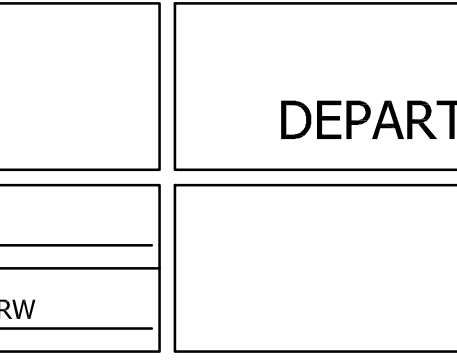
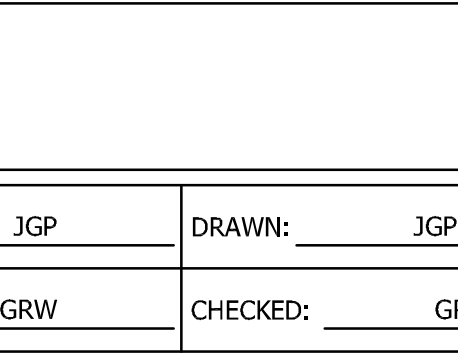
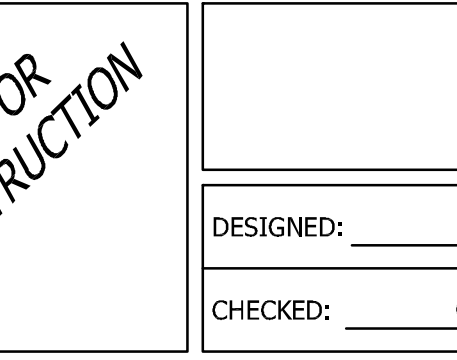
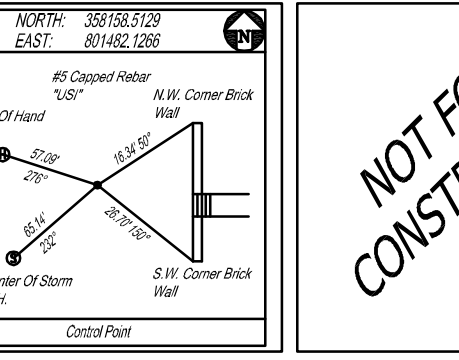
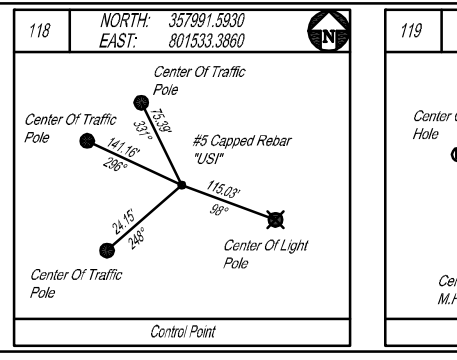
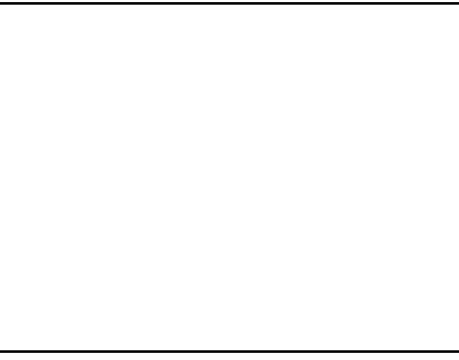
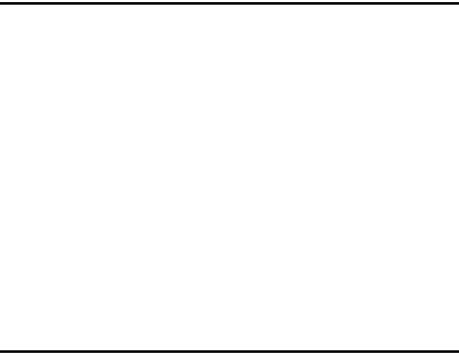
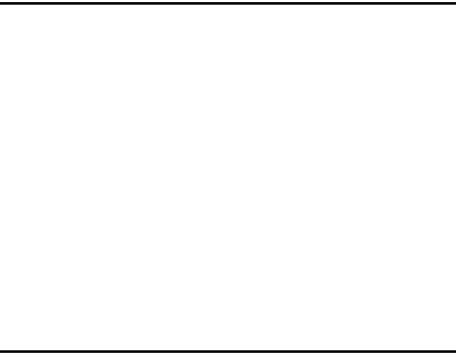
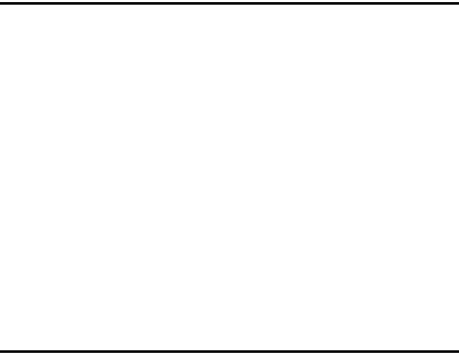
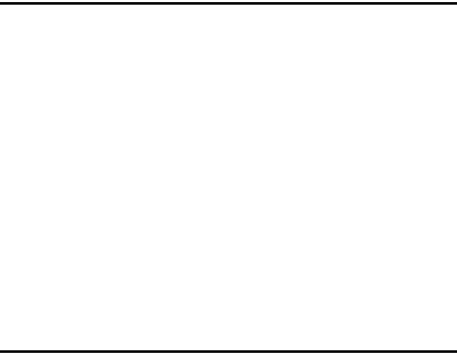
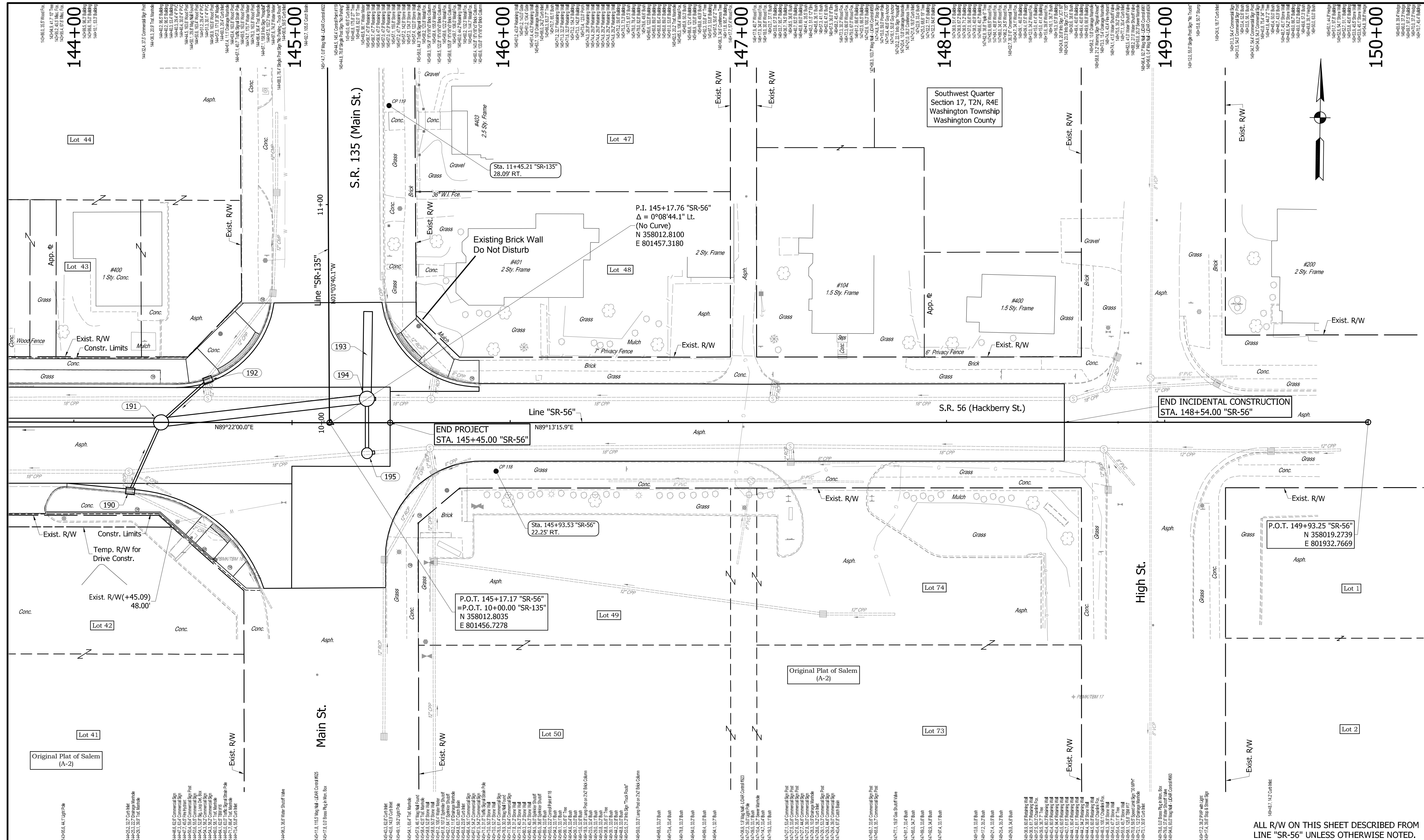
DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

INDIANA  
DEPARTMENT OF TRANSPORTATION

PROFILE  
S.R. 56

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS
	44 of 221
CONTRACT R-39905	PROJECT 1600873





NOT FOR CONSTRUCTION

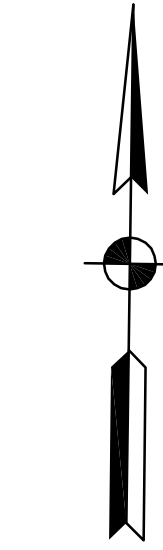
INDIANA DEPARTMENT OF TRANSPORTATION

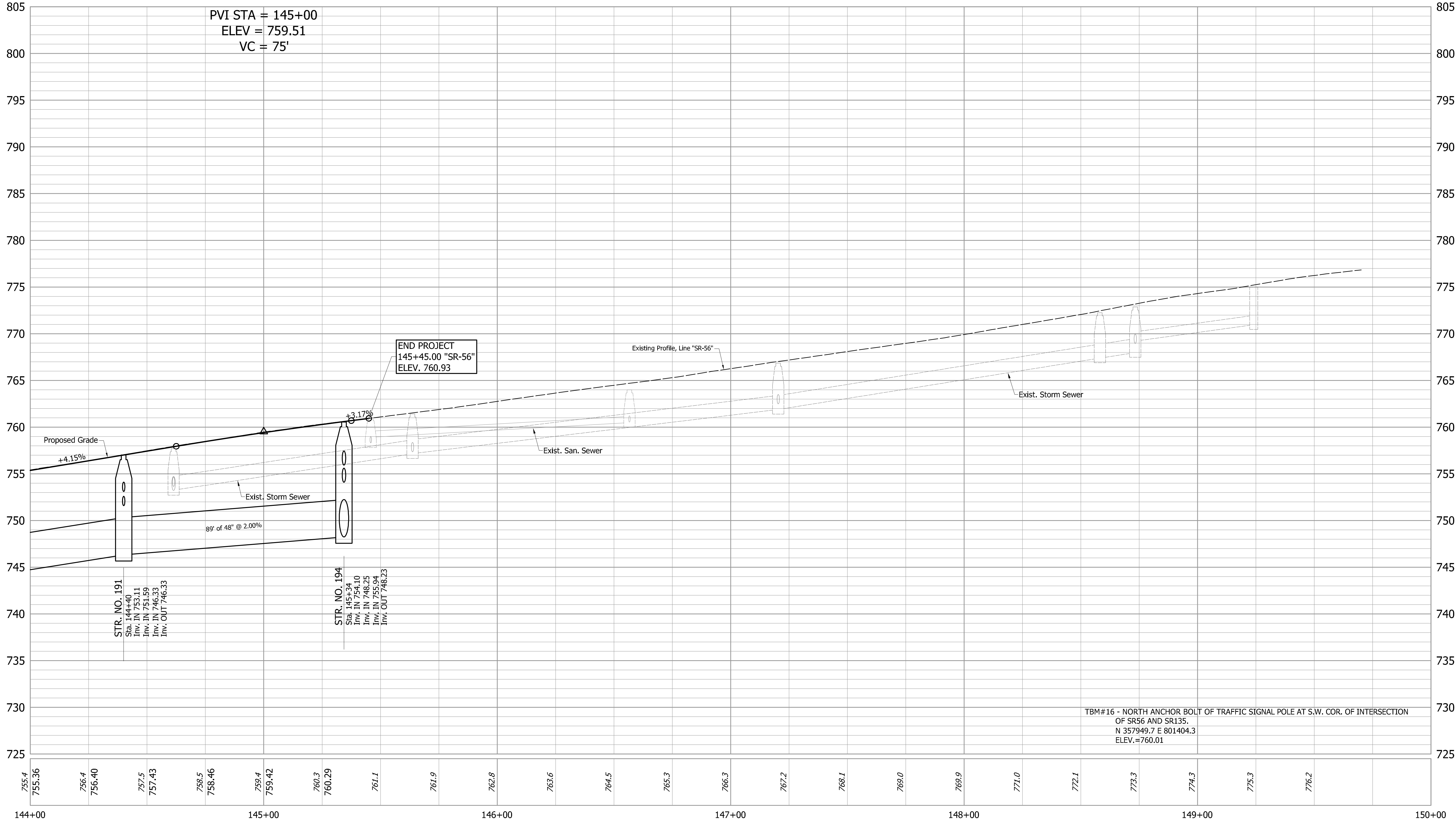
PLAN S.R. 56

DESIGNED: JGP DRAWN: JGP  
 CHECKED: GRW CHECKED: GRW

HORIZONTAL SCALE		BRIDGE FILE	
1" = 20'			
VERTICAL SCALE		DESIGNATION	
1" = 20'		1600873	
SURVEY BOOK		SHEETS	
		45 of 221	
CONTRACT		PROJECT	
R-39905		1600873	

ALL R/W ON THIS SHEET DESCRIBED FROM LINE "SR-56" UNLESS OTHERWISE NOTED.





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<b>NOT FOR CONSTRUCTION</b>	
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DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PROFILE  
S.R. 56**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 46 of 221
CONTRACT R-39905	PROJECT 1600873

Temporary and Permanent  
Right-of-Way Locations. Please  
refer to pages B-118 to B-120  
for land use and amounts.

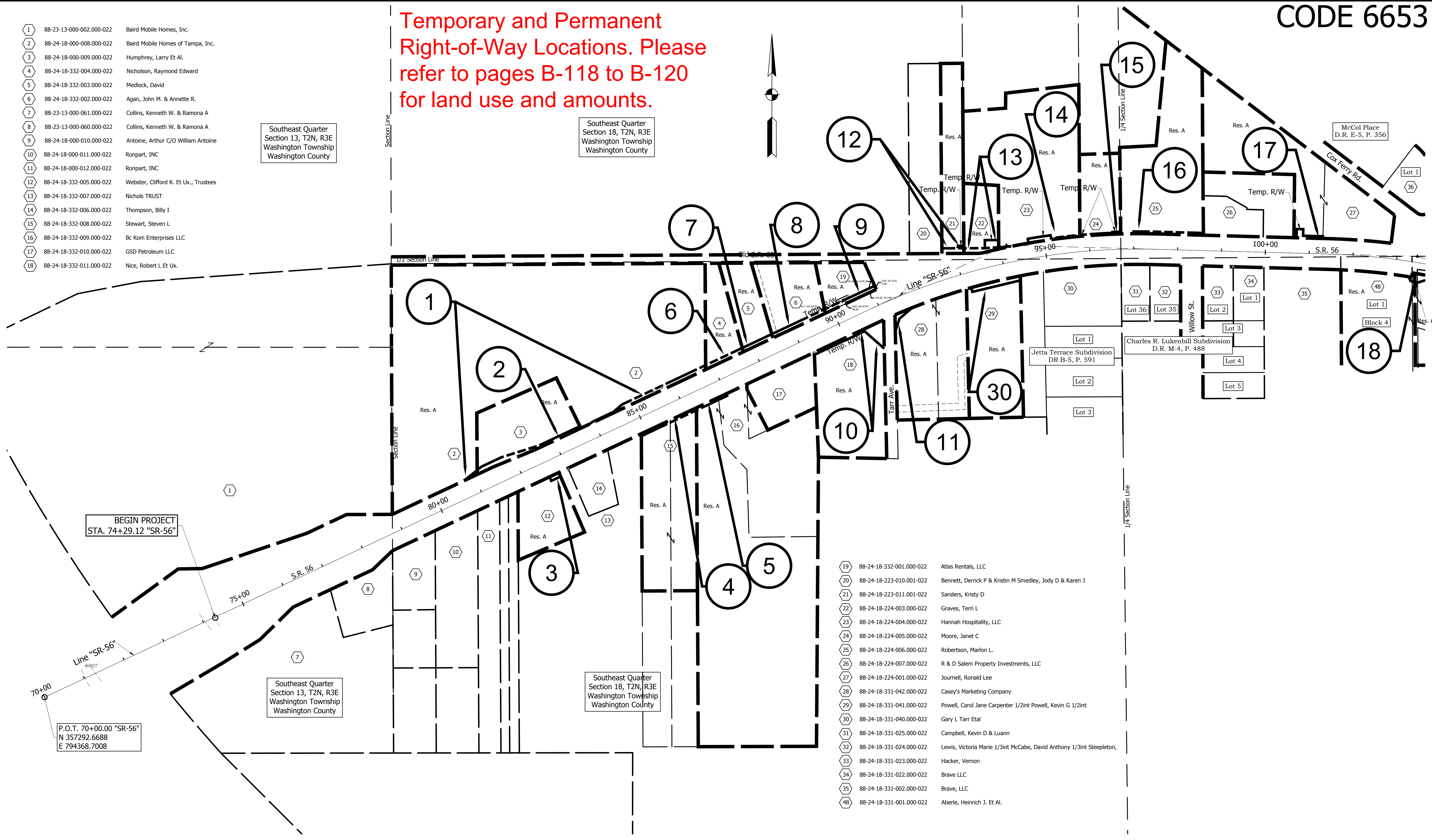
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- 2 88-24-18-000-008.000-022 Baird Mobile Homes of Tampa, Inc.
- 3 88-24-18-000-009.000-022 Humphrey, Larry Et Al.
- 4 88-24-18-332-004.000-022 Nicholson, Raymond Edward
- 5 88-24-18-332-003.000-022 Medlock, David
- 6 88-24-18-332-002.000-022 Agan, John M. & Annette R.
- 7 88-23-13-000-061.000-022 Collins, Kenneth W. & Ramona A
- 8 88-23-13-000-060.000-022 Collins, Kenneth W. & Ramona A
- 9 88-24-18-000-010.000-022 Antoine, Arthur C/O William Antoine
- 10 88-24-18-000-011.000-022 Ronpart, INC
- 11 88-24-18-000-012.000-022 Ronpart, INC
- 12 88-24-18-332-005.000-022 Webster, Clifford K. Et Ux., Trustees
- 13 88-24-18-332-007.000-022 Nichols TRUST
- 14 88-24-18-332-006.000-022 Thompson, Billy I
- 15 88-24-18-332-008.000-022 Stewart, Steven L
- 16 88-24-18-332-009.000-022 Bc Kom Enterprises LLC
- 17 88-24-18-332-010.000-022 GSD Petroleum LLC
- 18 88-24-18-332-011.000-022 Nice, Robert L Et Ux.

Southeast Quarter  
Section 13, T2N, R3E  
Washington Township  
Washington County

Southeast Quarter  
Section 18, T2N, R3E  
Washington Township  
Washington County

Southeast Quarter  
Section 13, T2N, R3E  
Washington Township  
Washington County

Southeast Quarter  
Section 18, T2N, R3E  
Washington Township  
Washington County



- 19 88-24-18-332-001.000-022 Atlas Rentals, LLC
- 20 88-24-18-223-010.001-022 Bennett, Derrick P & Kristin M Smedley, Jody D & Karen J
- 21 88-24-18-223-011.001-022 Sanders, Kristy D
- 22 88-24-18-224-003.000-022 Graves, Terri L
- 23 88-24-18-224-004.000-022 Hannah Hospitality, LLC
- 24 88-24-18-224-005.000-022 Moore, Janet C
- 25 88-24-18-224-006.000-022 Robertson, Marlon L.
- 26 88-24-18-224-007.000-022 R & D Salem Property Investments, LLC
- 27 88-24-18-224-001.000-022 Journell, Ronald Lee
- 28 88-24-18-331-042.000-022 Casey's Marketing Company
- 29 88-24-18-331-041.000-022 Powell, Carol Jane Carpenter 1/2int Powell, Kevin G 1/2int
- 30 88-24-18-331-040.000-022 Gary L Tarr Etal
- 31 88-24-18-331-025.000-022 Campbell, Kevin D & Luann
- 32 88-24-18-331-024.000-022 Lewis, Victoria Marie 1/3int McCabe, David Anthony 1/3int Steepleton,
- 33 88-24-18-331-023.000-022 Hacker, Vernon
- 34 88-24-18-331-022.000-022 Brave LLC
- 35 88-24-18-331-002.000-022 Brave, LLC
- 48 88-24-18-331-001.000-022 Aberle, Heinrich J. Et Al.

BEGIN PROJECT  
STA. 74+29.12 "SR-56"

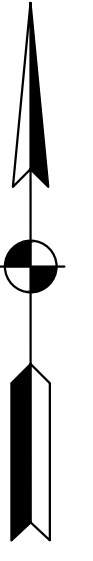
P.O.T. 70+00.00 "SR-56"  
N 357292.6688  
E 794368.7008

NOT FOR  
CONSTRUCTION

DESIGNED: BDC DRAWN: BDC  
CHECKED: GRW CHECKED: GRW

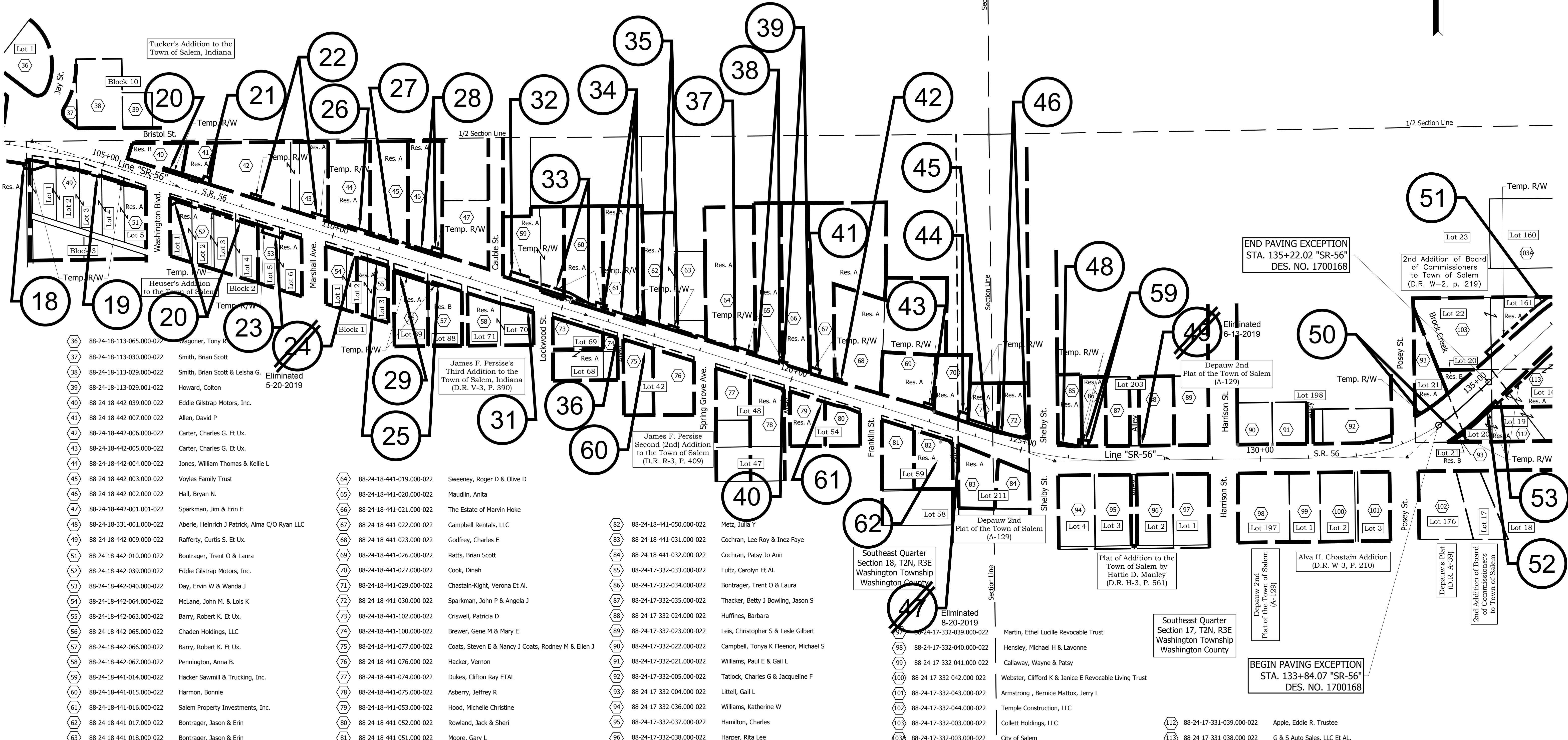
INDIANA  
DEPARTMENT OF TRANSPORTATION  
  
PLAT NO. 1  
S.R. 56

HORIZONTAL SCALE 1" = 100'	BRIDGE FILE
VERTICAL SCALE 1" = 100'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 8 of 89
CONTRACT R-39905	PROJECT 1600873



Southeast Quarter  
Section 18, T2N, R3E  
Washington Township  
Washington County

Southeast Quarter  
Section 17, T2N, R3E  
Washington Township  
Washington County



- 36 88-24-18-113-065.000-022 Wagoner, Tony R
- 37 88-24-18-113-030.000-022 Smith, Brian Scott
- 38 88-24-18-113-029.000-022 Smith, Brian Scott & Leisha G. Eliminated 5-20-2019
- 39 88-24-18-113-029.001-022 Howard, Colton
- 40 88-24-18-442-039.000-022 Eddie Gilstrap Motors, Inc.
- 41 88-24-18-442-007.000-022 Allen, David P
- 42 88-24-18-442-006.000-022 Carter, Charles G. Et Ux.
- 43 88-24-18-442-005.000-022 Carter, Charles G. Et Ux.
- 44 88-24-18-442-004.000-022 Jones, William Thomas & Kellie L
- 45 88-24-18-442-003.000-022 Voyles Family Trust
- 46 88-24-18-442-002.000-022 Hall, Bryan N.
- 47 88-24-18-442-001.001-022 Sparkman, Jim & Erin E
- 48 88-24-18-331-001.000-022 Aberle, Heinrich J Patrick, Alma C/O Ryan LLC
- 49 88-24-18-442-009.000-022 Rafferty, Curtis S. Et Ux.
- 51 88-24-18-442-010.000-022 Bontrager, Trent O & Laura
- 52 88-24-18-442-039.000-022 Eddie Gilstrap Motors, Inc.
- 53 88-24-18-442-040.000-022 Day, Ervin W & Wanda J
- 54 88-24-18-442-064.000-022 McLane, John M. & Lois K
- 55 88-24-18-442-063.000-022 Barry, Robert K. Et Ux.
- 56 88-24-18-442-065.000-022 Chaden Holdings, LLC
- 57 88-24-18-442-066.000-022 Barry, Robert K. Et Ux.
- 58 88-24-18-442-067.000-022 Pennington, Anna B.
- 59 88-24-18-441-014.000-022 Hacker Sawmill & Trucking, Inc.
- 60 88-24-18-441-015.000-022 Harmon, Bonnie
- 61 88-24-18-441-016.000-022 Salem Property Investments, Inc.
- 62 88-24-18-441-017.000-022 Bontrager, Jason & Erin
- 63 88-24-18-441-018.000-022 Bontrager, Jason & Erin

- 64 88-24-18-441-019.000-022 Sweeney, Roger D & Olive D
- 65 88-24-18-441-020.000-022 Maudlin, Anita
- 66 88-24-18-441-021.000-022 The Estate of Marvin Hoke
- 67 88-24-18-441-022.000-022 Campbell Rentals, LLC
- 68 88-24-18-441-023.000-022 Godfrey, Charles E
- 69 88-24-18-441-026.000-022 Ratts, Brian Scott
- 70 88-24-18-441-027.000-022 Cook, Dinah
- 71 88-24-18-441-029.000-022 Chastain-Kight, Verona Et Al.
- 72 88-24-18-441-030.000-022 Sparkman, John P & Angela J
- 73 88-24-18-441-102.000-022 Criswell, Patricia D
- 74 88-24-18-441-100.000-022 Brewer, Gene M & Mary E
- 75 88-24-18-441-077.000-022 Coats, Steven E & Nancy J Coats, Rodney M & Ellen J
- 76 88-24-18-441-076.000-022 Hacker, Vernon
- 77 88-24-18-441-074.000-022 Dukes, Clifton Ray ETAL
- 78 88-24-18-441-075.000-022 Asberry, Jeffrey R
- 79 88-24-18-441-053.000-022 Hood, Michelle Christine
- 80 88-24-18-441-052.000-022 Rowland, Jack & Sheri
- 81 88-24-18-441-051.000-022 Moore, Gary L

- 82 88-24-18-441-050.000-022 Metz, Julia Y
- 83 88-24-18-441-031.000-022 Cochran, Lee Roy & Inez Faye
- 84 88-24-18-441-032.000-022 Cochran, Patsy Jo Ann
- 85 88-24-17-332-033.000-022 Fultz, Carolyn Et Al.
- 86 88-24-17-332-034.000-022 Bontrager, Trent O & Laura
- 87 88-24-17-332-035.000-022 Thacker, Betty J Bowling, Jason S
- 88 88-24-17-332-024.000-022 Huffines, Barbara
- 89 88-24-17-332-023.000-022 Leis, Christopher S & Leslie Gilbert
- 90 88-24-17-332-022.000-022 Campbell, Tonya K Fleenor, Michael S
- 91 88-24-17-332-021.000-022 Williams, Paul E & Gail L
- 92 88-24-17-332-005.000-022 Tatlock, Charles G & Jacqueline F
- 93 88-24-17-332-004.000-022 Littell, Gail L
- 94 88-24-17-332-036.000-022 Williams, Katherine W
- 95 88-24-17-332-037.000-022 Hamilton, Charles
- 96 88-24-17-332-038.000-022 Harper, Rita Lee

- 97 88-24-17-332-039.000-022 Martin, Ethel Lucille Revocable Trust
- 98 88-24-17-332-040.000-022 Hensley, Michael H & Lavonne
- 99 88-24-17-332-041.000-022 Callaway, Wayne & Patsy
- 100 88-24-17-332-042.000-022 Webster, Clifford K & Janice E Revocable Living Trust
- 101 88-24-17-332-043.000-022 Armstrong, Bernice Mattox, Jerry L
- 102 88-24-17-332-044.000-022 Temple Construction, LLC
- 103 88-24-17-332-003.000-022 Collett Holdings, LLC
- 104 88-24-17-332-003.000-022 City of Salem

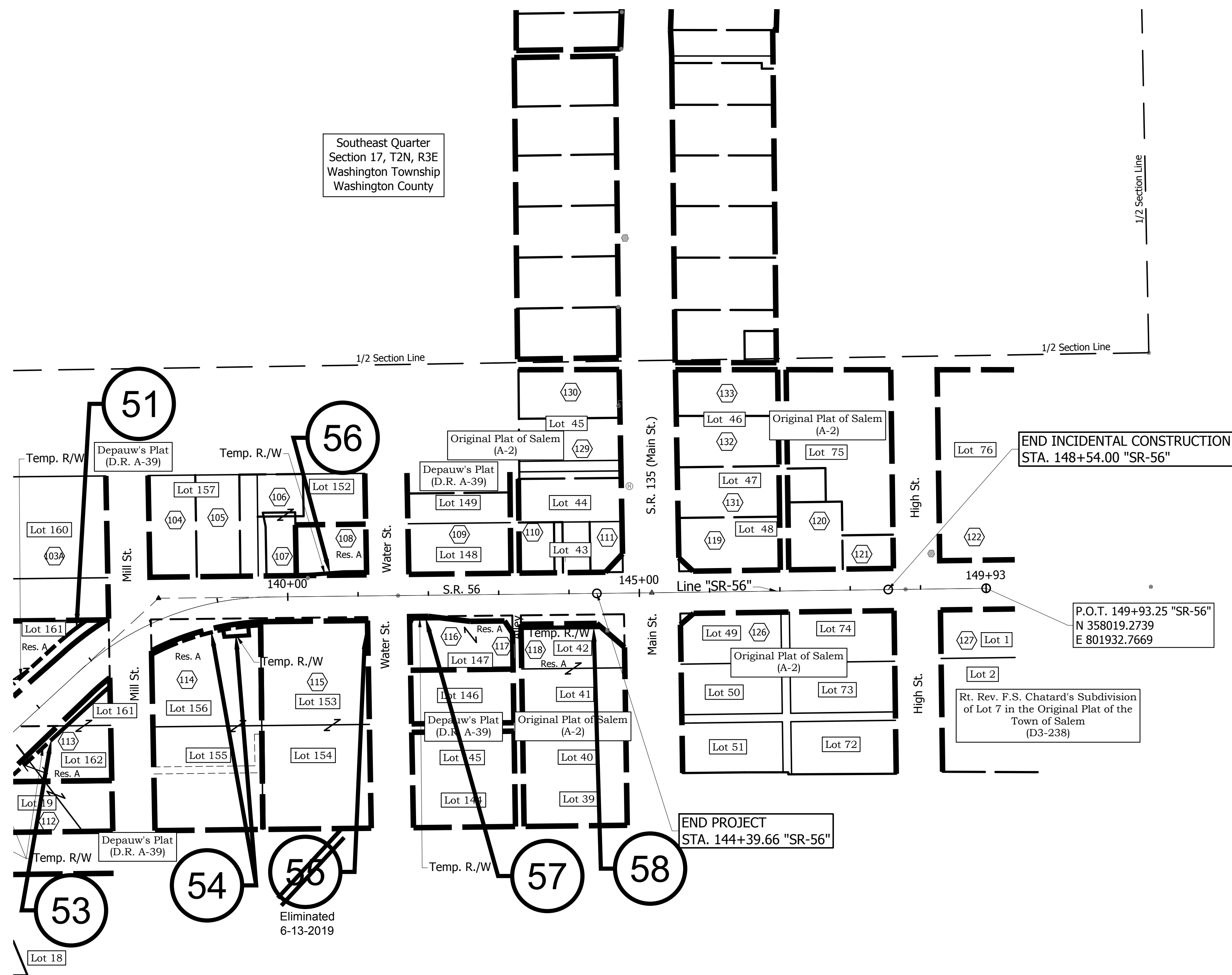
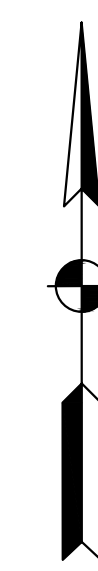
- 105 88-24-17-331-039.000-022 Apple, Eddie R. Trustee
- 106 88-24-17-331-038.000-022 G & S Auto Sales, LLC Et AL

NOT FOR CONSTRUCTION

DESIGNED: BDC DRAWN: BDC  
CHECKED: GRW CHECKED: GRW

INDIANA DEPARTMENT OF TRANSPORTATION  
PLAT NO. 1  
S.R. 56

HORIZONTAL SCALE 1" = 100'	BRIDGE FILE
VERTICAL SCALE 1" = 100'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 9 of 89
CONTRACT R-39905	PROJECT 1600873



- (034) 88-24-17-332-003.000-022 City of Salem
- (104) 88-24-17-331-035.000-022 Fultz, Randall D Sr. Etal
- (105) 88-24-17-331-034.000-022 Fultz, Randall D Sr & Carolyn S.
- (106) 88-24-17-331-033.000-022 Brown, Donovan L TRUST
- (107) 88-24-17-331-032.000-022 Brown, Donovan L TRUST
- (108) 88-24-17-331-031.000-022 Hoke, Vincent R. & Janet
- (109) 88-24-17-331-023.000-022 Freel, Jerry L
- (110) 88-24-17-331-022.000-022 Wilcoxson, Jimmie D & Kimberly A Roll, Mercedes V
- (111) 88-24-17-331-021.000-022 TripleNetZeroDebt. Com, LLC
- (112) 88-24-17-331-039.000-022 Apple, Eddie R TRUST
- (113) 88-24-17-331-038.000-022 G & S Auto Sales, LLC Et Al.
- (114) 88-24-17-331-040.000-022 Reeder & Kline, Inc. Et Al.
- (115) 88-24-17-331-041.000-022 Salem Savings & Loan Assoc.
- (116) 88-24-17-331-042.000-022 Indiana-Patoka Development Corporation
- (117) 88-24-17-331-043.000-022 Indiana-Patoka Development Corporation
- (118) 88-24-17-331-050.000-022 LLB 3 Food Mart, LLC Et Al.
- (119) 88-24-17-331-015.000-022 Morris, Ginger 1/3int Morris, Christopher Lane 1/3int Brannen,
- (120) 88-24-17-331-014.000-022 Troutman, Sarah P
- (121) 88-24-17-331-013.000-022 Tompkins, Evelyn Kay 1/100int Gibson, Kathy Jo 99/100int
- (122) 88-24-17-331-006.000-022 Grassmyer, Richard M & Marcia L
- (127) 88-24-17-331-064.000-022 Church Of Christ (Trustees)
- (129) 88-24-17-331-020.000-022 Wolf, Nathan V & Angela B
- (130) 88-24-17-331-019.000-022 Burton, Bennie B. & Bonnie L.
- (131) 88-24-17-331-016.000-022 Hendricks, Jerry L.
- (132) 88-24-17-331-017.000-022 Hendricks, Jerry L.
- (133) 88-24-17-331-018.000-022 Smedley, Robin L.

Southeast Quarter  
Section 17, T2N, R3E  
Washington Township  
Washington County

Southeast Quarter  
Section 17, T2N, R3E  
Washington Township  
Washington County

NOT FOR  
CONSTRUCTION

DESIGNED: _____ BDC	DRAWN: _____ BDC
CHECKED: _____ GRW	CHECKED: _____ GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PLAT NO. 1  
S.R. 56**

HORIZONTAL SCALE 1" = 100'	BRIDGE FILE
VERTICAL SCALE 1" = 100'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 10 of 89
CONTRACT R-39905	PROJECT 1600873

# USI Consultants, Inc.

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## Parcel Area Report

**Project Name:** S:\2017Proj\2017-401 SR 56 Salem  
1600873\ROW\DWG\2017-401-rw base.dwg

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Parcel Name	Square Feet	Acres (Gross Fee)	Acres (Temp)
1 FEE Residential	149	0.003	
1A FEE Residential	2,127	0.049	
2 FEE Residential	2,113	0.049	
3 TEMP DRIVE Residential	235		0.005
4 FEE Residential	694	0.016	
5 FEE Residential	380	0.009	
6 FEE Residential	843	0.019	
7 FEE Residential	669	0.015	
8 TEMP DRIVE Residential	1,192		0.027
9 TEMP DRIVE Residential	1,144		0.026
10 FEE PER Residential	4,435	0.102	
10A TEMP DRIVE Residential	621		0.014
11 FEE Residential	454	0.01	
12 FEE PER Residential	1,300	0.03	
12A TEMP DRIVE Residential	74		0.002
13 FEE PER Residential	1,029	0.024	
13A TEMP DRIVE Residential	548		0.013
14 FEE PER Residential	8,391	0.193	
14A TEMP DRIVE Residential	722		0.017
15 TEMP DRIVE Residential	295		0.007
16 TEMP DRIVE Residential	795		0.018
17 TEMP DRIVE GRADE Residential	525		0.012
18 TEMP DRIVE Residential	170		0.004
19 TEMP DRIVE GRADE Commercial	1,186		0.027
20 FEE PER Commercial	1,676	0.038	
20A TEMP DRIVE Commercial	823		0.019
20B TEMP DRIVE Commercial	201		0.005
21 TEMP DRIVE Commercial	442		0.01
22 TEMP DRIVE Commercial	160		0.004
22A TEMP DRIVE Commercial	285		0.007

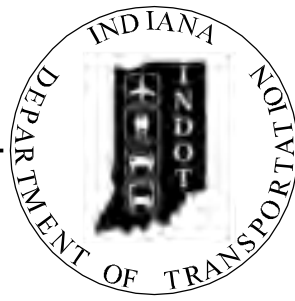
Parcel Name	Square Feet	Acres (Gross Fee)	Acres (Temp)
23 TEMP DRIVE Commercial	516		0.012
25 TEMP GRADE Commercial	206		0.005
25A TEMP DRIVE Commercial	130		0.003
26 FEE PER Residential	2,444	0.056	
27 FEE PER Residential	2,452	0.056	
28 FEE PER Residential	2,263	0.052	
28A TEMP DRIVE Residential	226		0.005
29 TEMP DRIVE Commercial	370		0.008
30 TEMP DRIVE Commercial	1,225		0.028
31 FEE PER Commercial	1,910	0.044	
32 FEE PER Commercial	1,197	0.027	
32A TEMP DRIVE GRADE Commercial	1,322		0.03
33 FEE PER Commercial	2,060	0.047	
33A TEMP GRADE Commercial	296		0.007
33B TEMP DRIVE Commercial	119		0.003
34 FEE PER Residential	2,071	0.048	
34A TEMP GRADE Residential	35		0.001
34B TEMP DRIVE Residential	81		0.002
35 TEMP DRIVE GRADE Residential	570		0.013
36 FEE PER Residential	325	0.007	
37 TEMP DRIVE GRADE REV Commercial	433		0.01
38 TEMP DRIVE Commercial	47		0.001
39 TEMP DRIVE Commercial	47		0.001
40 FEE PER Residential	692	0.016	
41 TEMP DRIVE Residential	275		0.006
42 TEMP DRIVE Residential	88		0.002
43 TEMP DRIVE Residential	188		0.004
44 TEMP DRIVE GRADE Residential	468		0.011
45 TEMP DRIVE Residential	461		0.011
46 FEE Residential	75	0.002	
46A TEMP GRADE Residential	69		0.002
48 FEE PER Residential	238	0.005	
48A TEMP GRADE Residential	94		0.002
50 TEMP GRADE Residential	92		0.002
50A TEMP GRADE Residential	178		0.004
51 TEMP DRIVE Commercial	2,631		0.06
52 TEMP GRADE Commercial	411		0.009
53 TEMP GRADE Commercial	1,709		0.039
54 FEE PER Commercial	3,772	0.087	
54A TEMP DRIVE Commercial	419		0.01
56 TEMP GRADE WALK Residential	260		0.006
57 TEMP GRADE Commercial	130		0.003

<b>Parcel Name</b>	<b>Square Feet</b>	<b>Acres (Gross Fee)</b>	<b>Acres (Temp)</b>
58 TEMP DRIVE Commercial	593		0.014
59 TEMP DRIVE Residential	176		0.004
60 FEE PER Residential	616	0.014	
61 FEE PER Residential	694	0.016	
62 FEE PER Residential	753	0.017	
		1.051	0.535



PROJECT	DESIGNATION NO.
1600875	1600875
CONTRACT	
R-39905	

# INDIANA DEPARTMENT OF TRANSPORTATION



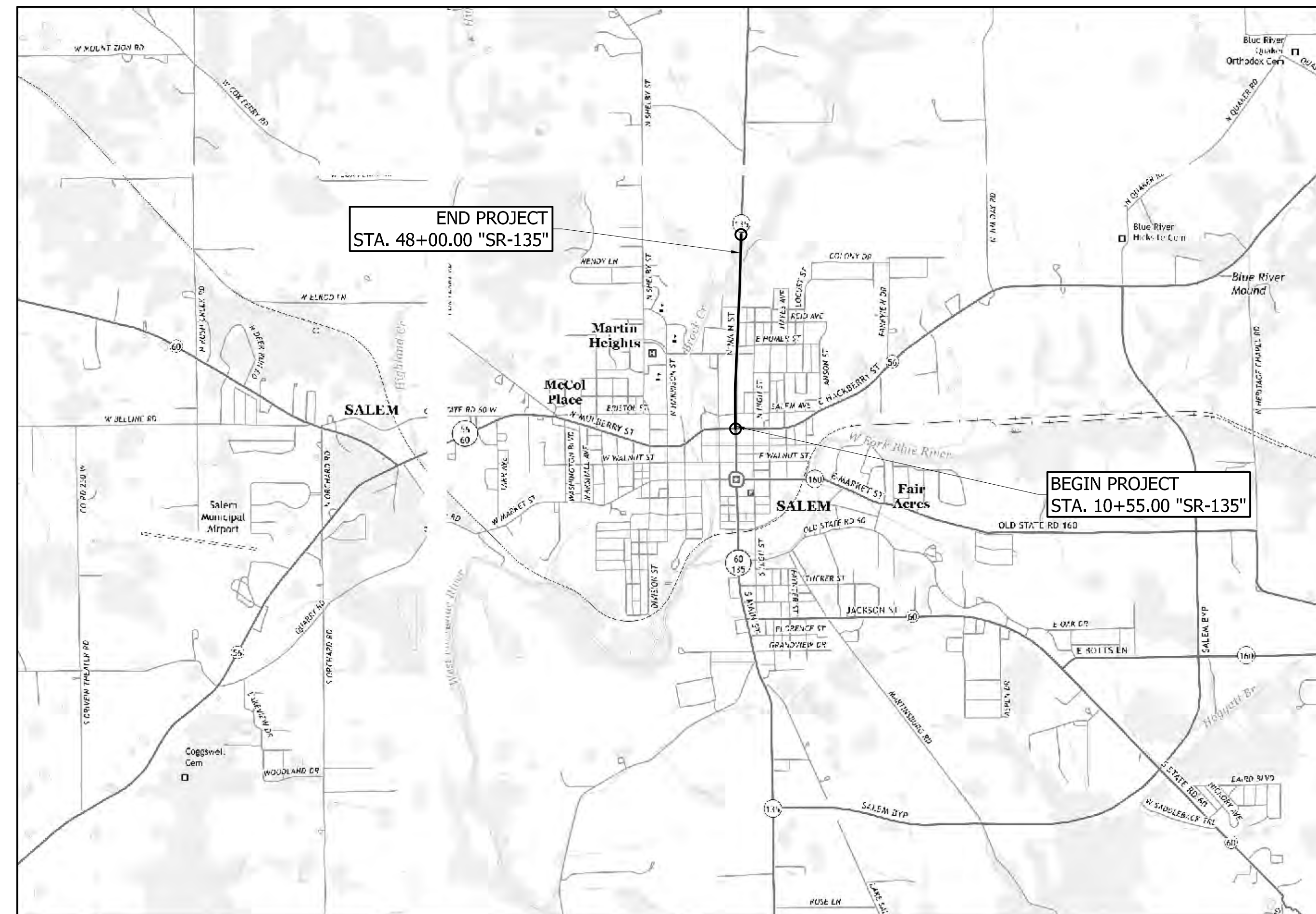
TRAFFIC DATA	SR 135
A.A.D.T. (2018)	6,540 V.P.D.
A.A.D.T. (2040)	8,400 V.P.D.
D.H.V. (2040)	976 V.P.H.
DIRECTIONAL DISTRIBUTION	49%
TRUCKS	3% D.H.V. 5% A.A.D.T.
DESIGN DATA	SR 135
DESIGN SPEED	30-45 MPH
PROJECT DESIGN CRITERIA	Partial 3R
FUNCTIONAL CLASSIFICATION	Principal Arterial
RURAL/URBAN	Urban (Suburban)
TERRAIN	Level
ACCESS CONTROL	None

KIN PROJECT INFORMATION	
DESIGNATION	PROJECT DESCRIPTION
1600873	S.R. 56 SMALL TOWN PAVEMENT REPLACEMENT
1600875	S.R. 135 ROAD RESURFACE
1700168	BRIDGE REPLACEMENT S.R. 56 OVER BROCK CREEK

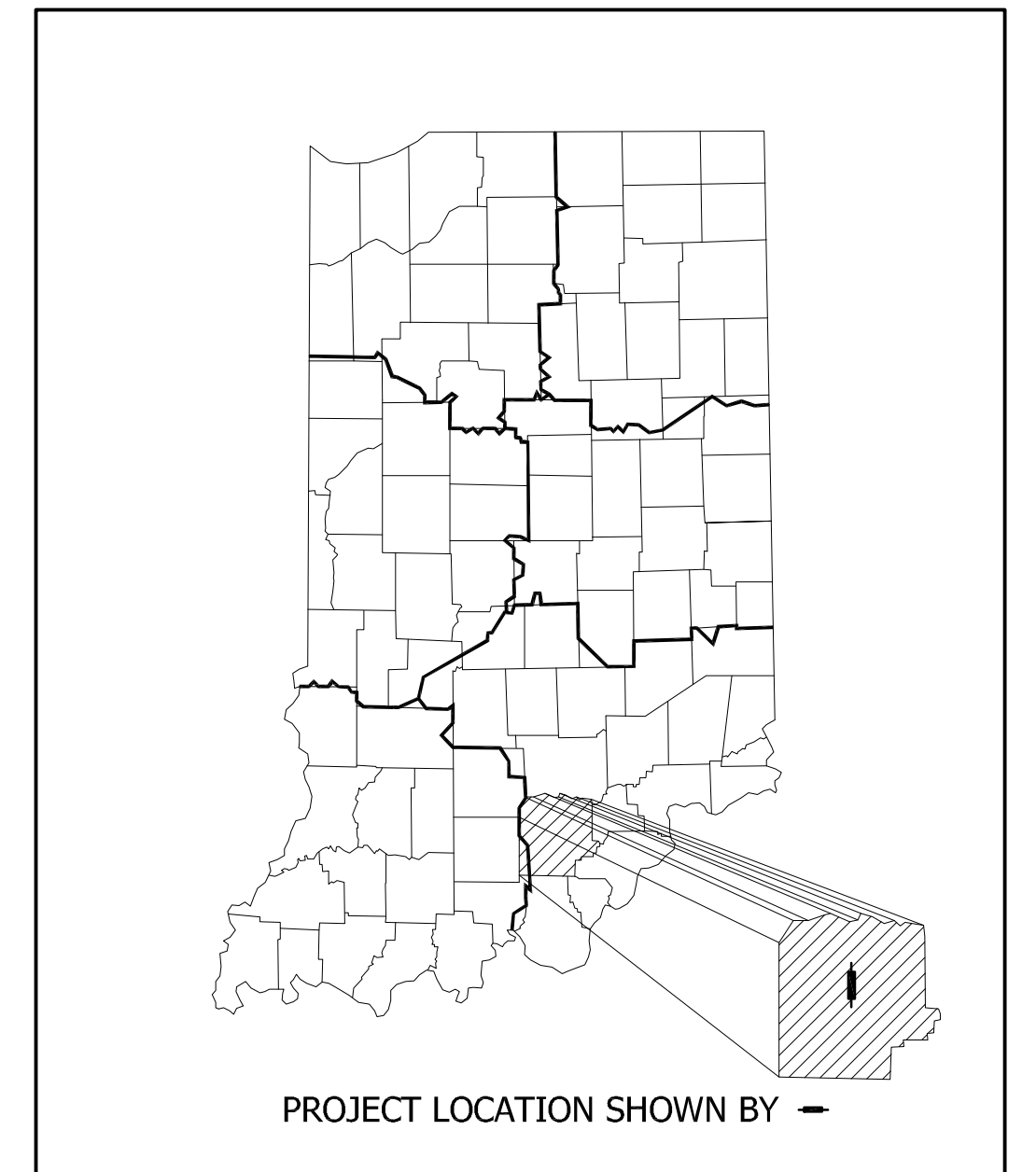
## ROAD PLANS

ROUTE: SR 135 AT: RP 43+00 TO RP 44+00  
PROJECT NO. 1600875 P.E.  
1600875 R/W  
1600875 CONST.

S.R. 135 RESURFACE FROM S.R. 56 TO 0.72 MILES NORTH OF S.R. 56 IN  
WASHINGTON COUNTY, INDIANA  
Gross Length: 0.72 Mi.  
Net Length: 0.72 Mi.

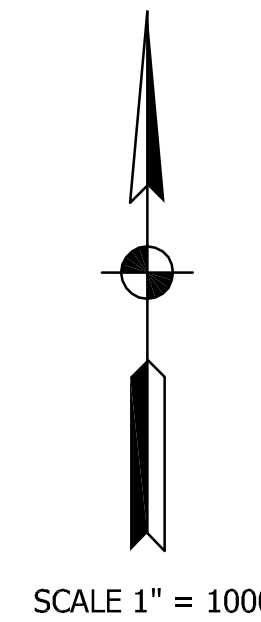


WASHINGTON COUNTY  
LOCATION MAP



LATITUDE 38° 36' 32" N LONGITUDE 86° 06' 03" W

CULVERT ASSETS	
DESIGNATION NO.	CULVERT ASSET I.D.
1600875	-
1600875	-
1600875	-



SCALE 1" = 1000'

STAGE 2 PLANS - 05/31/19

[INDIANA DEPARTMENT OF TRANSPORTATION  
STANDARD SPECIFICATIONS DATED 2018  
TO BE USED WITH THESE PLANS]



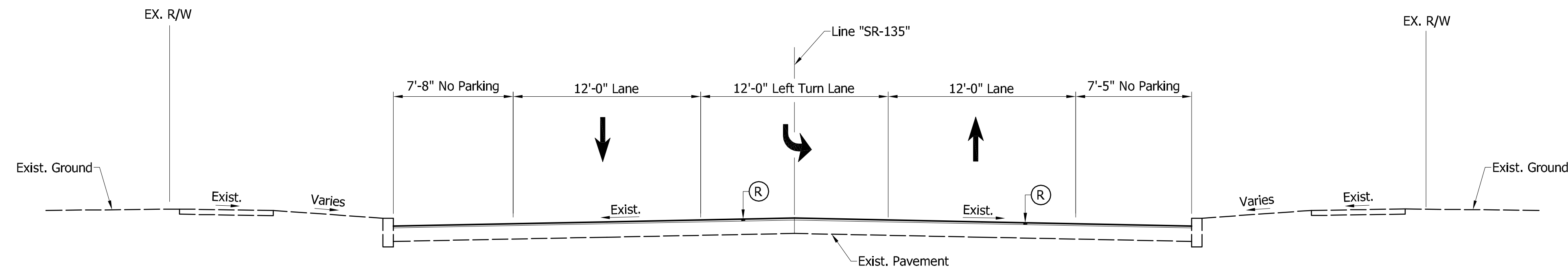
PLANS PREPARED BY:	USI Consultants, Inc.	317-544-4996
		PHONE NUMBER
CERTIFIED BY:		DATE
APPROVED FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION	DATE

DESIGNATION NO.	1600875
SHEETS	1 of 19
CONTRACT	R-39905
PROJECT NO.	1600875

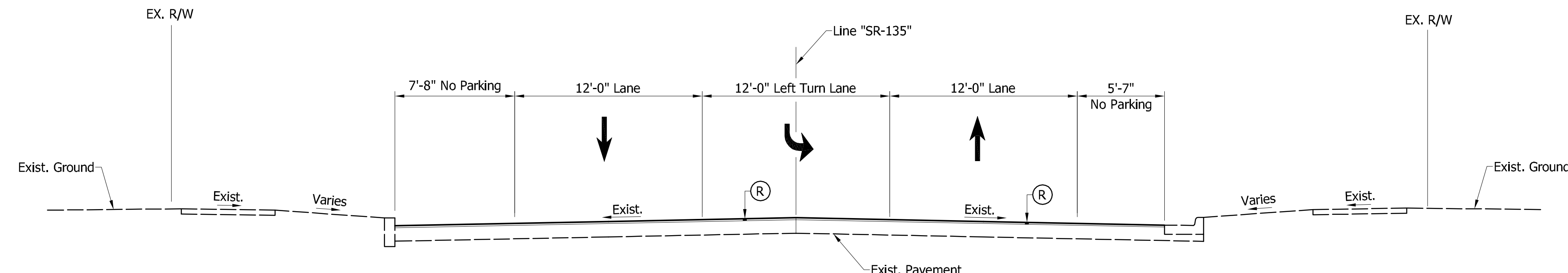
**LEGEND**

- (R) HMA Resurface / Surface Milling  
xxx#/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on  
xx" Surface Milling
- (D) HMA For Approaches, Type B  
xxx#/Syd. HMA Surface, Type B on  
Approach Milling

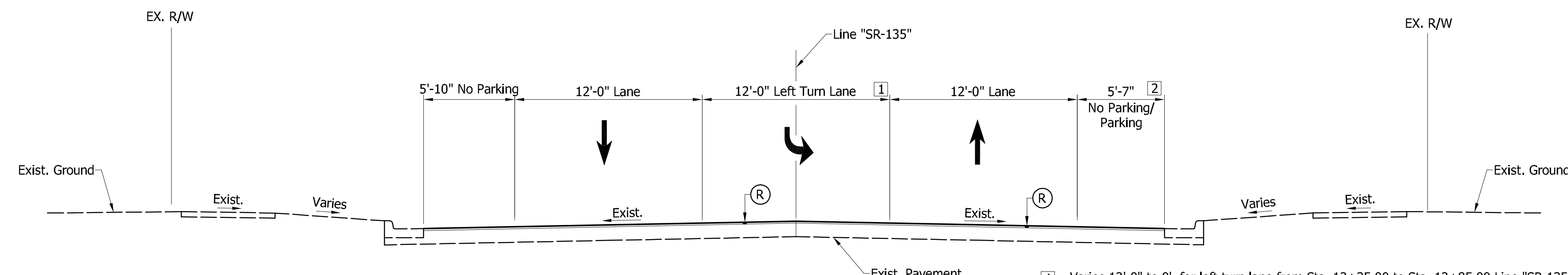
NOTE TO REVIEWER: PAVEMENT DESIGN HAS BEEN SUBMITTED AND WILL BE INCLUDED WITH STAGE 3 PLANS.



**TYPICAL CROSS SECTION LINE "SR-135"**  
Sta. 10+16.29 to Sta.10+65.31 "SR-135"

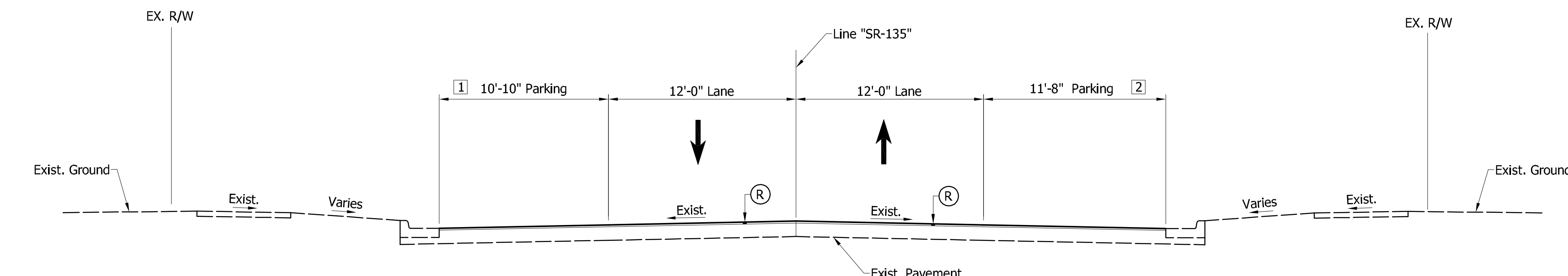


**TYPICAL CROSS SECTION LINE "SR-135"**  
Sta. 10+65.31 to Sta. 11+77.33 "SR-135"



**TYPICAL CROSS SECTION LINE "SR-135"**  
Sta. 11+77.33 to Sta.12+85.00 "SR-135"

- 1 Varies 12'-0" to 0' for left turn lane from Sta. 12+35.00 to Sta. 12+85.00 Line "SR-135"
- 2 Varies 5'-7" to 11'-6" for No Parking / Parking from Sta. 12+35.00 to Sta. 12+85.00 Line "SR-135"



**TYPICAL CROSS SECTION LINE "SR-135"**  
Sta. 12+43.17.00 to Sta. 26+43.17 "SR-135"

- 1 Varies 10'-10" to 0' for Parking lane from Sta. 25+81.63 to Sta. 26+43.17 Line "SR-135"
- 2 Varies 11'-8" to 0' for Parking from Sta. 25+81.63 to Sta. 26+25.48 Line "SR-135"

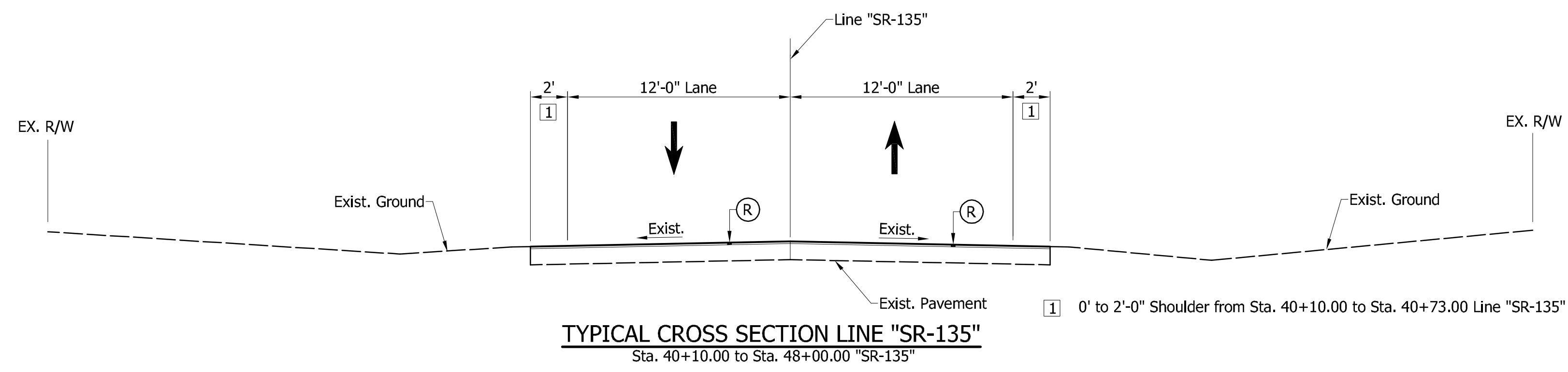
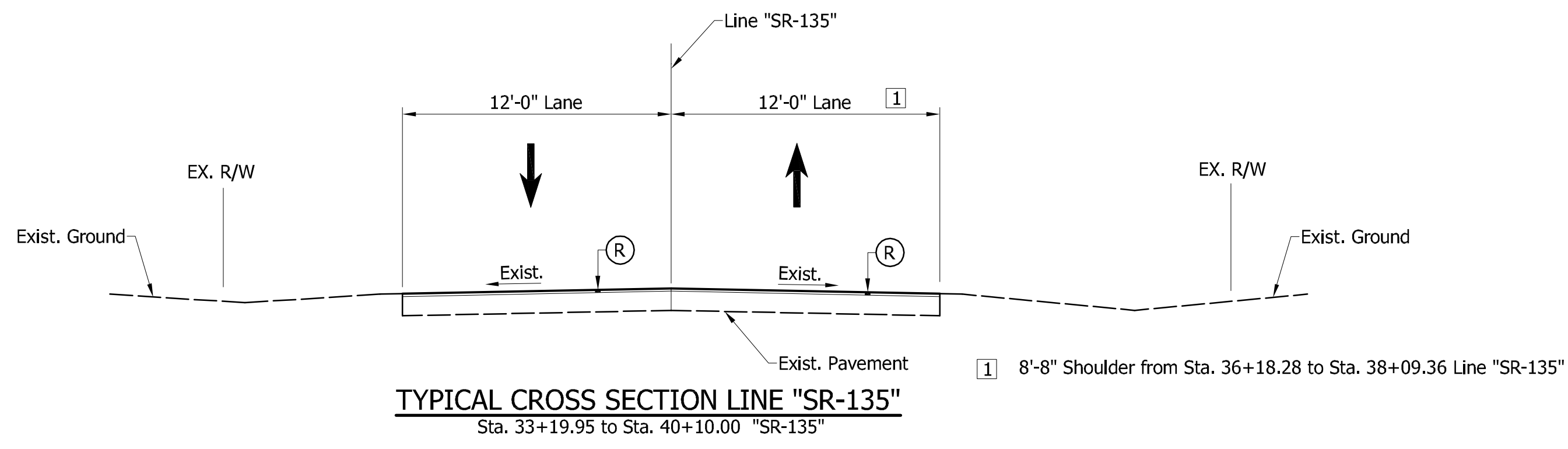
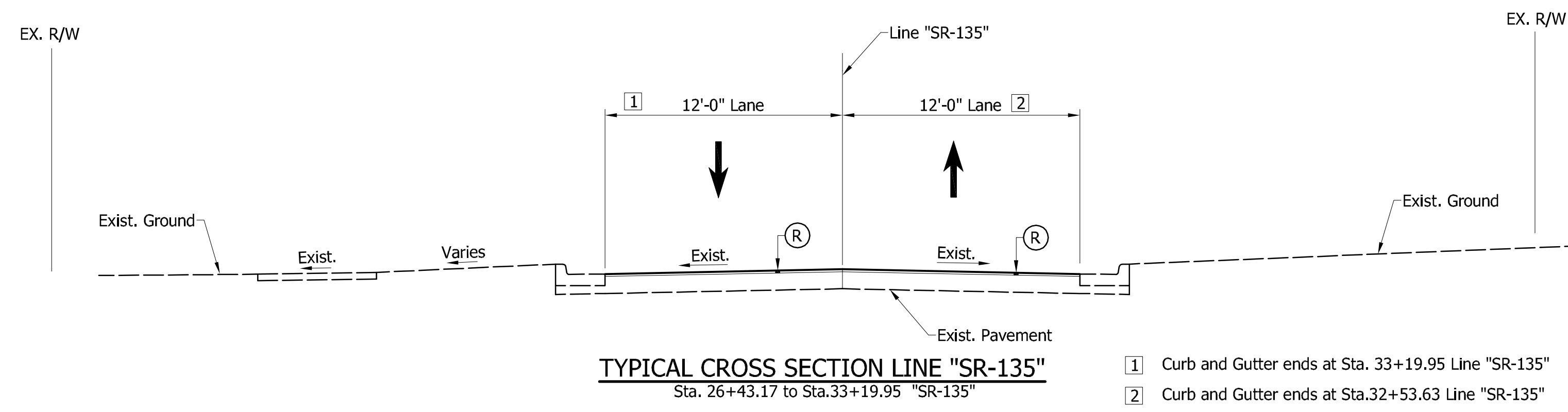
**NOT FOR CONSTRUCTION**

DESIGNED: BDC	DRAWN: BDC
CHECKED: GRW	CHECKED: GRW

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

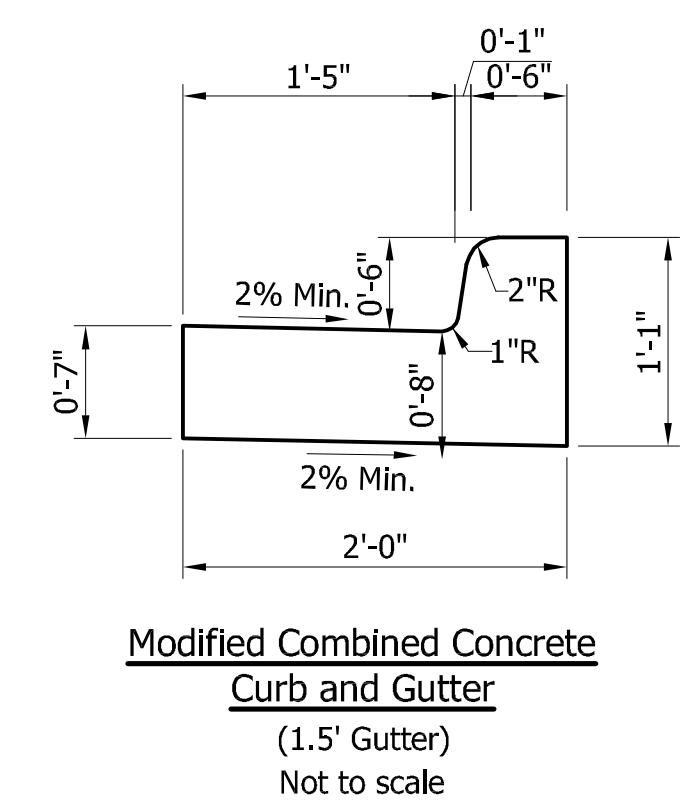
**TYPICAL CROSS SECTION  
S.R. 135**

HORIZONTAL SCALE 1" = 5'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600875
SURVEY BOOK	SHEETS 3 of 19
CONTRACT R-39905	PROJECT 1600875



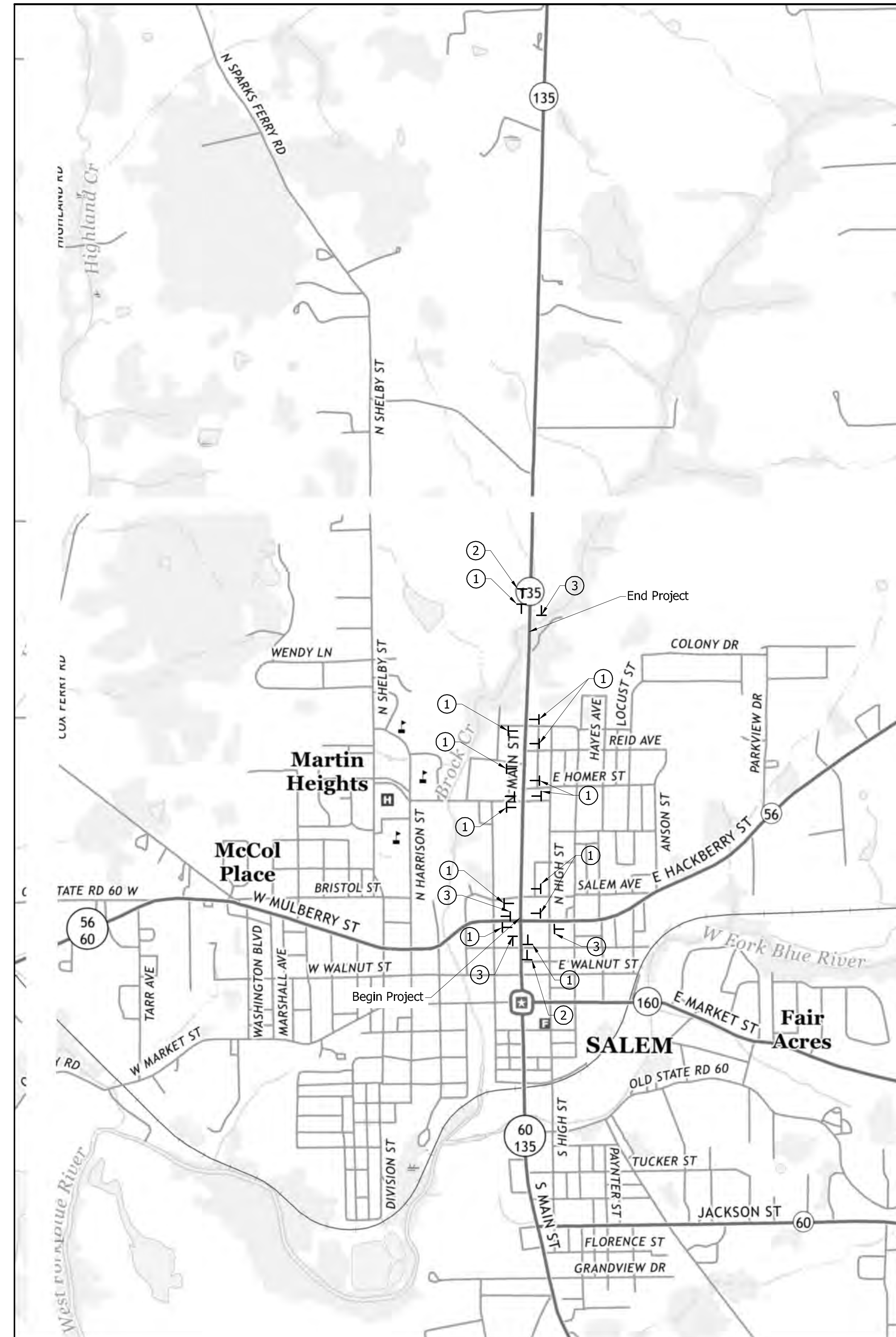
**LEGEND**




- (R) HMA Resurface / Surface Milling  
xxx#/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on  
xx" Surface Milling
- (D) HMA For Approaches, Type B  
xxx#/Syd. HMA Surface, Type B on  
Approach Milling
- (C) Compacted Aggregate, No. 53



S:\2017proj\2017-402 SR 135 Salem 1600875\Production Drawings\03-Typical Cross Sections\2017-402-TypicalSections\135-Avg\_TPS-5\20190109.00727.MXD

<b>NOT FOR CONSTRUCTION</b>	DESIGNED: _____ BDC		DRAWN: _____ BDC		INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE	
	CHECKED: _____ GRW		CHECKED: _____ GRW			1" = 5'	DESIGNATION	
						1" = 5'	1600875	
						SURVEY BOOK	SHEETS	
				CONTRACT	4 of 19	PROJECT		
				R-39905	1600875		PROJECT	



TRAFFIC MAINTENCE SIGNS					
LEGEND	DESCRIPTION	SIZE	TYPE	NO.	
①		XW20-1a	48" X 48"	A	10
②		XW2-6-A	78" X 42"	A	2
③		XG20-2	60" X 24"	A	4

**NOTES:**

- Contractor to utilize flaggers for any construction activities where flaggers are deemed necessary by the Engineer to maintain a safe construction zone.
- See Standard Drawings for worksite lane closures.
- Contractor must maintain access to the homeowners and businesses while the road is under construction.

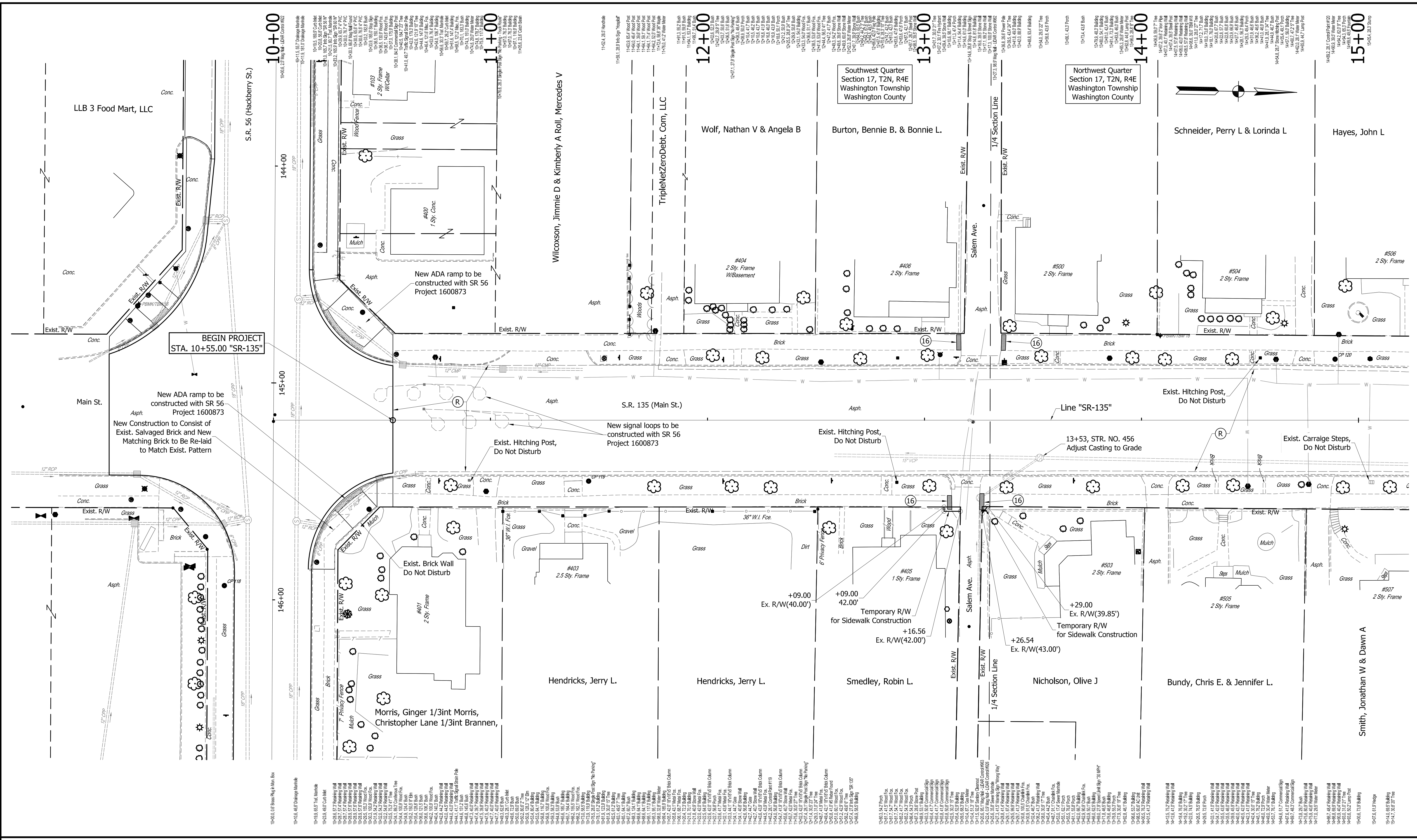
**LEGEND**  
T Construction Sign

**NOT FOR CONSTRUCTION**

DESIGNED: JGP DRAWN: JGP  
CHECKED: GRW CHECKED: GRW

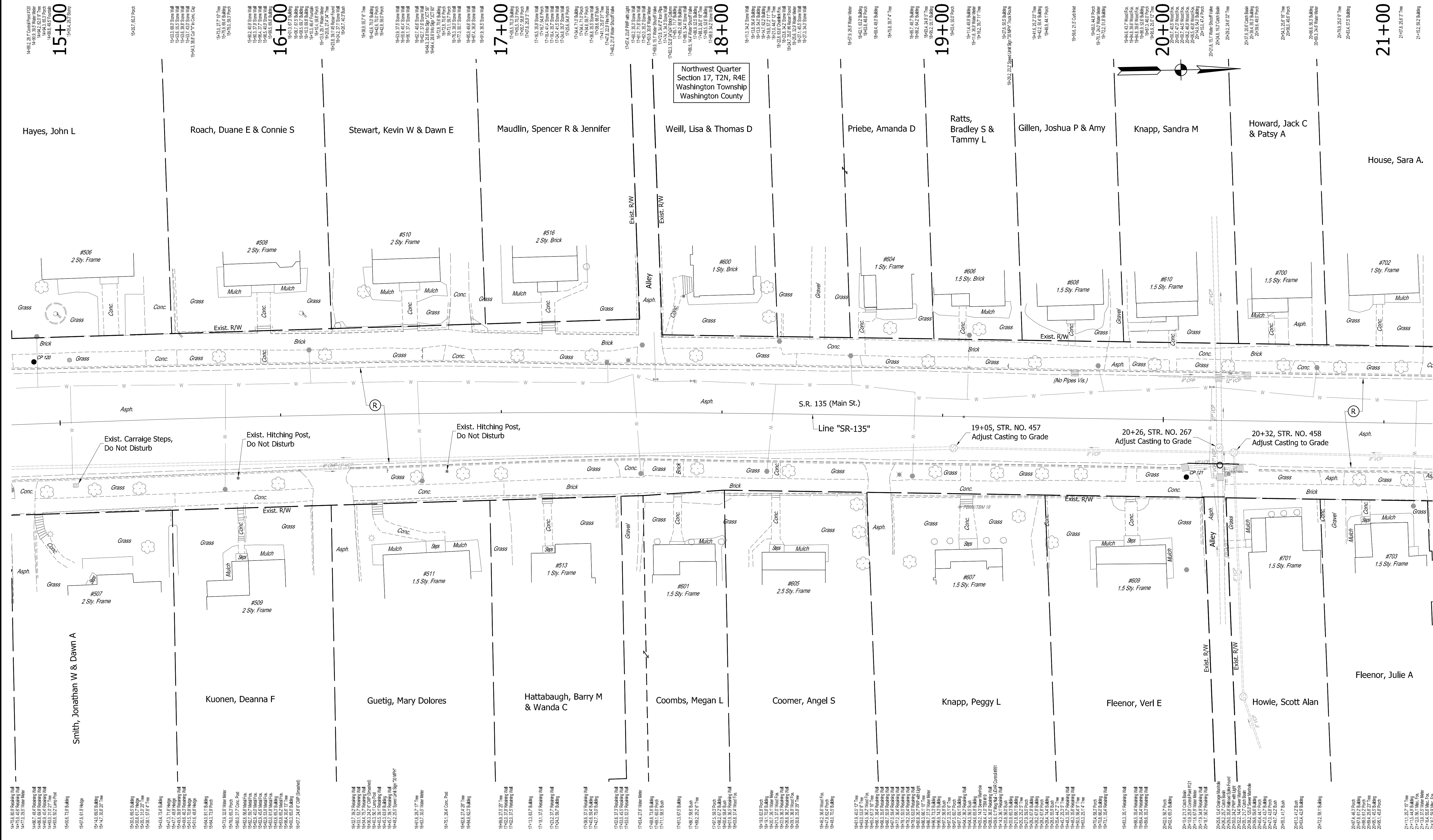
INDIANA  
DEPARTMENT OF TRANSPORTATION  
  
TRAFFIC MAINTENANCE  
S.R. 135

HORIZONTAL SCALE	BRIDGE FILE
NO SCALE	
VERTICAL SCALE	DESIGNATION
NO SCALE	1600875
SURVEY BOOK	SHEETS
	5 of 19
CONTRACT	PROJECT
R-39905	1600875



<b>LEGEND</b> (R) HMA RESURFACE / SURFACE MILLING See Typical Cross Sections		(16) 2' DETECTABLE WARNING SURFACE		<b>NOT FOR CONSTRUCTION</b>	<b>INDIANA</b> <b>DEPARTMENT OF TRANSPORTATION</b>		HORIZONTAL SCALE 1" = 20' VERTICAL SCALE N/A	BRIDGE FILE DESIGNATION 1600875
DESIGNED: BDC	DRAWN: BDC	CHECKED: GRW	CHECKED: GRW		<b>PLAN SHEET</b> <b>S.R. 135 - LINE "SR-135"</b>		SURVEY BOOK CONTRACT R-39905	SHEETS 6 of 19 PROJECT 1600875

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**LEGEND**

(R) HMA RESURFACE / SURFACE MILLING  
See Typical Cross Sections

**NOT FOR CONSTRUCTION**

DESIGNED: BDC	DRAWN: BDC
CHECKED: GRW	CHECKED: GRW

**INDIANA DEPARTMENT OF TRANSPORTATION**

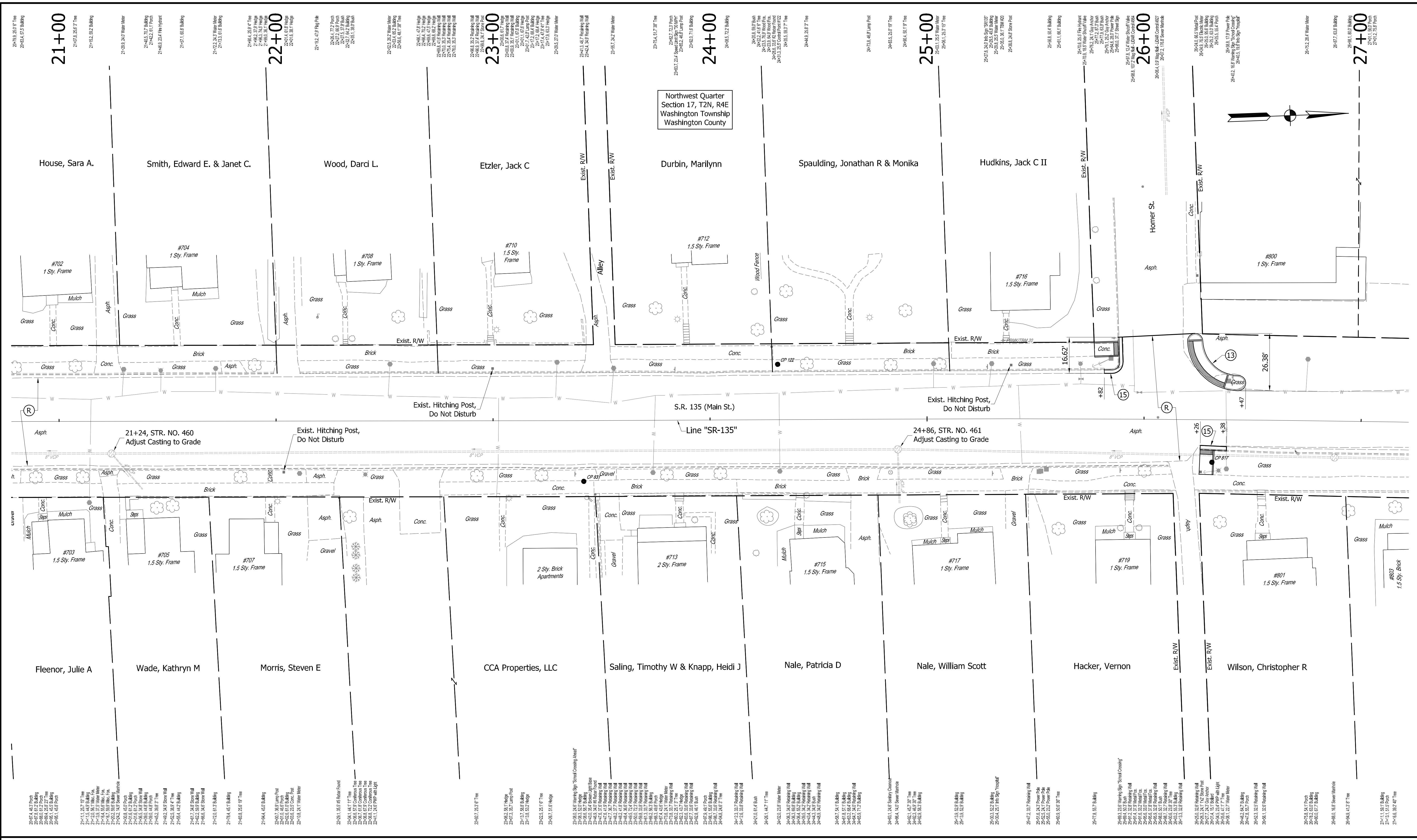
**PLAN SHEET**

**S.R. 135 - LINE "SR-135"**

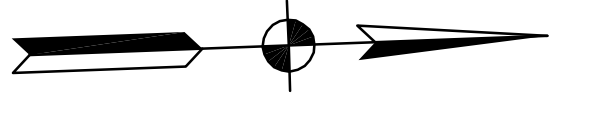
HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
N/A	1600875
SURVEY BOOK	SHEETS
	7 of 19
CONTRACT	PROJECT
R-39905	1600875

20+02.157' Pole	20+02.157' Pole
19+50.9, 26.8' Water Meter	19+50.9, 26.8' Water Meter
19+45.1, 63.7' Building	19+45.1, 63.7' Building
19+43.8, 48.8' Pole	19+43.8, 48.8' Pole
19+41.4, 43.0' Building	19+41.4, 43.0' Building
19+15.0, 36.5' Water Meter	19+15.0, 36.5' Water Meter
19+15.2, 30.7' Pole	19+15.2, 30.7' Pole
19+21.0, 15.7' Water Street Valve	19+21.0, 15.7' Water Street Valve
20+48.8, 18.2' Chain Beam	20+48.8, 18.2' Chain Beam
20+26.2, 24.6' Pole	20+26.2, 24.6' Pole
20+53.0, 30.6' Chain Beam	20+53.0, 30.6' Chain Beam
20+39.4, 55.2' Building	20+39.4, 55.2' Building
20+38.6, 40.7' Pole	20+38.6, 40.7' Pole
20+54.3, 25.6' Pole	20+54.3, 25.6' Pole
20+80.0, 60.7' Pole	20+80.0, 60.7' Pole
20+65.6, 56.2' Building	20+65.6, 56.2' Building
20+65.0, 56.6' Water Meter	20+65.0, 56.6' Water Meter
20+70.8, 26.0' Pole	20+70.8, 26.0' Pole
20+62.4, 57.2' Building	20+62.4, 57.2' Building
20+102.8, 26.8' Pole	20+102.8, 26.8' Pole
20+152.2, 59.2' Building	20+152.2, 59.2' Building

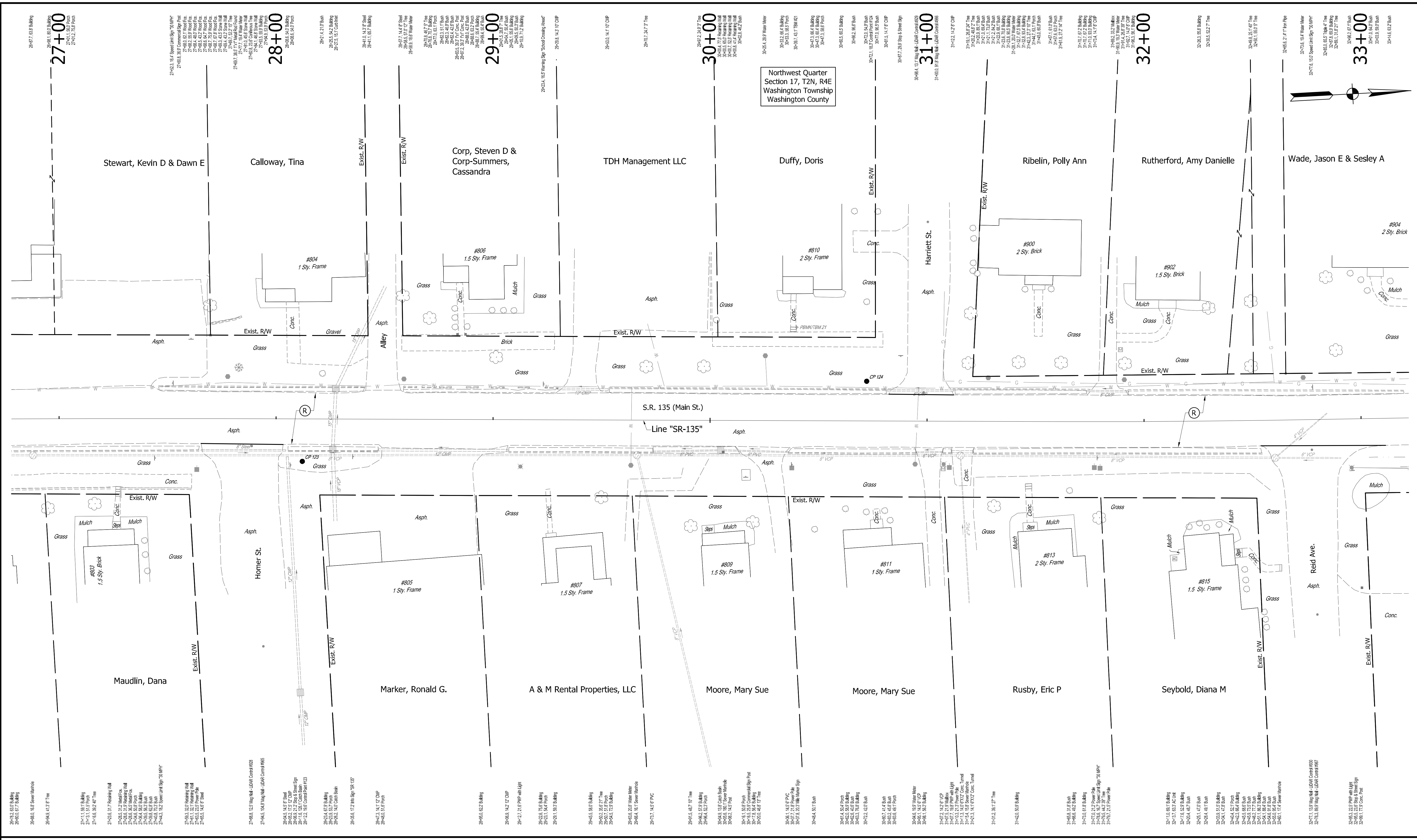
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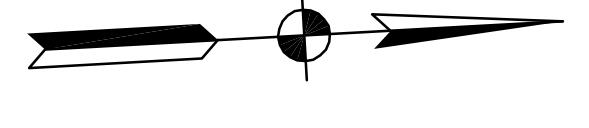
Northwest Quarter  
Section 17, T2N, R4E  
Washington Township  
Washington County



<p><b>LEGEND</b></p> <p>(R) HMA RESURFACE / SURFACE MILLING See Typical Cross Sections</p> <p>(13) CONCRETE CURB</p> <p>(15) MODIFIED COMBINED CONCRETE CURB &amp; GUTTER</p>		<p><b>NOT FOR CONSTRUCTION</b></p>	<p><b>INDIANA DEPARTMENT OF TRANSPORTATION</b></p> <p><b>PLAN SHEET</b></p> <p><b>S.R. 135 - LINE "SR-135"</b></p>		<p>HORIZONTAL SCALE</p> <p>1" = 20'</p>	<p>BRIDGE FILE</p> <p>1600875</p>
<p>DESIGNED: BDC</p> <p>CHECKED: GRW</p>	<p>DRAWN: BDC</p> <p>CHECKED: GRW</p>		<p>VERTICAL SCALE</p> <p>N/A</p>	<p>DESIGNATION</p> <p>1600875</p>	<p>SURVEY BOOK</p> <p>R-39905</p>	<p>SHEETS</p> <p>8 of 19</p>

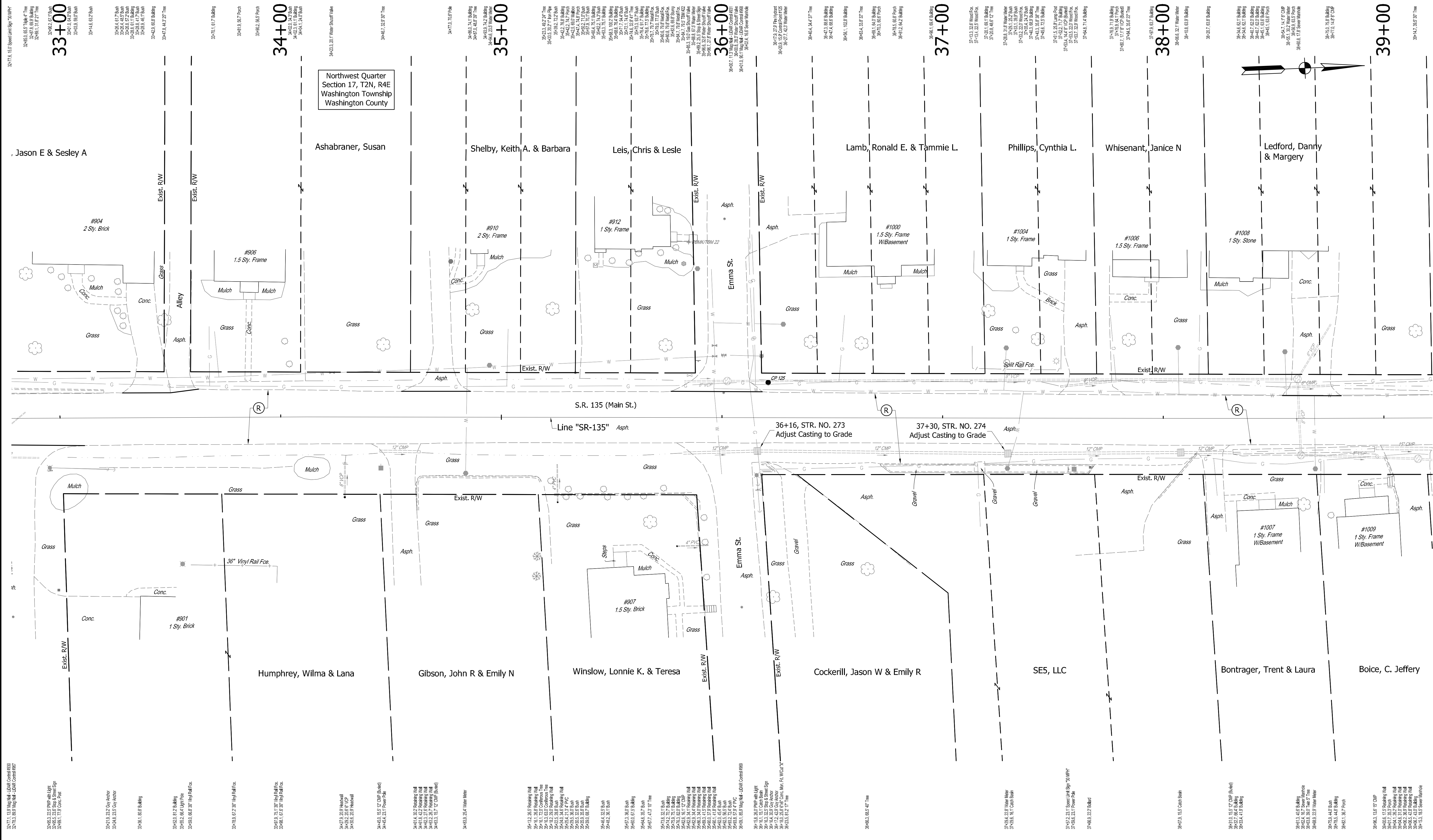


Northwest Quarter  
Section 17, T2N, R4E  
Washington Township  
Washington County



<b>LEGEND</b> (R) HMA RESURFACE / SURFACE MILLING See Typical Cross Sections		<b>NOT FOR CONSTRUCTION</b>		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 20' VERTICAL SCALE N/A		BRIDGE FILE DESIGNATION 1600875	
DESIGNED: BDC CHECKED: GRW		DRAWN: BDC CHECKED: GRW		<b>PLAN SHEET</b> S.R. 135 - LINE "SR-135"		SURVEY BOOK CONTRACT R-39905		SHEETS 9 of 19 PROJECT 1600875	

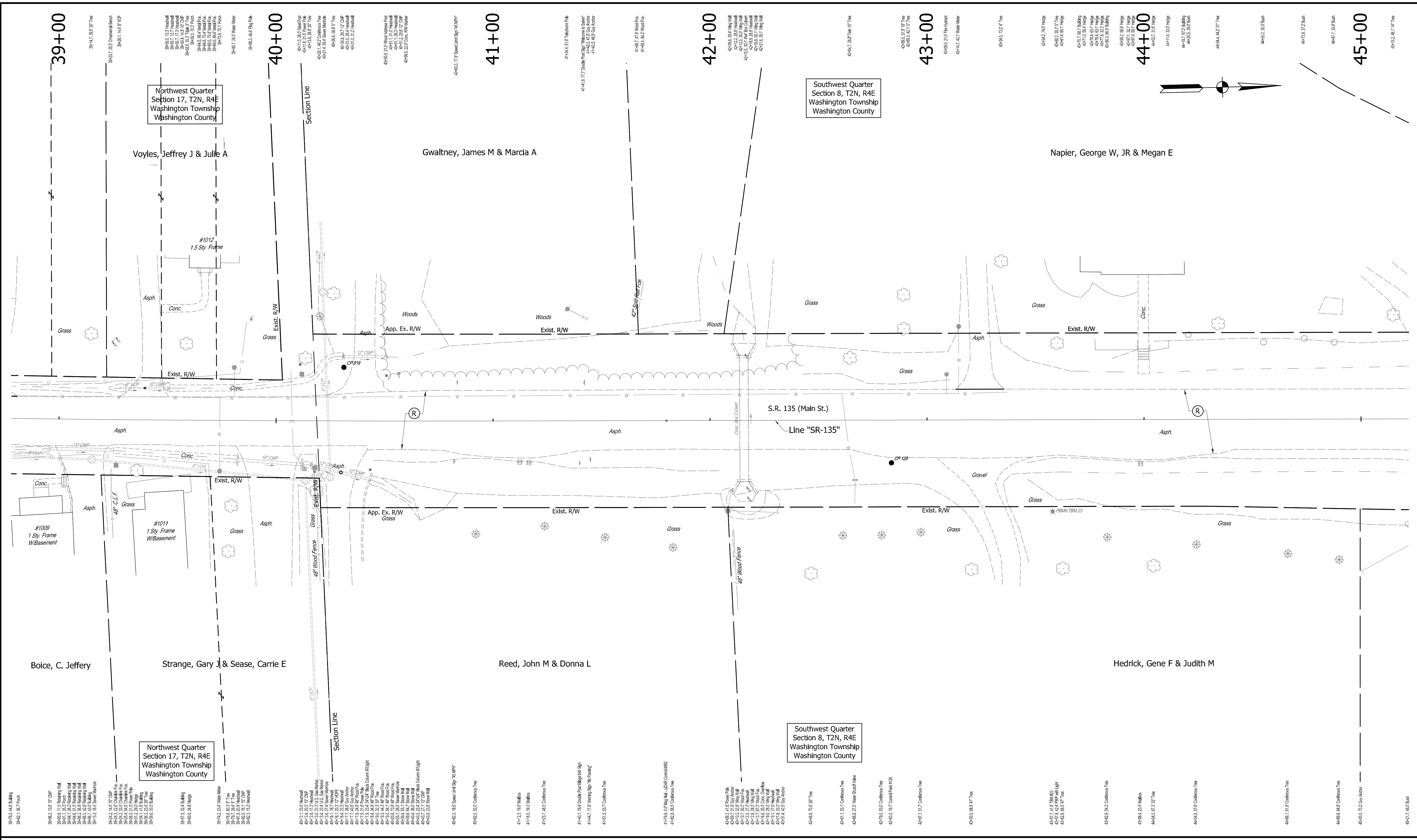




<p><b>LEGEND</b></p> <p>(R) HMA RESURFACE / SURFACE MILLING See Typical Cross Sections</p> <p>(13) CONCRETE CURB</p> <p>(15) MODIFIED COMBINED CONCRETE CURB &amp; GUTTER</p> <p>(16) 2' DETECTABLE WARNING SURFACE</p>		<p><b>INDIANA</b> <b>DEPARTMENT OF TRANSPORTATION</b></p> <p><b>PLAN SHEET</b> <b>S.R. 135 - LINE "SR-135"</b></p>	<p>HORIZONTAL SCALE</p> <p>1" = 20'</p>	<p>BRIDGE FILE</p> <p>DESIGNATION</p> <p>1600875</p>
<p>DESIGNED: BDC</p> <p>CHECKED: GRW</p>	<p>DRAWN: BDC</p> <p>CHECKED: GRW</p>		<p>SURVEY BOOK</p> <p>CONTRACT</p> <p>R-39905</p>	<p>SHEETS</p> <p>10 of 19</p> <p>PROJECT</p> <p>1600875</p>

**NOT FOR CONSTRUCTION**

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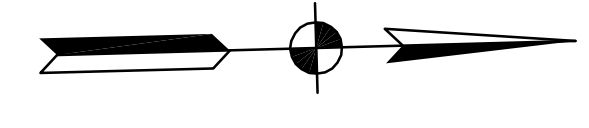


Northwest Quarter  
Section 17, T2N, R4E  
Washington Township  
Washington County

Southwest Quarter  
Section 8, T2N, R4E  
Washington Township  
Washington County

Northwest Quarter  
Section 17, T2N, R4E  
Washington Township  
Washington County

Southwest Quarter  
Section 8, T2N, R4E  
Washington Township  
Washington County



**LEGEND**

(R) HMA RESURFACE / SURFACE MILLING  
See Typical Cross Sections

**NOT FOR  
CONSTRUCTION**

DESIGNED: BDC	DRAWN: BDC
CHECKED: GRW	CHECKED: GRW

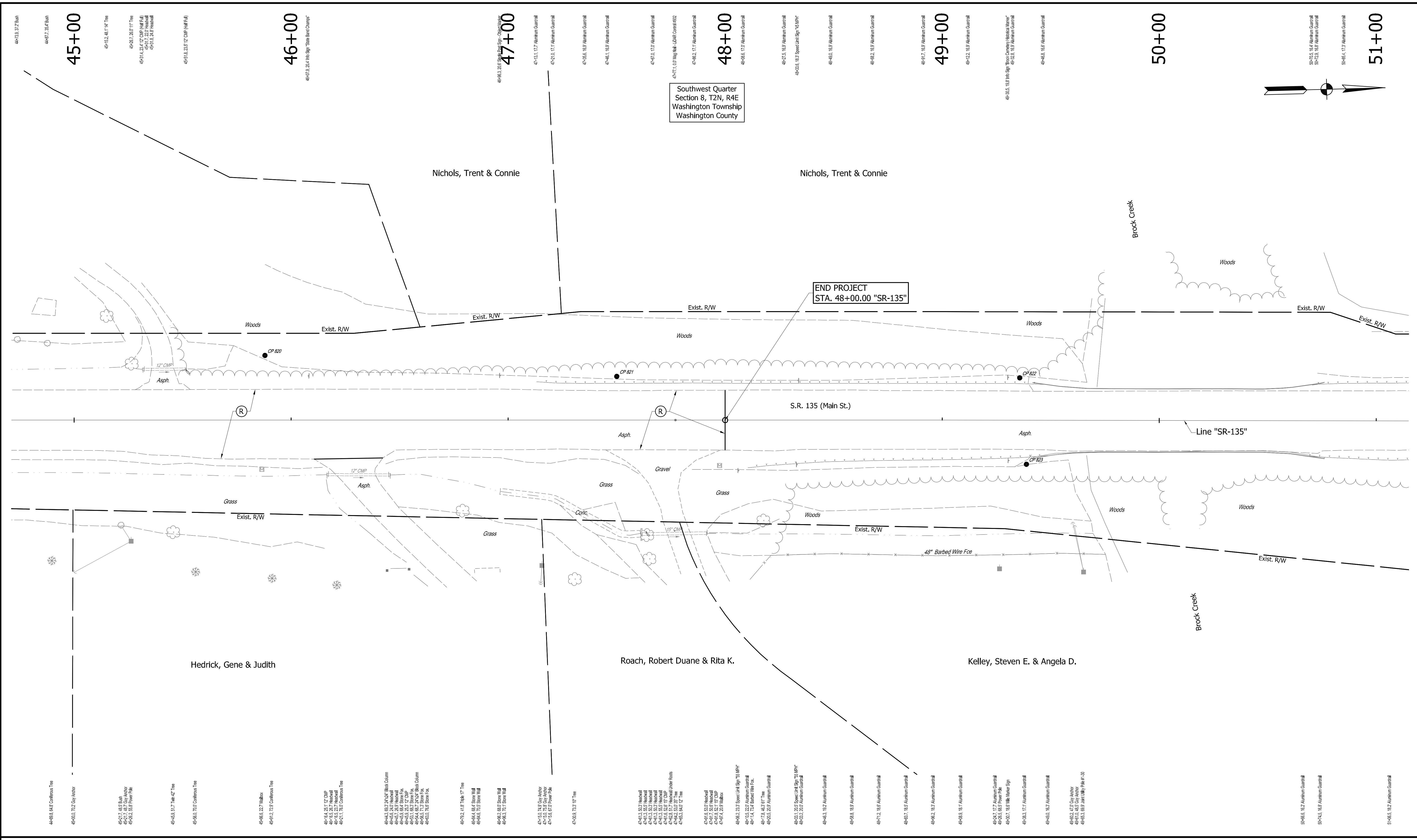
**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET**

**S.R. 135 - LINE "SR-135"**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE N/A	DESIGNATION 1600875
SURVEY BOOK R-39905	SHEETS 11 of 19
	PROJECT 1600875

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Southwest Quarter  
Section 8, T2N, R4E  
Washington Township  
Washington County

END PROJECT  
STA. 48+00.00 "SR-135"

**LEGEND**

(R) HMA RESURFACE / SURFACE MILLING  
See Typical Cross Sections

**NOT FOR  
CONSTRUCTION**

DESIGNED: BDC DRAWN: BDC  
CHECKED: GRW CHECKED: GRW

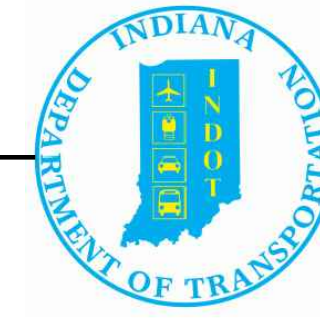
**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET  
S.R. 135 - LINE "SR-135"**

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
N/A	1600875
SURVEY BOOK	SHEETS
	12 of 19
CONTRACT	PROJECT
R-39905	1600875

PROJECT	DESIGNATION NO.
1600873	1700168
CONTRACT	BRIDGE FILE
R-39905	056-88-10260

# INDIANA DEPARTMENT OF TRANSPORTATION



STRUCTURE INFORMATION				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
056-88-10260	Continuous Reinforced Haunched Concrete Slab Bridge	2 Spans: 46'-9", 46'-9" Skew: Square	Brock Creek	134+54 "SR56"

TRAFFIC DATA	S.R. 56
A.A.D.T. 2021	15,120 V.P.D.
A.A.D.T. 2041	16,880 V.P.D.
D.H.V. 2041	1,530 V.P.H.
DIRECTIONAL DISTRIBUTION	49%
TRUCKS	3% D.H.V. 5% A.A.D.T.
DESIGN DATA	S.R. 56
DESIGN SPEED	30 MPH
PROJECT DESIGN CRITERIA	4R (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Urban Arterial
RURAL/URBAN	Urban (Built-Up)
TERRAIN	Level
ACCESS CONTROL	None

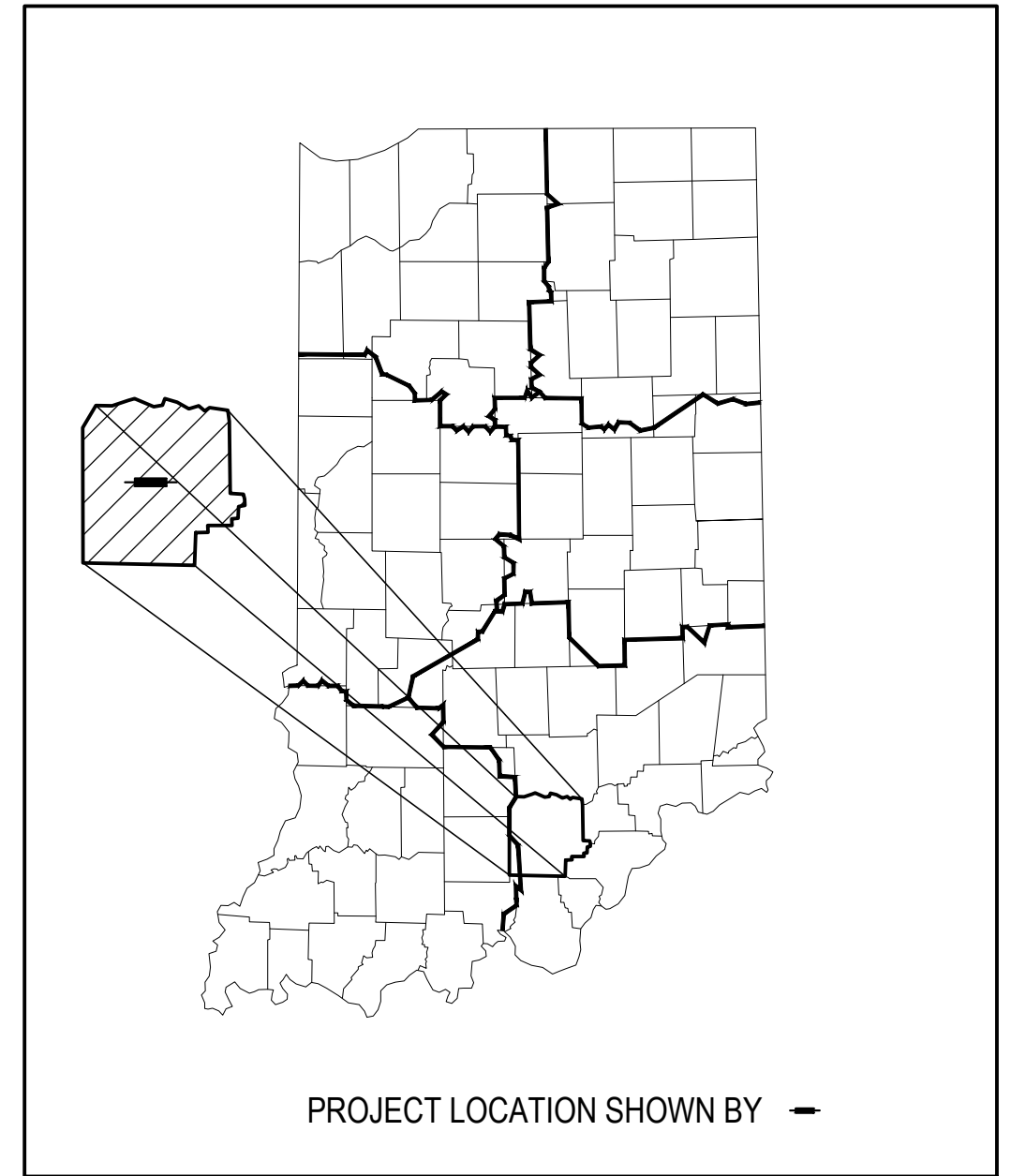
KINNED PROJECT INFORMATION	
DESIGNATION	PROJECT DESCRIPTION
1600873	SR56 PAVEMENT REPLACEMENT PROJECT (LEAD DES. NO.)
1600875	SR135 PAVEMENT RESURFACING PROJECT

## BRIDGE PLANS FOR SPANS OVER 20 FEET

ROUTE: SR56 AT: RP 98+84  
PROJECT NO. 1600873 P.E.  
1600873 R/W  
1600873 CONST.

Des. No. 1700168

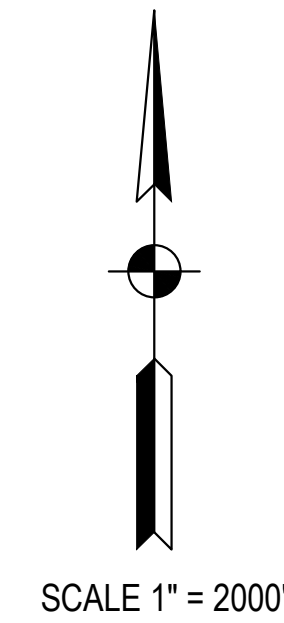
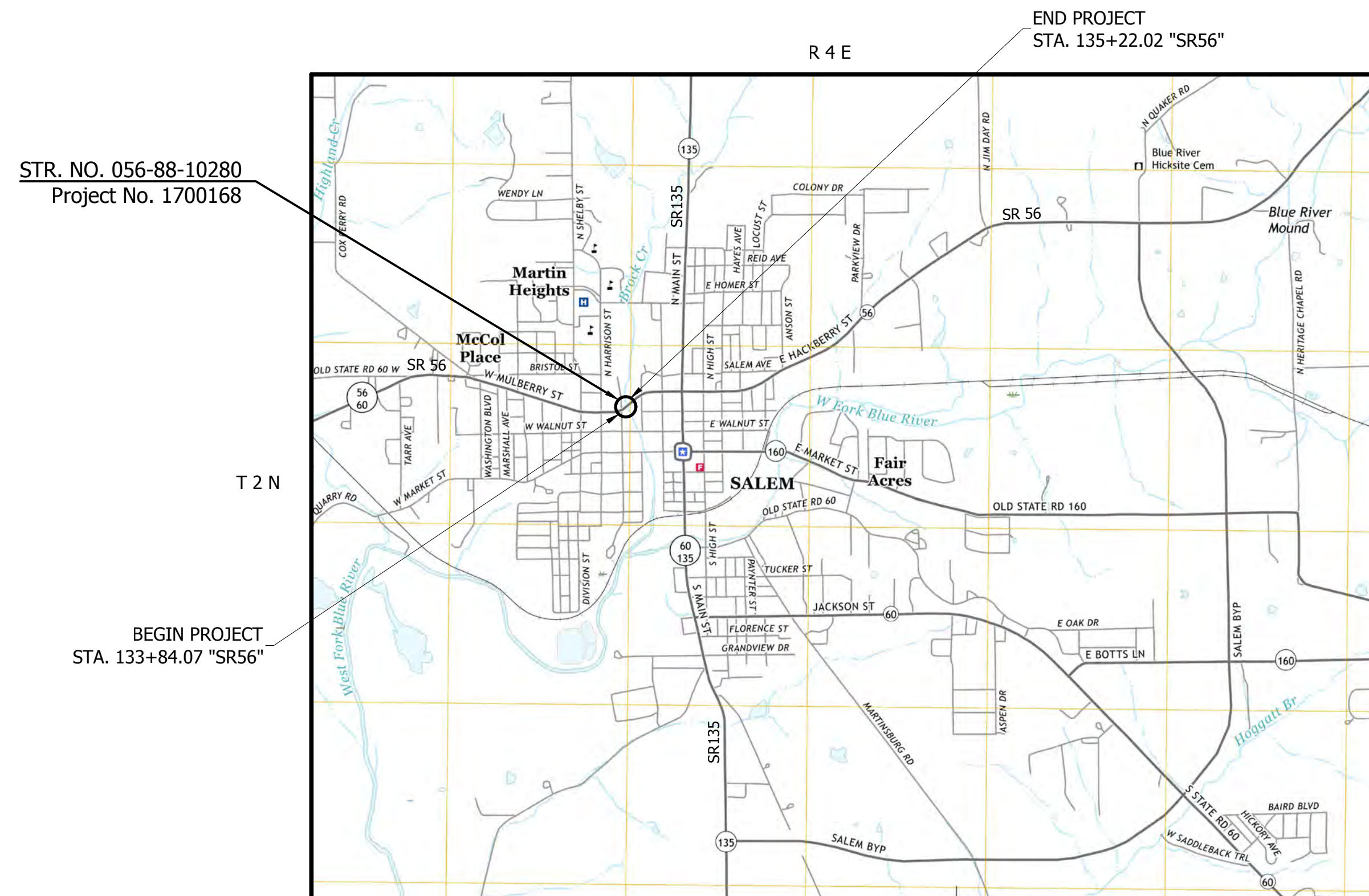
BRIDGE REPLACEMENT ON SR 56 OVER BROCK CREEK  
LOCATED 0.25 MILES WEST OF SR 135  
SECTION 17, T-2-N, R-4-E, WASHINGTON TOWNSHIP, WASHINGTON COUNTY



LATITUDE: 38°36'28.7" N LONGITUDE: 86°06'15.5" W

BRIDGE LENGTH = 0.018 mi.  
ROAD LENGTH = 0.008 mi.  
TOTAL LENGTH = 0.026 mi.  
MAX. GRADE = -4.18%

HUC: 05140104120020



SCALE 1" = 2000'

LOCATION MAP

STAGE 2 PLANS 5-24-19

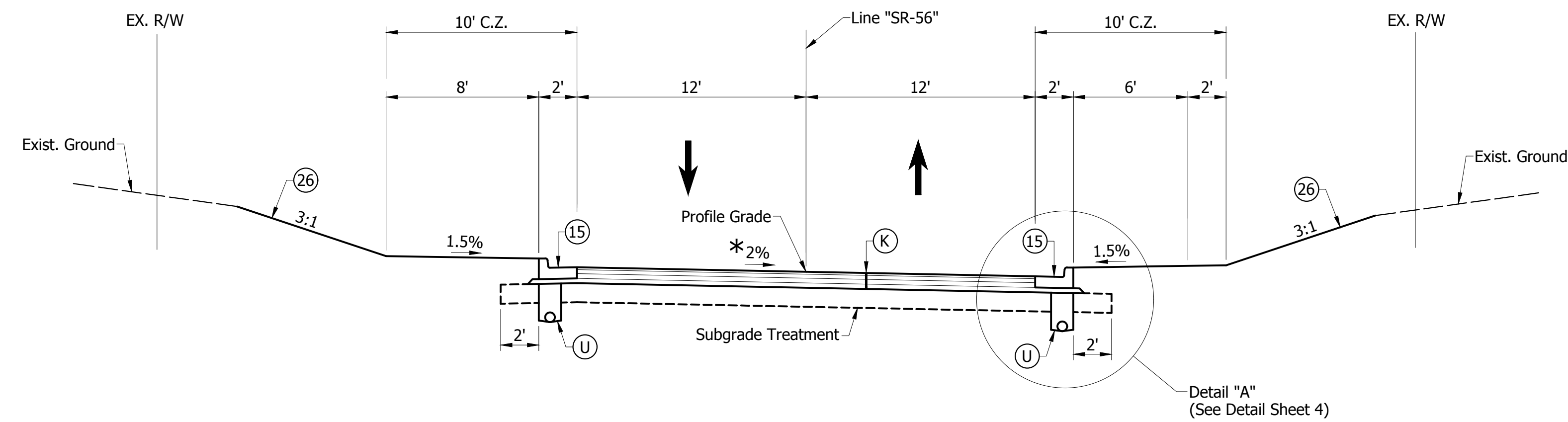
INDIANA DEPARTMENT OF TRANSPORTATION  
STANDARD SPECIFICATIONS DATED 2020  
TO BE USED WITH THESE PLANS



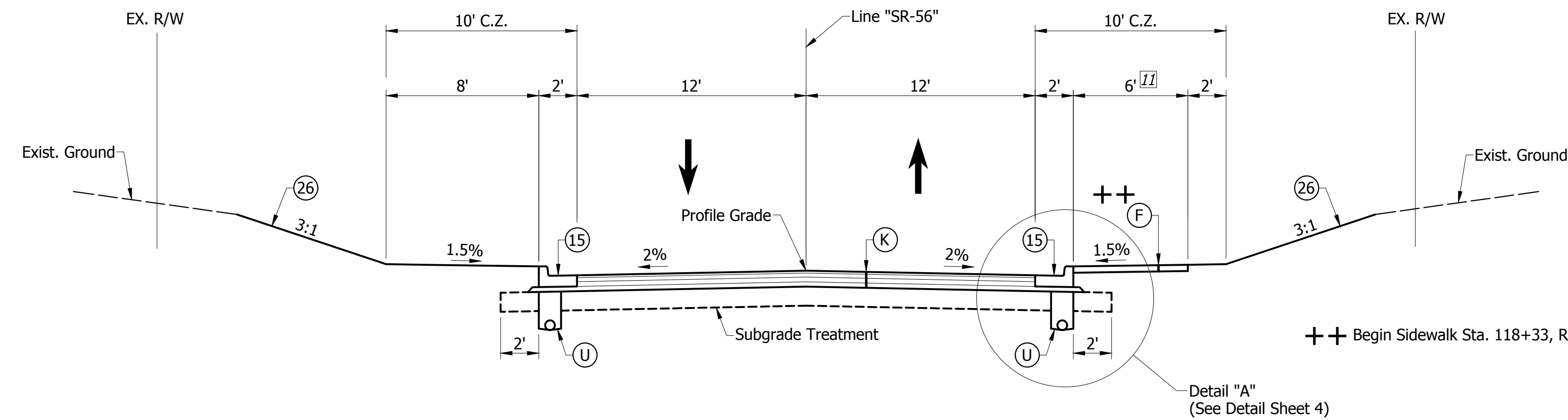
NOT FOR CONSTRUCTION

PLANS PREPARED BY:	USI Consultants, Inc.	317-544-4996
		PHONE NUMBER
CERTIFIED BY:		DATE
APPROVED FOR LETTING:		DATE
	INDIANA DEPARTMENT OF TRANSPORTATION	

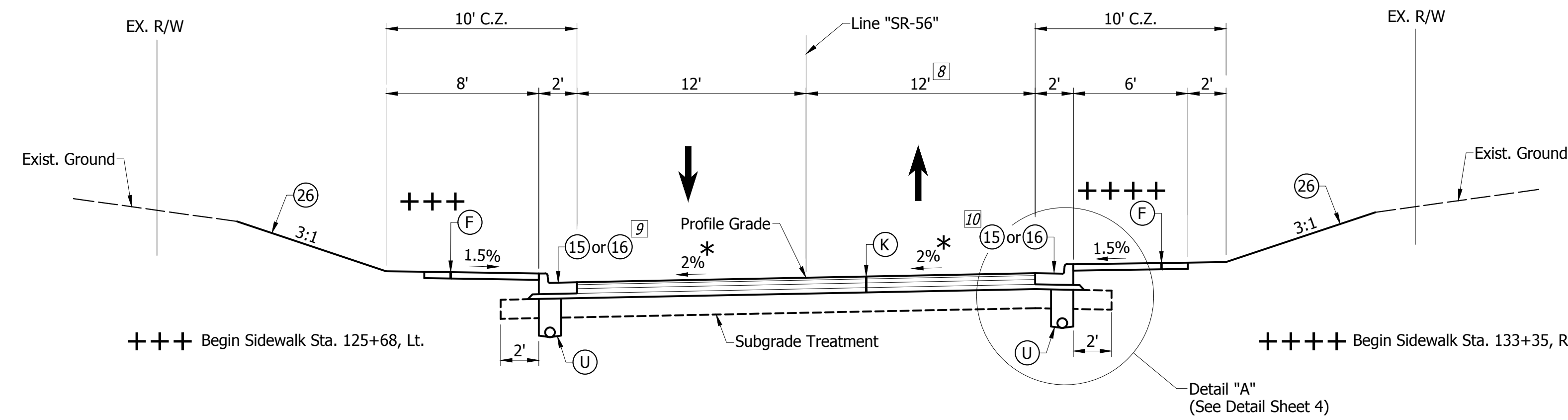
BRIDGE NO.	056-88-10260
DESIGNATION NO.	1700168
SHEETS	1 of 17
PROJECT NO.	1600873
CONTRACT	R-39905



**TYPICAL CROSS SECTION LINE "SR-56"**  
Sta. 104+50.00 to 106+26.00 "SR-56"



**TYPICAL CROSS SECTION LINE "SR-56"**  
Sta. 106+26.00 to 123+62.00 "SR-56"  
Sta. 127+84.00 to 130+63.00 "SR-56"



**TYPICAL CROSS SECTION LINE "SR-56"**  
Sta. 123+62.00 to 127+84.00 "SR-56"  
Sta. 130+63.00 to 133+84.07 "SR-56"  
Sta. 135+22.02 to 135+63.00 "SR-56"

**LEGEND**

- (K) HMA Full Depth Pavement  
xxx#/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Intermediate 19.0 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Base 19.0 mm on  
xxx#/Syd. QC/QA-HMA, 2, 64, Base 19.0 mm
- (F) Sidewalk, Concrete
- (U) Underdrain
- (15) Modified Combined Concrete Curb and Gutter (1.5' Gutter)
- (16) Combined Concrete Curb and Gutter, Type "C"
- (26) Sodding, Nursery

- Notes:  
C.Z. = Clear Zone  
\* See Superelevation Diagram
- [8] Varies from 12' @ Sta. 132+49.51 to 14' @ Sta. 133+33.37 "SR-56"  
Varies from 14.31' @ Sta. 135+22.02 to 12' @ Sta. 135+71.50 "SR-56"
  - [9] Combined Concrete Curb and Gutter, Type "C"  
From Sta. 133+63.96 to 135+46.00 "SR-56" Lt.
  - [10] Combined Concrete Curb and Gutter, Type "C"  
From Sta. 133+33.37 "SR-56" Rt. to 133+84.07 "SR-56" Rt.  
From Sta. 135+22.02 "SR-56" Rt. to 136+89.81 "PR-1" Rt.
  - [11] 4' From Sta. 110+35 to 110+66 "SR-56" Rt.

**FOR INFORMATION ONLY**

**NOT FOR CONSTRUCTION**

DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

INDIANA  
DEPARTMENT OF TRANSPORTATION

**TYPICAL CROSS SECTIONS**  
S.R. 56

HORIZONTAL SCALE 1" = 5'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 3 of 17
CONTRACT R-39905	PROJECT 1600873



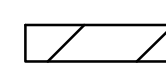


**FOR INFORMATION ONLY**

**PHASE 1C CONSTRUCTION SEQUENCE**

1. Install phase 1C signs as shown on plans.
2. Construct proposed storm sewer, main line pavement, street approaches drives, and sidewalks from Sta. 112+80.00 to Sta. 133+84.07 "SR 56".

Notes: 1. Access to all drives must be maintained.

**LEGEND**

-  Phase Construction
-  Construction Sign
-  Type III-B Barricade

**NOT FOR CONSTRUCTION**

DESIGNED: BDC	DRAWN: BDC
CHECKED: GRW	CHECKED: GRW

INDIANA  
DEPARTMENT OF TRANSPORTATION

TRAFFIC MAINTENANCE  
PHASE 1C

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE
VERTICAL SCALE 1" = 50'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 4 of 17
CONTRACT R-39905	PROJECT 1600873



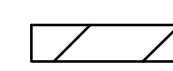


**FOR INFORMATION ONLY**

**PHASE 1D CONSTRUCTION SEQUENCE**

1. Install phase 1D signs as shown on plans.
2. Construct proposed storm sewer, main line pavement, street approaches drives, and sidewalks from Sta. 135+22.02 to Sta. 144+39.66 "SR 56".  
Mill and resurface from Sta. 144+39.66 to Sta. 148+54.00 "SR 56".

Notes: 1. Access to all drives must be maintained.

**LEGEND**

-  Phase Construction
-  Construction Sign
-  Type III-B Barricade

**NOT FOR CONSTRUCTION**

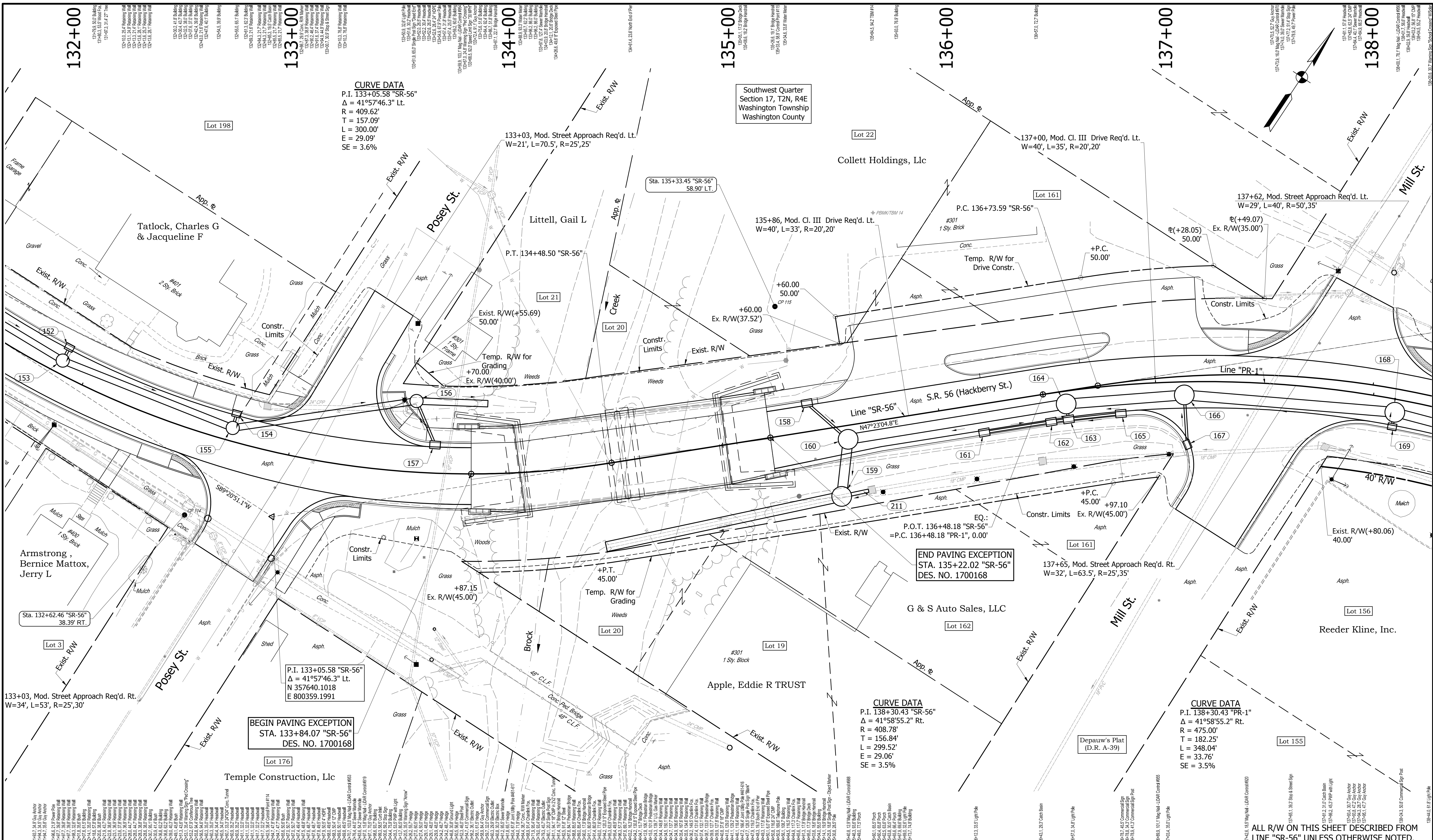
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CHECKED: _____ GRW _____	CHECKED: _____ GRW _____

INDIANA  
DEPARTMENT OF TRANSPORTATION

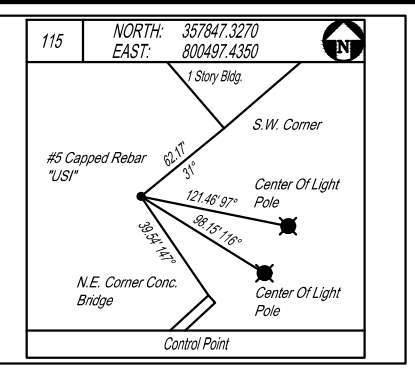
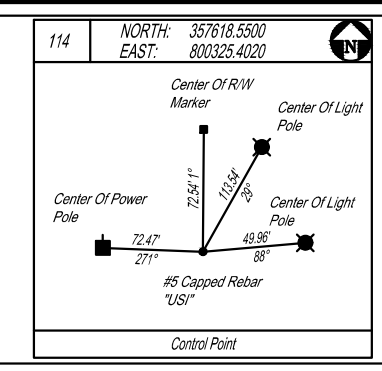
TRAFFIC MAINTENANCE  
PHASE 1D

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE
VERTICAL SCALE 1" = 50'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 5 of 17
CONTRACT R-39905	PROJECT 1600873

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**FOR INFORMATION ONLY**



**NOT FOR CONSTRUCTION**

DESIGNED:	JGP	DRAWN:	JGP
CHECKED:	GRW	CHECKED:	GRW

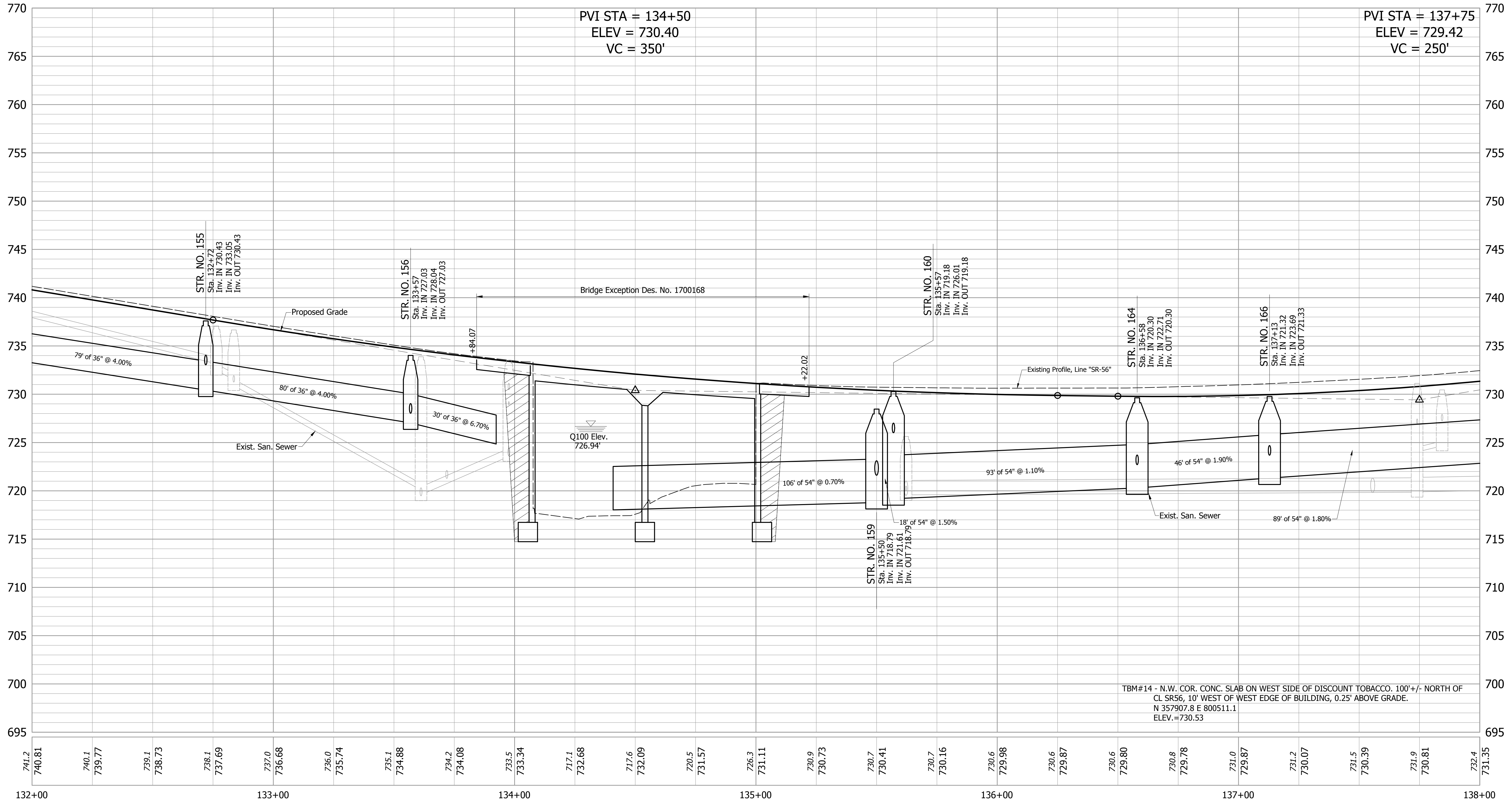
INDIANA  
DEPARTMENT OF TRANSPORTATION

PLAN  
S.R. 56

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
1" = 20'	1600873
SURVEY BOOK	SHEETS
	6 of 17
CONTRACT	PROJECT
R-39905	1600873

ALL R/W ON THIS SHEET DESCRIBED FROM LINE "SR-56" UNLESS OTHERWISE NOTED.





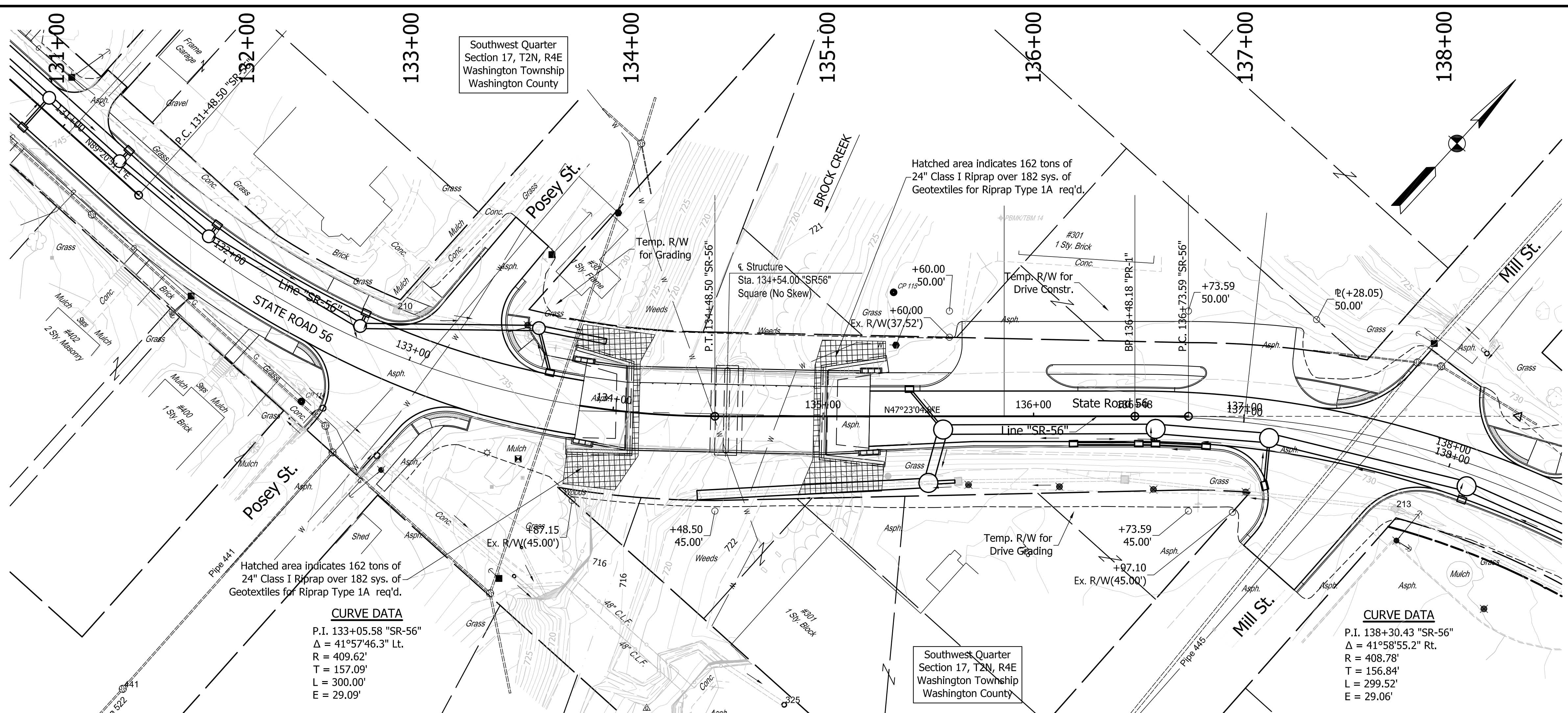
**FOR INFORMATION ONLY**

**NOT FOR CONSTRUCTION**

DESIGNED: JGP	DRAWN: JGP
CHECKED: GRW	CHECKED: GRW

INDIANA DEPARTMENT OF TRANSPORTATION	
PROFILE S.R. 56	

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 1600873
SURVEY BOOK	SHEETS 7 of 17
CONTRACT R-39905	PROJECT 1600873



**HYDRAULIC DATA**

**SITE DATA:**  
 Drainage Area: = 7.90 sq. mile  
 Q100: = 4250 cfs  
 Elevation @ Q100: = 726.94 ft.  
 Approx. Skew = 0 degrees

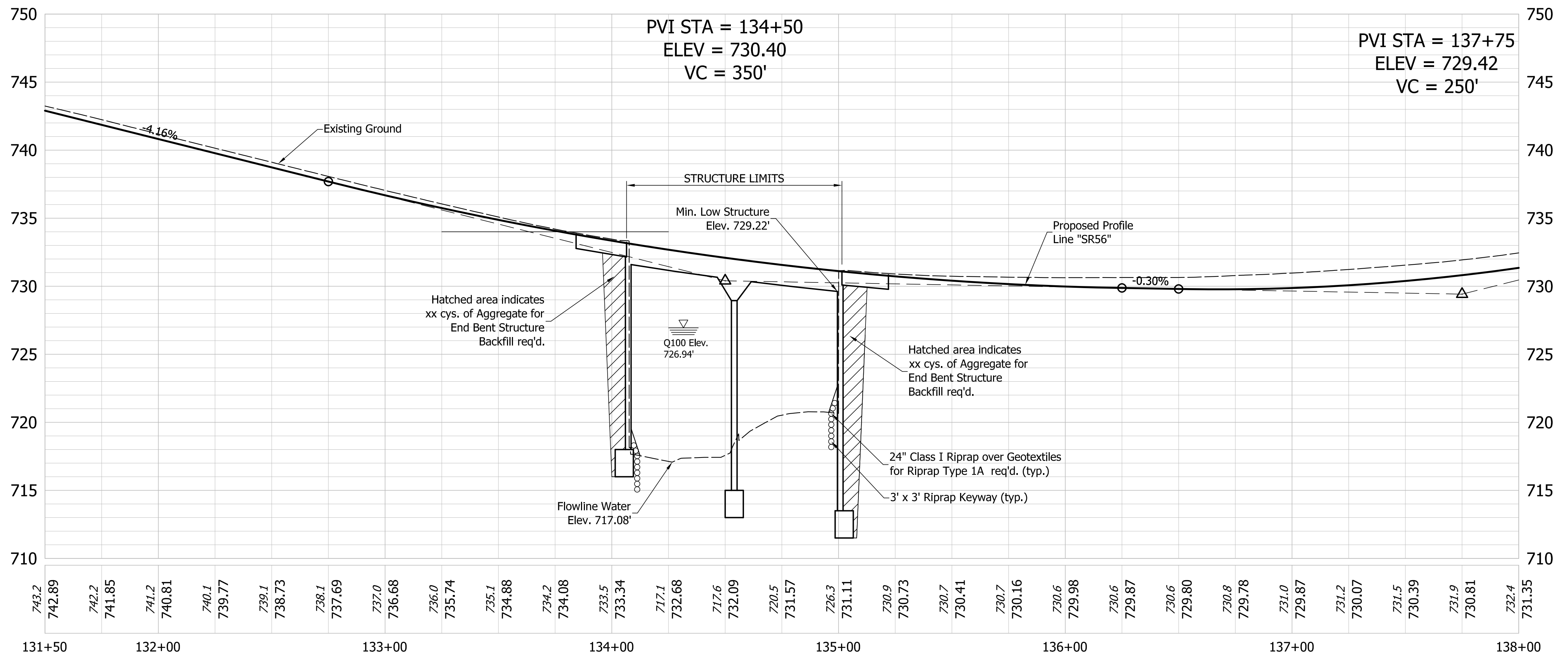
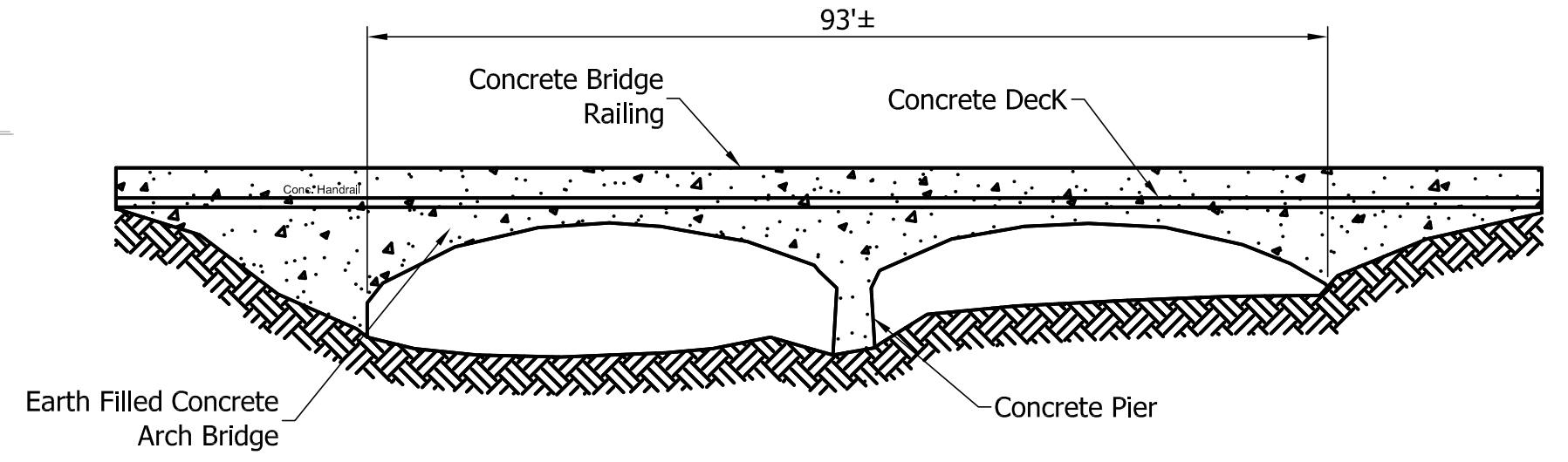
**EXISTING STRUCTURE**  
 Existing Backwater: = 1.02 ft.  
 Velocity @ Q100: = 6.70 ft./sec  
 Existing Waterway Opening below Q100 Elev. (Str.): = 629.26 sq. ft.  
 Existing Road Overflow Waterway Area: = 0.00 sq. ft.  
 Existing Low Structure Elevation: = 729.01 ft.

**PROPOSED STRUCTURE**  
 Backwater: = 0.93 ft.  
 Velocity @ Q100 = 6.16 ft./sec.  
 Gross Waterway Opening req'd. below Q100 Elev. (Str.): = 683.20 sq. ft.  
 Road Overflow Waterway Area below Q100 Elev.: = 0.00 sq. ft.  
 Minimum Low Structure Elevation: = 729.01 ft.

**Q100**  
 Q100 = 4250 cfs.  
 Q100 Elevation = 727.01 ft.  
 Q100 Contraction Scour: = 5.43 ft.  
 Q100 Total Scour: = 10.23 ft.  
 Q100 Low Scour Elevation: = 706.52 ft.  
 Q100 Max Velocity: = 7.59 ft./sec.

**Q500**  
 Q500 = 4800 cfs.  
 Q500 Elevation = 728.18 ft.  
 Q500 Contraction Scour: = 7.15 ft.  
 Q500 Total Scour: = 11.95 ft.  
 Q500 Low Scour Elevation: = 704.85 ft.  
 Q500 Max Velocity: = 7.21 ft./sec.

Scour data is based on a flowline of 716.75 ft. and erodible material



**EXISTING STRUCTURE**  
 Not to Scale  
 (Two Span Reinforced Concrete Arch Bridge to be removed)

**EARTHWORK SUMMARY**

Common Excavation	= XX Cys
Fill + 20%	= XX Cys
Waterway Excavation	= XX Cys
Usable Waterway Excavation	= XX Cys
Borrow	= XX Cys
Foundation Excavation	= XX Cys.

No direct payment for Benching. Benching to be paid for as Common Excavation.

**CONTINUOUS REINFORCED HAUNCHED CONCRETE SLAB BRIDGE**  
 TWO SPANS @ 46'-9", SQUARE  
 31'-4" CLEAR ROADWAY WIDTH  
 SR 56 OVER BROCK CREEK  
 WASHINGTON COUNTY, INDIANA

**NOT FOR CONSTRUCTION**

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MJH	DRAWN: DWB	
CHECKED: BMA	CHECKED: MJH	

**INDIANA DEPARTMENT OF TRANSPORTATION**  
**BRIDGE LAYOUT**

HORIZONTAL SCALE	BRIDGE FILE
1"=30'	056-88-10260
VERTICAL SCALE	DESIGNATION
1"=5'	1700168
SURVEY BOOK	SHEETS
---	10 of 17
CONTRACT	PROJECT
R-39905	1600873

S:\2017\Proj\2017-444 SR 56 Bridge\Drawings\2017-444 Bridge Layout.dwg Bridge Layout, 10/17/2019 9:12:20 AM

**DESIGN DATA**

Superstructure and Substructure designed for HL-93 loading, in accordance with AASHTO LRFD Bridge Design Specifications, 8th Edition, 2017, and its subsequent Interim Specifications.

Designed for actual dead load plus 35#/sft. future wearing surface.

Slab designed with 1/2" wearing surface.

**UNIT STRESSES:**

Class "C" Concrete F<sub>c</sub> = 4000 p.s.i.  
 Class "A" Concrete F<sub>c</sub> = 3500 p.s.i.  
 Class "B" Concrete F<sub>c</sub> = 3000 p.s.i.  
 Reinforcing Steel (Grade 60) F<sub>y</sub> = 60,000 p.s.i.

**GENERAL NOTES:**

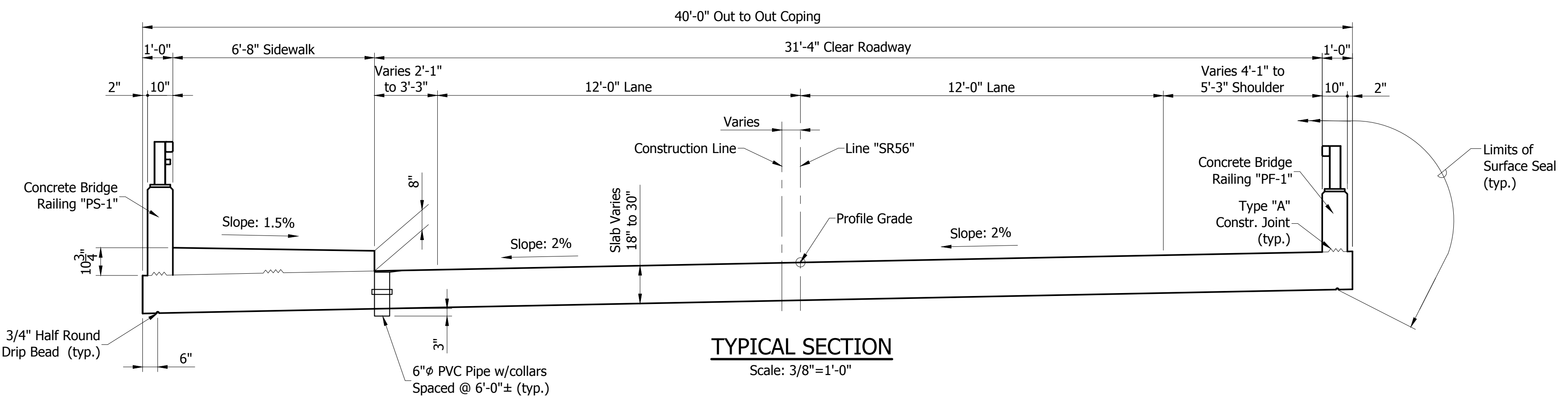
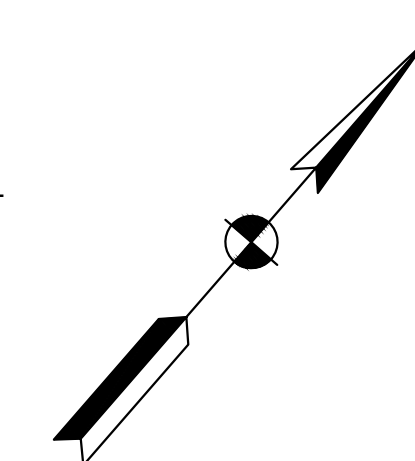
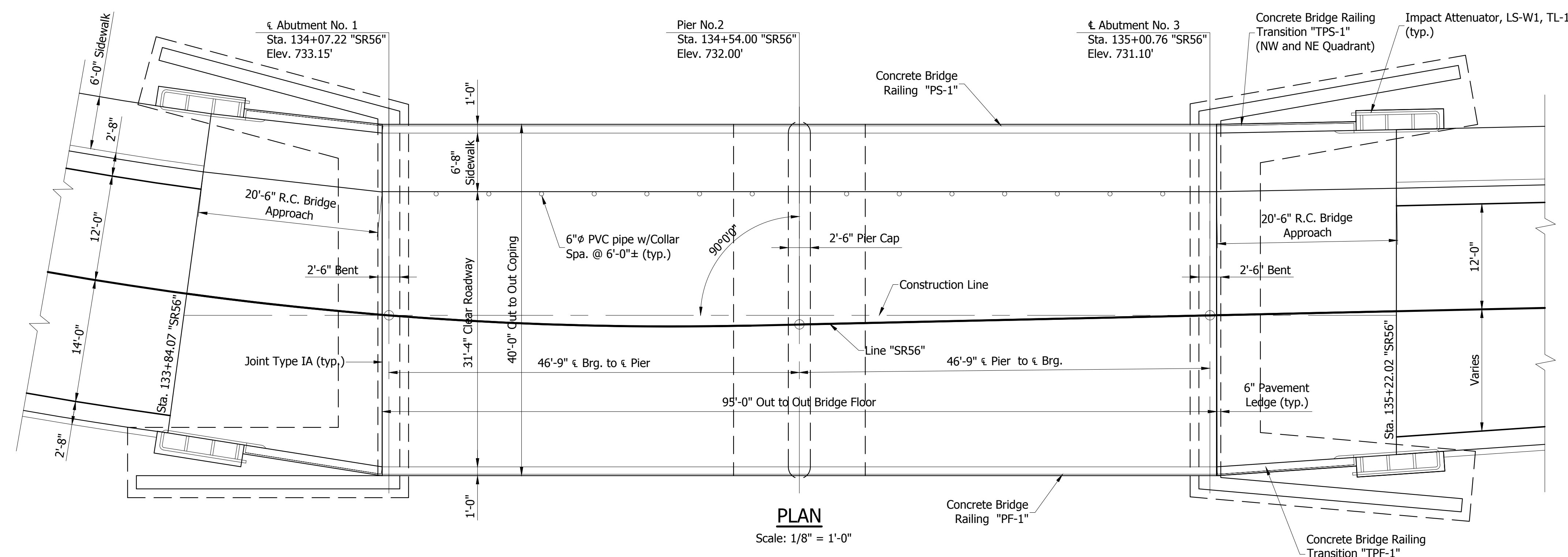
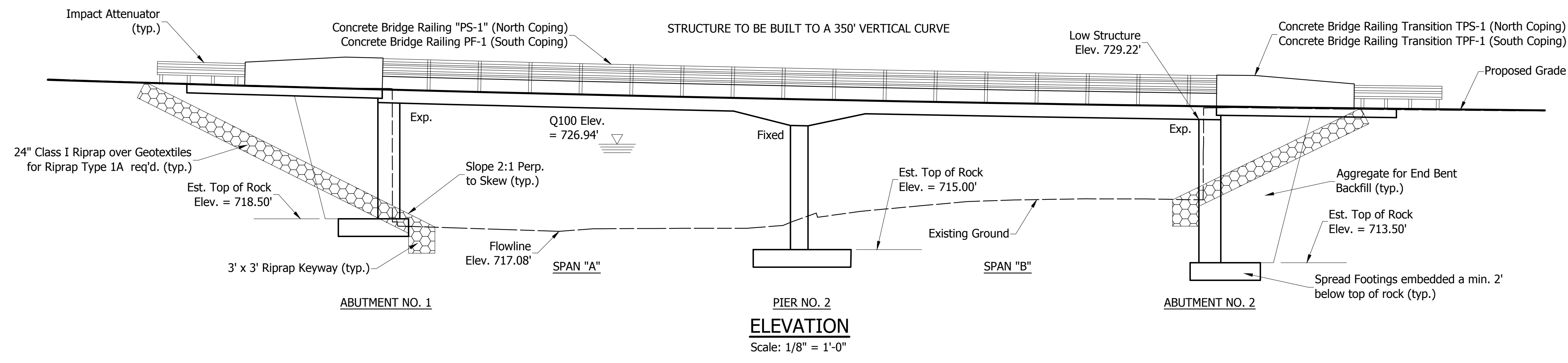
All dimensions are in feet and inches and all elevations are in feet unless otherwise noted.

Reinforcing steel covering shall be 2 1/2" in top and 1" min. in bottom of floor slabs, and 2" in all other parts, unless noted.

Surface Seal the top of bridge floor, face of deck copings, underside of bridge floor from coping to drip bead, exposed pier caps and approach slabs. Estimated Quantity = 5169 Sft.

**SEISMIC DESIGN DATA**

Seismic Performance Zone Zone 1  
 Acceleration Coefficient Sp1=0.112  
 Seismic Soil Profile Type Class D



**CONTINUOUS REINFORCED HAUNCHED CONCRETE SLAB BRIDGE**  
 TWO SPANS @ 46'-9", SQUARE  
 31'-4" CLEAR ROADWAY WIDTH  
 SR 56 OVER BROCK CREEK  
 WASHINGTON COUNTY, INDIANA

**NOT FOR CONSTRUCTION**

RECOMMENDED FOR APPROVAL: _____	DATE _____
DESIGNED: MJH	DRAWN: DWB
CHECKED: BMA	CHECKED: MJH

**INDIANA DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN**

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	056-88-10260
VERTICAL SCALE	DESIGNATION
AS NOTED	1700168
SURVEY BOOK	SHEETS
---	11 of 17
CONTRACT	PROJECT
R-39905	1600873

# **APPENDIX C**

## **Early Coordination**



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204

PHONE: (317) 233-6795

**Eric J. Holcomb, Governor**  
**Joe McGuinness, Commissioner**

## Sample Early Coordination Letter

May 14, 2018

{See Attached List}

Re: Designation Numbers 1600873, 1600875, 1700168,  
DES. No. 1600873, Road & Sidewalk Reconstruction, SR 56, 0.9 Mile East of West Junction of SR 60 to 0.8 Mile East of SR 135 (High Street)  
DES No. 1600875, Road & Sidewalk Reconstruction, SR 135, SR 56 to 0.72 Mile North of SR 56  
DES No. 1700168, Bridge Replacement, Bridge No. 056-88-01478 SR 56 over Brock Creek  
Salem, Washington County, Indiana

Dear Interested Agency:

Indiana Department of Transportation (INDOT) and Federal Highway Administration propose to utilize federal funds to proceed with the above referenced project. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above designation numbers (Des. No.) and description in your reply. We will incorporate your comments into a study of the project environmental impacts. Your cooperation in this endeavor is appreciated.

### PROJECT LOCATION

The proposed project is located in Salem, Washington County, Indiana, on SR 56 and SR 135. Specifically, the project is located in Sections 8, 13, 17, and 18, Township 2 North, Range 4 East on the 7.5' Salem, IN United States Geological Survey (USGS) Topographic quadrangle.

### EXISTING CONDITIONS

Existing Bridge: The SR 56 bridge (Bridge ID#: 056-88-01478) over Brock Creek is a concrete cast-in-place, two lane arch bridge structure constructed in 1933 with a maximum span of 45 feet and a structure length of 95 feet. The SR 56 bridge replacement will consist of the complete removal and replacement of the three span arch structure. The proposed new footprint will be similar in size to the existing structure. According to the most recent bridge inspection dated September 15, 2015, the bituminous wearing surface of the bridge has numerous cracks. The curb and copings have exposed rebar and the drains are clogged. The arches have wide cracks and are scaling with efflorescence. There is cracking, scaling and efflorescence in all the spandrel walls. The west side abutment footing is exposed and the bridge seats are cracking. Pier cap #2 has cracking, scaling and spalling with exposed rebar. Complete replacement of the structure is anticipated to prolong the use of this structure another 75 years.

Pavement History/Condition: SR 56 was constructed as a 20 feet wide concrete pavement in 1934. The pavement was widened with hot mix asphalt (HMA) to 30 feet wide in 1941 and has been resurfaced many times since.

SR 135 was first constructed with a 20 feet wide HMA in 1931. The pavement was widened to 30 feet wide in 1939, from SR 56 to Homer Street. Several resurfacings have occurred since then.



The existing pavement is generally in poor condition with some rutting, alligator cracking and areas with poor drainage.

Existing Sidewalk/ ADA Issues: Existing sidewalk along SR 56 is intermittent, providing little to no pedestrian use along the SR 56 corridor. Areas of ADA non-compliance include the following:

- Sidewalk width less than 4 feet

- Sidewalk cross slope greater than 2%

- Vertical surface discontinuities greater than ½ inch

- Lack of Americans with Disabilities Act (ADA) compliant curb ramps

Existing sidewalk along SR 135 is mostly continuous along both sides of the road from SR 56 to Homer Street. Some of the sidewalks are brick, however, the majority of the sidewalks are concrete. Large trees are adjacent to the sidewalk and have caused numerous locations of upheaval. Curb ramps are not ADA compliant.

Existing Drainage: SR 56, from the west end to Cauble Street, no curbing exists and drainage is generally from east to west, conveyed in shallow roadside drainage, occasional inlets, and culvert pipes. East of Cauble Street to Posey Street, the edge of pavement is bordered by curbing. No inlets exist along the corridor until Posey Street. East of the bridge over Brock Creek, roadside drainage is via ditches, beehive inlets, and storm sewer located along the south side of the roadway. . Roadside drainage exits the project site at Highland Creek, through a 24 inch storm sewer, and along the west and east side of Brock Creek.

SR 135, from SR 56 to Harriet Street, roadside drainage is collected at the curb line and enters into inlets and storm sewer. North of Harriet Street, no curbing exists. Drainage is conveyed in shallow roadside ditches and culverts running north to the end of the project. Roadside drainage exits the project site at the south end, entering into the SR 56 storm sewer system, through a 27 inch clay tile that crosses SR 135 and drains west toward Brock Creek, through a 15 inch corrugated metal pipe (CMP) north of Homer Street, a 4 feet x 4 feet reinforced concrete box culvert, and at the north end of the project into Brock Creek.

## **PROJECT NEED AND PURPOSE**

The need for this project is based on the deteriorated conditions of the existing bridge and roadway, as described in the Existing Conditions section above.

The purpose of this project is to provide a long-term solution to the poor bridge and pavement condition while improving drainage and providing a roadway cross section that meets ADA requirements.

## **PROPOSED IMPROVEMENTS**

The proposed project includes complete pavement replacement, storm sewers, new curb, gutter, and sidewalk. The new SR 56 bridge over Brock Creek is proposed to be a 2-span (45 feet, 45 feet) haunched, reinforced concrete slab structure that will match the existing in total length. A minor profile grade raise will occur at the structure to satisfy the hydraulic and geometric requirements. The waterway opening will increase slightly from the existing since the waterway area blocked by the arches will be eliminated. The typical roadway section at the bridge will consist of two 12 feet wide travel lanes with 2 feet wide curb offset (north side), a 4 feet wide shoulder (south side), and a 6 feet – 6 inches sidewalk along the north side. The proposed bridge railing is a PS type along the north side and a PF type along the south side. The PS and PF railings are open tube type railings that will provide a view up and down the stream. The proposed clear roadway width will be 30 feet and the total out-to-out bridge width will be 38 feet – 6 inches. Approximately 60 trees located next to the pavement along SR 135 will be removed.

## **MAINTENANCE OF TRAFFIC**

Due to the tight urban conditions and full pavement replacement, it is recommended to close SR 56 and SR 135 to through traffic, setting up a state detour for each that takes some of the traffic off of the corridor. The SR 56 state detour would consist of US 150, and SR 135 (south leg, including the Salem Bypass). The SR 135 state detour would consist of SR 56, SR 39, and SR 250. These state detours would not be in effect concurrently.

The proposed state detours will help reduce traffic along the corridor, however, local traffic will need access to homes and businesses. Preliminary coordination with the city of Salem has helped identify local detours to use during construction.

SR 56 Local Detour: Market Street, starting at either Quarry Road, or Tarr Avenue (depending on segment of SR 56 under construction) is the preferred local detour. Market Street currently has an ordinance against heavy vehicles, but the city indicated that this is not due to structural reasons and could be modified during construction.

SR 135 Local Detour: Shelby Street to Sparks Ferry Road will be the SR 135 preferred local detour.

Even with State and local detours in place, providing access to the home owners and businesses will necessitate construction in segments. During the design process, the Designer will detail possible segments for construction for both SR 56 and SR 135 that ensure access, while maintaining constructability.

## **HISTORIC RESOURCES**

Based on the Indiana State Historic Preservation Office's Indiana Buildings, Bridges, and Cemeteries Map, the SR 56 Bridge over Brock Creek is listed as a "Notable" County Survey Site; however, it is listed as not eligible for the National Register of Historic Places on the Historic Bridge Inventory. The Salem Downtown Historic District is partially located adjacent to the eastern end of the project area. Over all there are five (5) "Non-Contributing"; approximately sixty-nine (69) "Contributing"; nine (9) "Notable"; and one (1) "Outstanding" County Survey Sites identified along the project area.

A field inspection of the proposed project area will be conducted by a historian to document any above-ground structures within the Area of Potential Effect (APE). A Historic Properties Report (HPR) will be written and submitted to the INDOT Cultural Resources Office (CRO), IDNR, Department of Historic Preservation & Archaeology (DHPA), and consulting parties for review and concurrence. An archaeological investigation will be conducted within any undisturbed areas to be impacted by the project and coordinated with INDOT-CRO and DHPA.

All work will be in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, CFR Part 800 (Revised January 2001) and Final Rule on Revision of Current Regulations, dated December 12, 2000, and incorporating amendments effective August 5, 2004. Investigations and recommendations will be accomplished or directly supervised by a Qualified Professional meeting the standards set forth in 36 CFR 61 or the National Historic Preservation Act and 312-IAC-21 of the Indiana Administrative Code.

## **EARLY COORDINATION**

As part of our early coordination effort for the proposed project, please study the enclosed information and provide a written evaluation of any identified potential impacts upon resources that are under your

jurisdiction. It is requested, that you return a reply within 30 days of receipt of this packet. If no reply has been received within 30 days, it will be indicated in the environmental document, which is to be prepared for the referenced project, that your agency has no comment on the project. If you have any questions, please contact Samuel P. Snell, Archaeological Principal Investigator, at 317.912.3499; sams@MetricEnv.com; or 6971 Hillside Court, Indianapolis, Indiana 46250; or Whitney Carlin, INDOT Project Manager, at 812.524.3752 or wcarlin@INDOT.IN.gov.

On behalf of INDOT,  
Metric Environmental, LLC



Samuel P. Snell  
Archaeological Principal Investigator

cc: File No. 17-0059-2  
Greg Wendling, USI Consultants, Inc.  
Whitney Carlin, INDOT Seymour District

Attachments: Location Map, USGS Topographic Map, USDA Soils Map, National Wetlands Inventory Map, Flood Insurance Rate Map, Photo Location Map, and Site Photographs

The Attachments were intentionally omitted. Please refer to Appendices B and F in the CE document.



**Early Coordination**  
**Des. No. 1600873, 1600875, 1700168, Bridge Rehabilitation (Bridge No. 056-88-1478)**  
**SR 56 over Brock Creek, SR 56, and SR 135 Road and Sidewalk Reconstruction**  
**Salem, Washington County, Indiana**

May 14, 2018

Natural Resources Conservation Service  
State Conservationist  
{rick.neilson@in.usda.gov}

Washington County Highway Department  
Rick Voyles, Highway Supervisor  
{rvoyles@washingtoncounty.in.gov}

Indiana Geological Survey  
{<https://igs.indiana.edu/eAssessment/>}

Washington County Commissioner  
Preston Shell, District Two  
{d2comm@washingtoncounty.in.gov}

Indiana Department of Transportation  
Office of Aviation  
{AFrench2@indot.in.gov}

IDEM Wellhead Proximity Determinator  
Electronic Review of Location  
{<http://www.in.gov/idem/cleanwater/pages/wellhead/>}

Midwest Regional Office  
National Parks Service  
Tokey Boswell  
{Tokey\_Boswell@nps.gov}

Federal Highway Administration  
Antonio Johnson – Seymour District  
{Antonio.Johnson@dot.gov}

Indiana Department of Natural Resources  
Division of Fish & Wildlife  
{environmentalreview@dnr.in.gov}

City of Salem, Streets and Sanitation  
Tom Voyles, Superintendent  
121 Tarr Avenue  
Salem, IN 47167

U.S. Department of Housing & Urban Develop.  
Chicago Regional Office  
Michael Wurl, Field Environmental Officer  
{Michael.E.Wurl@hud.gov}

October 2, 2019  
Floodplain Administrator  
{ron.voyles@cityofsalemin.com}

Indiana Department of Environmental  
Management  
Proposed Roadway Construction Projects Letter  
{<http://www.in.gov/idem/5284.htm>}

October 4, 2019  
IDEM Groundwater Section  
{sblazey@idem.IN.gov}

Indiana Department of Transportation  
Rickie Clark, Public Hearings Manager  
{rclark@indot.in.gov}

October 23, 2019  
Salem Parks and Recreation  
{brande.abbott@cityofsalemin.com}

USACE, Louisville District  
{gregory.a.mckay@usace.army.mil}

October 23, 2019  
Christian Life Baptist Church  
1311 W. Mulberry St., Salem, IN 47167

Washington County Surveyor's Office  
Diana Green  
{digreen@washingtoncounty.in.gov}

THIS IS NOT A PERMIT

State of Indiana  
DEPARTMENT OF NATURAL RESOURCES  
Division of Fish and Wildlife  
Early Coordination/Environmental Assessment

DNR #: ER-20563

Request Received: May 14, 2018

**Requestor:** Metric Environmental  
Samuel P Snell  
6971 Hillsdale Court  
Indianapolis, IN 46250

**Project:** SR 56 bridge (#056-88-01478) replacement over Brock Creek, Des #1700168; and SR 56 and SR 135 road and sidewalk reconstruction, Des #s 1600873 & 1600875; Salem

**County/Site info:** Washington

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

**Regulatory Assessment:** Pursuant to the Flood Control Act (IC 14-28-1) any proposal to construct, excavate, or fill in or on the floodway of a stream or other flowing waterbody which has a drainage area greater than one square mile will require the formal approval for construction in a floodway under the Flood Control Act, IC 14-28-1. Please submit a copy of this letter with the permit application.

**Natural Heritage Database:** The Natural Heritage Program's data have been checked. The American Badger (*Taxidea taxus*), a state species of special concern, has been documented within 1/2 mile southwest of the project area.

**Fish & Wildlife Comments:** Badgers are a wide ranging species that prefer an open, prairie-type habitat, with Indiana being at the eastern edge of their natural range. The range of the badger continues to expand as a result of land-use changes from forest to farmland and open pastureland. Impacts to the American badger or its preferred habitat are unlikely as a result of this project.

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Stream Crossing & Wildlife Passage:

The new, replacement, or rehabbed structure, and any bank stabilization under the structure, should not create conditions that are less favorable for wildlife passage under the structure compared to the current conditions. Riprap must not be placed in the active thalweg channel or placed in the streambed in a manner that precludes fish or aquatic organism passage (riprap must not be placed above the existing streambed elevation). Riprap may be used only at the toe of the sideslopes up to the ordinary high water mark (OHWM). The banks above the OHWM must be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to Southern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion.

Where hard armoring is needed, wildlife passage can be facilitated by using a smooth-surfaced material instead of riprap, such as articulated concrete block mats, fabric-formed concrete mats or other similar smooth-surfaced materials as these

**State of Indiana**  
**DEPARTMENT OF NATURAL RESOURCES**  
**Division of Fish and Wildlife**  
**Early Coordination/Environmental Assessment**

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materials will not impair wildlife movement.

Information about bioengineering techniques can be found at <http://www.in.gov/legislative/iac/20120404-IR-312120154NRA.xml.pdf>. Also, the following is a USDA/NRCS document that outlines many different bioengineering and other bank stabilization techniques: <http://directives.sc.gov.usda.gov/17553.wba>.

1) Riparian Habitat:

We recommend a mitigation plan be developed (and submitted with the permit application) for any unavoidable habitat impacts that will occur. The DNR's Floodway Habitat Mitigation guidelines (and plant lists) can be found online at: <http://www.in.gov/legislative/iac/20140806-IR-312140295NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees). Impacts to wetland habitat should be mitigated at the appropriate ratio according to the 1991 INDOT/IDNR/USFWS Memorandum of Understanding.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas in the floodway with a mixture of native grasses, sedges, wildflowers, and also native hardwood trees and shrubs as soon as possible upon completion. Do not use any varieties of Tall Fescue or other non-native plants (e.g. crown-vetch).
2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 3 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
5. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure.
6. Do not construct any temporary runarounds or causeways.
7. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
8. Plant native hardwood trees along the top of the bank and right-of-way to replace the vegetation destroyed during construction.
9. Post "Do Not Mow or Spray" signs along the right-of-way.
10. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
11. Seed and protect disturbed streambanks that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net-free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and mulch all disturbed areas not protected by other methods.

THIS IS NOT A PERMIT

State of Indiana  
DEPARTMENT OF NATURAL RESOURCES  
Division of Fish and Wildlife  
Early Coordination/Environmental Assessment

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**Contact Staff:**

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife  
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



**Date:** June 13, 2018

Christie L. Stanifer  
Environ. Coordinator  
Division of Fish and Wildlife



# Indiana Department of Environmental Management

*We Protect Hoosiers and Our Environment.*

100 North Senate Avenue - Indianapolis, IN 46204  
(800) 451-6027 - (317) 232-8603 - [www.idem.IN.gov](http://www.idem.IN.gov)

Indiana Department of Transportation  
Greg Prince  
185 Agrico Lane  
Seymour , IN 47274  
Date

Metric Environmental, LLC  
Susan Castle  
6971 Hillside Court  
Indianapolis , IN 46250

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: Pavement Replacement, including pedestrian and storm water improvements along SR 56 (Des. No. 1600873) from 0.09 mile east of W. Jct. of SR 60 to 0.08 mile east of SR 135 (High Street), Resurfacing SR 135, including minor pedestrian improvements (Des. No. 1600875) from SR 56 to 0.72 mile north of SR 56, and a Bridge Project on SR 56, 0.20 mile west of SR 135, over Brock Creek (Des. No. 1700168).

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

## WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other

waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana ) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana . A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should

seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.

5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the follow statutes:
  - IC 14-26-2 Lakes Preservation Act 312 IAC 11
  - IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
  - IC 14-28-1 Flood Control Act 310 IAC 6-1
  - IC 14-29-1 Navigable Waterways Act 312 IAC 6
  - IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
  - IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>) . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
  - <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq> (<http://www.in.gov/idem/4917.htm#constreq>)), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

## AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations.

Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm> (<http://www.in.gov/idem/4148.htm>)) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you



must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>).

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit:

[http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon\\_testers\\_mitigators\\_list.pdf](http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf)

([http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon\\_testers\\_mitigators\\_list.pdf](http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf).) It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit:

<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>

(<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm>

(<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html>

(<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or

asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978 , or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2 , Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF> (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: [www.ai.org/legislative/iac/t03260/a00020.pdf](http://www.ai.org/legislative/iac/t03260/a00020.pdf) (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.

7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

## LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

## FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm> (<http://www.in.gov/idem/5284.htm>), is used.

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## Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

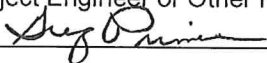
## Project Description

Pavement Replacement, including pedestrian and storm water improvements along SR 56 (Des. No. 1600873) from 0.09 mile east of W. Jct. of SR 60 to 0.08 mile east of SR 135 (High Street), Resurfacing SR 135, including minor pedestrian improvements (Des. No. 1600875) from SR 56 to 0.72 mile north of SR 56, and a Bridge Project on SR 56, 0.20 mile west of SR 135, over Brock Creek (Des. No. 1700168).

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 10-21-19

Signature of the INDOT  
Project Engineer or Other Responsible Agent



Greg Prince

Date: 10-21-19

Signature of the  
For Hire Consultant



Susan Castle



## Organization and Project Information

<b>Project ID:</b>	17-0059-11
<b>Des. ID:</b>	1600873 etal
<b>Project Title:</b>	SR 56 and SR 135 Small Town Pavement Replacement and Bridge Replacement, Salem, Washington
<b>Name of Organization:</b>	Metric Environmental, LLC
<b>Requested by:</b>	Susan Castle

## Environmental Assessment Report

### 1. Geological Hazards:

- Potential Karst
- Floodway

### 2. Mineral Resources:

- Bedrock Resource: High Potential
- Sand and Gravel Resource: None documented in the area

### 3. Active or abandoned mineral resources extraction sites:

- Petroleum Exploration Wells
- Abandoned Industrial Minerals Quarries

\*All map layers from Indiana Map ([maps.indiana.edu](http://maps.indiana.edu))

### DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

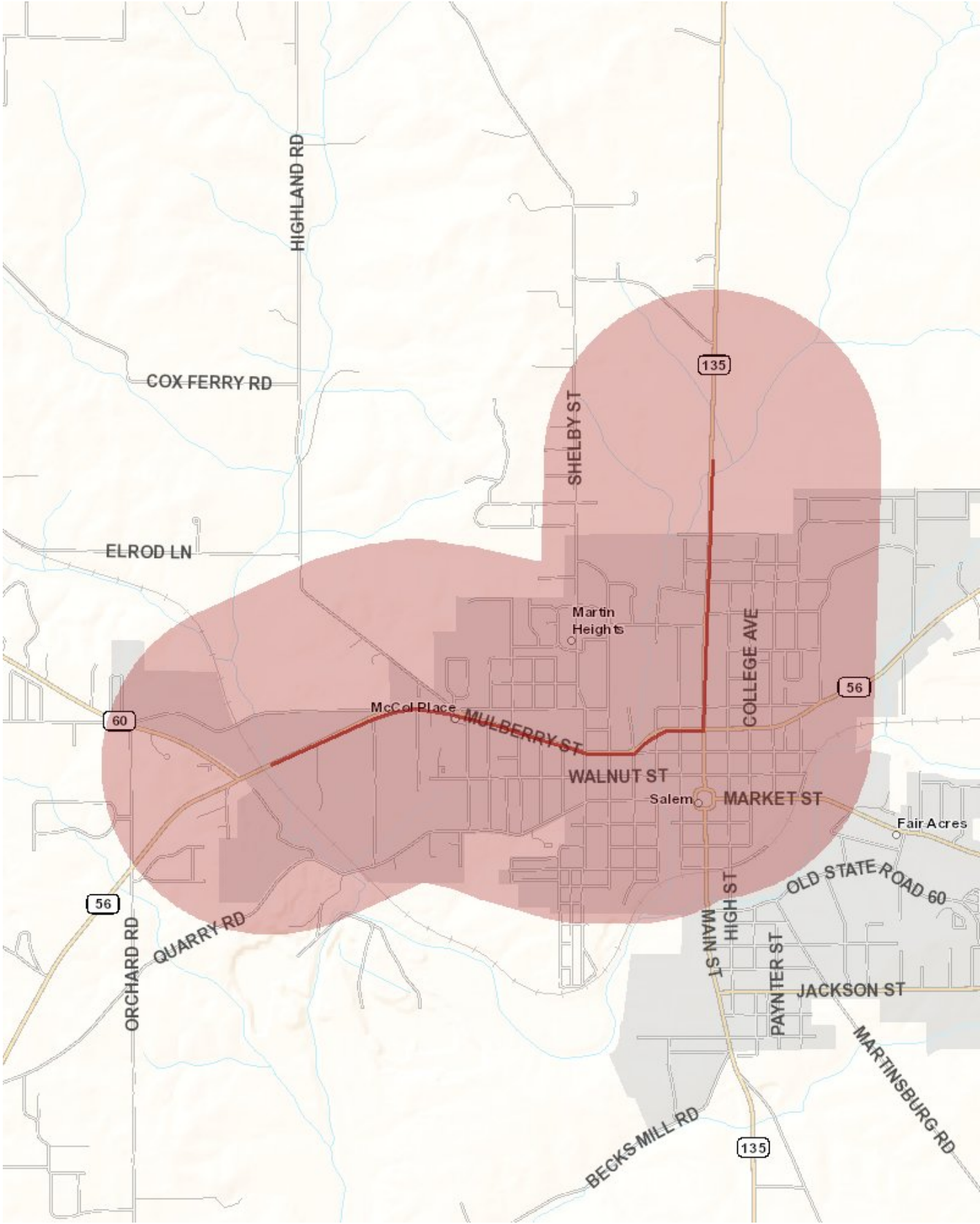
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: June 06, 2019



# Metadata:

- [https://maps.indiana.edu/metadata/Hydrology/Karst\\_Cave\\_Density.html](https://maps.indiana.edu/metadata/Hydrology/Karst_Cave_Density.html)
- [https://maps.indiana.edu/metadata/Geology/Petroleum\\_Wells.html](https://maps.indiana.edu/metadata/Geology/Petroleum_Wells.html)
- [https://maps.indiana.edu/metadata/Geology/Industrial\\_Minerals\\_Quarries\\_Abandoned.html](https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Quarries_Abandoned.html)
- [https://maps.indiana.edu/metadata/Hydrology/Floodplains\\_FIRM.html](https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html)
- [https://maps.indiana.edu/metadata/Geology/Bedrock\\_Geology.html](https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html)



# United States Department of the Interior



## FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

February 08, 2019

Consultation Code: 03E12000-2019-SLI-0509

Event Code: 03E12000-2019-E-02296

Project Name: Des. Nos. 1600873, 1600875, & 1700168, Small Town Reconstruction Project, Washington County, IN

Subject: List of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you



determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

## Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

**Indiana Ecological Services Field Office**

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

## Project Summary

Consultation Code: 03E12000-2019-SLI-0509

Event Code: 03E12000-2019-E-02296

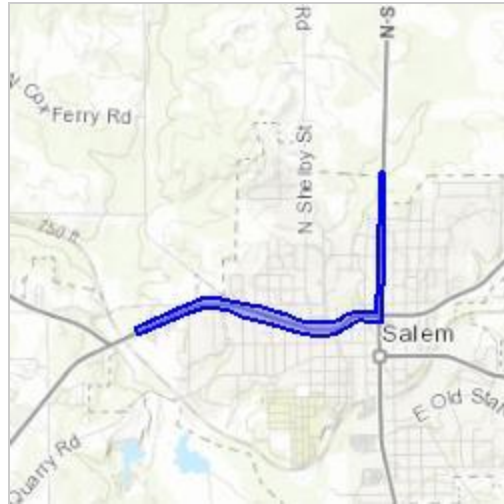
Project Name: Des. Nos. 1600873, 1600875, & 1700168, Small Town Reconstruction Project, Washington County, IN

Project Type: TRANSPORTATION

Project Description: The City of Salem, a Local Public Agency (LPA), proposes to proceed with a small town reconstruction project along SR 56 and SR 135 in Salem, Washington County, Indiana. The project is broken up into three Des. Nos. (1600873, 1600875, and 1700168). The project included complete pavement replacement, storm sewers, new curb, gutter, and sidewalk. The new SR 56 bridge over Brock Creek is proposed to be a 2-span (45'-0", 45'-0") haunched, reinforced concrete slab structure that will match the existing bridge in total length. A minor profile grade raise will occur at the structure to satisfy the hydraulic and geometric requirements. A typical roadway section at the bridge will consist of two 12 ft. wide travel lanes with 2 ft. wide curb offset (north side), a 4 ft. wide shoulder (south side), and a 6.5 ft. sidewalk along the north side. Suitable summer habitat is located within all quadrants of the project area. Approximately 5.4 acres of trees will be removed from the project area. The USFWS database was checked on February 9, 2018 and the review did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. All suitable summer habitat will be removed during the inactive season when endangered bats are not expected to be roosting. Construction is expected to extend from September 2020 to December 2021. If night work is necessary, temporary lighting will be used throughout the project area. However, no permanent lighting will be installed as a result of this project.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/place/38.61328226741236N86.10064469526964W>



Counties: Washington, IN

## Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries<sup>1</sup>, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

- 
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

## Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is <b>final</b> critical habitat for this species. Your location is outside the critical habitat. Species profile: <a href="https://ecos.fws.gov/ecp/species/5949">https://ecos.fws.gov/ecp/species/5949</a> Species survey guidelines: <a href="https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf">https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf</a>	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> <li>▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See <a href="http://www.fws.gov/midwest/endangered/mammals/nleb/index.html">www.fws.gov/midwest/endangered/mammals/nleb/index.html</a></li> </ul> Species profile: <a href="https://ecos.fws.gov/ecp/species/9045">https://ecos.fws.gov/ecp/species/9045</a> Species survey guidelines: <a href="https://ecos.fws.gov/ipac/guideline/survey/population/10043/office/31440.pdf">https://ecos.fws.gov/ipac/guideline/survey/population/10043/office/31440.pdf</a>	Threatened

## Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



## United States Department of the Interior



### FISH AND WILDLIFE SERVICE

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Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

February 11, 2019

Consultation Code: 03E12000-2019-I-0509

Event Code: 03E12000-2019-E-02370

Project Name: Des. Nos. 1600873, 1600875, & 1700168, Small Town Reconstruction Project, Washington County, IN

Subject: Concurrence verification letter for the 'Des. Nos. 1600873, 1600875, & 1700168, Small Town Reconstruction Project, Washington County, IN' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated to verify that the **Des. Nos. 1600873, 1600875, & 1700168, Small Town Reconstruction Project, Washington County, IN** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may

identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

**For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities:** If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

## Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

### Name

Des. Nos. 1600873, 1600875, & 1700168, Small Town Reconstruction Project, Washington County, IN

### Description

The City of Salem, a Local Public Agency (LPA), proposes to proceed with a small town reconstruction project along SR 56 and SR 135 in Salem, Washington County, Indiana. The project is broken up into three Des. Nos. (1600873, 1600875, and 1700168). The project included complete pavement replacement, storm sewers, new curb, gutter, and sidewalk. The new SR 56 bridge over Brock Creek is proposed to be a 2-span (45'-0", 45'-0") haunched, reinforced concrete slab structure that will match the existing bridge in total length. A minor profile grade raise will occur at the structure to satisfy the hydraulic and geometric requirements. A typical roadway section at the bridge will consist of two 12 ft. wide travel lanes with 2 ft. wide curb offset (north side), a 4 ft. wide shoulder (south side), and a 6.5 ft. sidewalk along the north side. Suitable summer habitat is located within all quadrants of the project area. Approximately 5.4 acres of trees will be removed from the project area. The USFWS database was checked on February 9, 2018 and the review did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. All suitable summer habitat will be removed during the inactive season when endangered bats are not expected to be roosting. Construction is expected to extend from September 2020 to December 2021. If night work is necessary, temporary lighting will be used throughout the project area. However, no permanent lighting will be installed as a result of this project.



## Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat. Therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

## Qualification Interview

1. Is the project within the range of the Indiana bat<sup>[1]</sup>?

[1] See [Indiana bat species profile](#)

**Automatically answered**

*Yes*

2. Is the project within the range of the Northern long-eared bat<sup>[1]</sup>?

[1] See [Northern long-eared bat species profile](#)

**Automatically answered**

*Yes*

3. Which Federal Agency is the lead for the action?

*A) Federal Highway Administration (FHWA)*

4. Are *all* project activities limited to non-construction<sup>[1]</sup> activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

*No*

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces<sup>[1]</sup>?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

*No*

6. Does the project include *any* activities **within** 0.5 miles of an Indiana bat and/or NLEB hibernaculum<sup>[1]</sup>?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

*No*

7. Is the project located **within** a karst area?

*Yes*

8. Will the project include *any* type of activity that could impact a **known** hibernaculum<sup>[1]</sup>, or impact a karst feature (e.g., sinkhole, losing stream, or spring) that could result in effects to a **known** hibernaculum?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

*No*

9. Is there *any* suitable<sup>[1]</sup> summer habitat for Indiana Bat or NLEB **within** the project action area<sup>[2]</sup>? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

*Yes*

10. Will the project remove *any* suitable summer habitat<sup>[1]</sup> and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

*Yes*

11. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

*No*

12. Have presence/probable absence (P/A) summer surveys<sup>[1][2]</sup> been conducted<sup>[3][4]</sup> **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

*No*

13. Does the project include activities **within documented Indiana bat habitat**<sup>[1][2]</sup>?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

*No*

14. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

*Yes*

15. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur<sup>[1]</sup>?

[1] Coordinate with the local Service Field Office for appropriate dates.

*B) During the inactive season*

16. Does the project include activities **within documented NLEB habitat**<sup>[1][2]</sup>?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

*No*

17. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

*Yes*

18. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

*B) During the inactive season*

19. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

*Yes*

20. Will the tree removal alter *any* **documented** Indiana bat or NLEB roosts and/or alter any surrounding summer habitat **within** 0.25 mile of a documented roost?

*No*

21. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

*No*

22. Are *all* trees that are being removed clearly demarcated?

*Yes*

23. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

*No*

24. Does the project include maintenance of the surrounding landscape at existing facilities (e.g., rest areas, stormwater detention basins)?

*No*

25. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

*No*

26. Does the project include slash pile burning?

*No*

27. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

*Yes*

28. Is there *any* suitable habitat<sup>[1]</sup> for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

*Yes*

29. Has a bridge assessment<sup>[1]</sup> been conducted **within** the last 24 months<sup>[2]</sup> to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

*Yes*

#### **SUBMITTED DOCUMENTS**

- *Bat Structure Assessment Form 10-5-17.pdf* <https://ecos.fws.gov/ipac/project/KFEVVKRZXBKHK55KY43U7HX3Y/projectDocuments/15288253>

30. Did the bridge assessment detect *any* signs of bats roosting in/under the bridge (bats, guano, etc.)?

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

*No*

31. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

*No*

32. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

*No*

33. Will the project involve the use of **temporary** lighting *during* the active season?

*Yes*

34. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

*Yes*

35. Will the project install new or replace existing **permanent** lighting?

*No*

36. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

*Yes*

37. Will the activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the active season<sup>[1]</sup>?

[1] Coordinate with the local Service Field Office for appropriate dates.

*No*

38. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge or structure removal, replacement, and/or maintenance, lighting, or use of percussives, limited to actions that DO NOT cause any stressors to the bat species, including as described in the BA/BO (i.e. activities that do not involve ground disturbance, percussive noise, temporary or permanent lighting, tree removal/trimming, nor bridge/structure activities)?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

*Yes*

39. Will the project raise the road profile **above the tree canopy**?

*No*

40. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) and/or increase noise levels above existing traffic/background levels consistent with a No Effect determination in this key?

**Automatically answered**

*Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, and conducted during the inactive season*

41. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

**Automatically answered**

*Yes, because the tree removal/trimming that occurs outside of the active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost*

42. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

**Automatically answered**

*Yes, because the tree removal/trimming that occurs outside of the active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost*

43. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

**Automatically answered**

*Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected*

44. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

*Yes*

45. **Hibernacula AMM 1**

Will the project ensure that on-site personnel will use best management practices<sup>[1]</sup>, secondary containment measures, or other standard spill prevention and countermeasures to avoid impacts to possible hibernacula?

[1] Coordinate with the appropriate Service Field Office on recommended best management practices for karst in your state.

*Yes*

46. **Hibernacula AMM 1**

Will the project ensure that, where practicable, a 300 foot buffer will be employed to separate fueling areas and other major containment risk activities from caves, sinkholes, losing streams, and springs in karst topography?

*Yes*

47. **Tree Removal AMM 1**

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal<sup>[1]</sup> in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

*Yes*



**48. Tree Removal AMM 2**

Can *all* tree removal activities be restricted to when Indiana bats are not likely to be present (e.g., the inactive season)<sup>[1]</sup>?

[1] Coordinate with the local Service Field Office for appropriate dates.

**Automatically answered**

*Yes*

**49. Tree Removal AMM 2**

Can *all* tree removal activities be restricted to when Northern long-eared bats are not likely to be present (e.g., the inactive season)<sup>[1]</sup>?

[1] Coordinate with the local Service Field Office for appropriate dates.

**Automatically answered**

*Yes*

**50. Tree Removal AMM 3**

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

*Yes*

**51. Tree Removal AMM 4**

Can the project avoid cutting down/removal of *all* (1) **documented**<sup>[1]</sup> Indiana bat or NLEB roosts<sup>[2]</sup> (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

*Yes*

**52. Lighting AMM 1**

Will *all* **temporary** lighting used during the removal of suitable habitat and/or the removal/trimming of trees within suitable habitat be directed away from suitable habitat during the active season?

*Yes*

**53. Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

*Yes*

## Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

*Yes*

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

*No*

3. How many acres<sup>[1]</sup> of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

*5.4*

4. Please describe the proposed bridge work:

*The existing structure will be removed and replaced*

5. Please state the timing of all proposed bridge work:

*September 2020 to December 2021*

## Avoidance And Minimization Measures (AMMs)

These measures **were accepted** as part of this determination key result:

#### GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

#### HIBERNACULA AMM 1

For projects located within karst areas, on-site personnel will use best management practices, secondary containment measures, or other standard spill prevention and countermeasures to avoid impacts to possible hibernacula. Where practicable, a 300 foot buffer will be employed to separate fueling areas and other major containment risk activities from caves, sinkholes, losing streams, and springs in karst topography.

#### LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

#### TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

#### TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

#### TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

#### TREE REMOVAL AMM 4

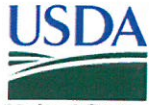
Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

## **Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat**

This key was last updated in IPaC on March 16, 2018. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.



United States Department of Agriculture

Natural Resources Conservation Service  
Indiana State Office  
6013 Lakeside Boulevard  
Indianapolis, Indiana 46278  
317-290-3200

May 21, 2018

Samuel P. Snell  
Archaeological Principal Investigator  
Metric Environmental  
6971 Hillside Court  
Indianapolis, Indiana 46250

Dear Mr. Snell:

The proposed project to reconstruct the sidewalk and road in Salem, Washington County, Indiana (Des. Nos 1600873, 1600875, 1700168), as referred to in your letter received May 11, 2018, will not be able to be determined at this time due to a lack of site specific information. Please resubmit when specific/potential impacts are determined.

If you need additional information, please contact Rick Neilson at 317-295-5875.

Sincerely,

GERALD L. ROACH  
Acting State Conservationist

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## Susan Castle

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**Subject:** FW: Des. No. 1600873 (Lead) SR 56 Pavement Replacement, 1600875 SR 135 Resurfacing Projects, and 1700168 Bridge replacement over Brock Creek, Salem, IN  
**Attachments:** Plan Sheets for Salem Parks and Recreation.pdf

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**From:** Susan Castle  
**Sent:** Wednesday, October 23, 2019 3:25 PM  
**To:** [brande.abbott@cityofsalemin.com](mailto:brande.abbott@cityofsalemin.com)  
**Subject:** Des. No. 1600873 (Lead) SR 56 Pavement Replacement, 1600875 SR 135 Resurfacing Projects, and 1700168 Bridge replacement over Brock Creek, Salem, IN

Hi Brande,

We are preparing the environmental document for the above referenced project. Based on our on-line review, there are two potential planned trails which may intersect or be constructed adjacent to this project.

- Salem Community Trail extension south from schools, is a potential urban trail managed by the Salem Park and Recreation Board that may intersect the project area at the east side of Posey Street.
- Salem Community Trail Courthouse, Fairgrounds, Community Park Trail, is a potential asphalt/concrete urban trail managed by the Salem Park and Recreation Board that may intersect and run adjacent to the west side of SR 135.

During this project a sidewalk and modified combined concrete curb and gutter will be constructed in the northeast quadrant of SR 56 at Posey Street and a combined concrete curb and gutter will be constructed on the southeast quadrant of SR 56 at Posey Street. Two detectable warning surfaces will be installed on the west side of SR 135 at Salem Avenue. At the northeast, northwest and southwest quadrants of SR 135 at Homer Street new modified combined concrete curb and gutter and / or concrete curb will be installed.

Three plan sheets are attached indicating the planned improvements and locations.

Will any of these construction activities impact the proposed planned trails?

Thank you and I look forward to hearing from you.

**Susan Castle**  
NEPA Senior Technical Consultant

Phone: 317.608.2730  
Mobile: 317.379.3649  
6971 Hillside Court, Indianapolis, IN 46250

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# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N955  
Indianapolis, Indiana 46204

PHONE: (317) 232-1477  
FAX: (317) 232-1499

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

May 29, 2018

Mr. Samuel P. Snell, Archaeological Principal Investigator  
Metric Environmental  
6971 Hillsdale Court  
Indianapolis, IN 46250

Subject: Early Coordination Review (*Des. No. 1600873, 1600875, & 1700168,*)

Dear Mr. Snell,

In response to your request on May 14, 2018 for early coordination review of road & sidewalk reconstruction and a bridge replacement project over Brock Creek in Salem, Washington County, Indiana; the Indiana Department of Transportation, Office of Aviation has reviewed the information and provides the following:

**Are there any existing or proposed public-use airports within 5 nautical miles of the project limits (IC 8-21-10-6)?**

The Salem Municipal Airport is located beyond .06 nautical miles southwest of the westernmost point of the proposed project site.

**Will an Indiana Tall Structure permit (IC 8-21-10-3-a) and/or Noise Sensitive (IC 8-21-10-3-b) permit be required?**

Based upon the provided information, an Indiana Tall Structure permit would not be required unless the project involves the construction of a temporary (e.g., crane) or permanent structure that penetrates a 100:1 slope from the nearest point of the Salem Municipal Airport runway.

For any questions related to Indiana Tall Structure and/or Noise Sensitive permitting, please contact James Kinder at (317) 232-1485 or [jkinder2@indot.in.gov](mailto:jkinder2@indot.in.gov).

Sincerely,

Adam French, MPA  
Chief Airport Inspector, Office of Aviation  
Indiana Department of Transportation

**APPENDIX D**  
**Section 106 of the National Historic**  
**Preservation Act**





# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

June 27, 2019

This letter was sent to the listed parties.

RE: Small Town Pavement Replacement and Bridge Project (Des. Nos. 1600873, 1600875, 1700168);  
DHPA No. 22240

Dear Consulting Party,

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with Small Town Pavement Replacement and Bridge Project (Des. Nos. 1600873, 1600875, 1700168). Green 3, LLC is under contract with INDOT to advance the environmental documentation for the referenced project.

This letter is part of the Section 106 review process for this project. A Section 106 early coordination letter was distributed on February 27, 2019.

The proposed undertaking is on Main Street/SR 135 and SR 56 from the CSX Railroad to College Avenue on SR 56 and from SR 56 to the bridge on Main Street/SR 135 in Washington County, Indiana. It is within Washington Township, Salem and Becks Mill Quads, Sections 17 and 18, Township 2 North, Range 4 East and Section 13, Township 2 North, Range 3 East.

The purpose of the project is to improve the pavement condition, improve safety on the road and bridge, and provide ADA compliance on the sidewalk. It is not known at this time if right-of-way will be acquired for the project. The need for the project DES. Nos. 1600873 and 1600875 is due to the poor pavement conditions of SR 56 and SR 135, as well as areas where sidewalks exist that do not comply with ADA requirements. In respect to DES. No. 1700168, INDOT Bridge No. 056-88-01478, NBI No. 20180, is narrow, and in poor condition. The project consists of three parts. Pavement Replacement Project along SR 56 (Des. No. 1600873) will consist of a pavement replacement, storm sewers, new curb, gutter and sidewalks, and ramps in compliance with the Americans with Disabilities Act (ADA) along SR 56. The vertical alignment is being modified in order to best fit the existing corridor with some grade changes anticipated. The project will provide continuous sidewalk along SR 56 from the east end of the project to the extent sidewalk (Spring Grove Road). Continuous sidewalk will be constructed along one side of the roadway, and a graded grass area will be provided on the other side of the roadway. The project includes replacing the existing SR 56 bridge over Brock Creek (Des. No. 1700168). The new bridge is proposed to be a two-span (45 feet each), haunched, reinforced concrete slab structure that will match the existing bridge in total length. A minor profile grade will occur at the structure to satisfy the hydraulic and geometric requirements. The waterway opening will increase slightly from the existing since the waterway area blocked by the arches will be eliminated. The typical roadway section at the bridge will consist of two twelve-foot travel lanes with a two-foot curb offset (north side), a four-foot shoulder (south side),

and a six-foot six-inch sidewalk along the north side. The railings are an open tube type that will provide a view up and down the stream. The proposed clear roadway width will be 30 feet and the total out-to-out bridge width will be 38 feet, 6 inches. The Pavement Replacement Project along Main St./SR 135 (Des. No. 1600875) will consist of milling 1.5 inches of Hot Mix Asphalt and replacing with 1.5 inches of HMA surface course and reconstructing curb ramps. Permanent right-of-way acquired is anticipated to be 0.32 acre on SR 56 (31 parcels) and 0.1 acre (one parcel) along SR 135. Total temporary right-of-way will be 0.47 acre on SR 56 and 0.14 acre on SR 135. Temporary right-of-way will be required in the North Main Street Historic District in two locations.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process. Entities that have accepted consulting party status are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains the Salem Downtown Historic District listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, the North Main Street Historic District, IHSSI #s 175-567-27001-050; the Schulz/Gladden House, #175-567-27029, and the Warder and Alice Stevens House #175-567-G2 and the Ray and Hester Ault House, #175-567-G29 were recommended eligible for listing in the NRHP.

With regards to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified #0 (no) sites within the project area. As a result of these efforts, no further work is recommended.

The effects finding and 800.11(e) documentation is available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard copy of this material, please respond to this email with your request within seven (7) days.

Please review the information and comment within thirty (30) calendar days of receipt.

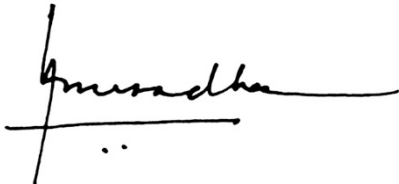
For questions concerning specific project details, you may contact Connie Zeigler at (317) 634-4110 or e-mail to [connie@green3studio.com](mailto:connie@green3studio.com). All future responses regarding the proposed project should be forwarded to Green 3, LLC at the following address:

Connie Zeigler  
Architectural Historian

Green 3 LLC  
1104 Prospect Street  
Indianapolis, IN 46203  
[connie@green3studio.com](mailto:connie@green3studio.com)

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager  
Cultural Resources Office  
Environmental Services

Enclosures:

Topographic Map **The Topographic Map was intentionally omitted from this letter. Please refer to page D-20 in this document.**

Distribution List:

Indiana State Historic Preservation Officer  
INDOT-CRO  
Michelle Allen, FHWA  
Indiana Landmarks, Southern Regional Office  
Washington County Historian  
Washington County Historical Society  
Delaware Tribe of Indians, Oklahoma  
Miami Tribe of Oklahoma

## Delaware Nation Response

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July 22, 2019

To Whom It May Concern:

The Delaware Nation Historic Preservation Department received correspondence regarding the following referenced project(s).

**Project: Des. No.: 1600873, 1600875, 1700168**

Our office is committed to protecting tribal heritage, culture and religion with particular concern for archaeological sites potentially containing burials and associated funerary objects.

The Lenape people occupied the area indicated in your letter during prior to European contact until their eventual removal to our present locations. According to our files, the location of the proposed project does not endanger cultural, or religious sites of interest to the Delaware Nation. **Please continue with the project as planned** keeping in mind during construction should an archaeological site or artifacts inadvertently be uncovered, all construction and ground disturbing activities should immediately be halted until the appropriate state agencies, as well as this office, are notified (within 24 hours), and a proper archaeological assessment can be made.

Please note the Delaware Nation, the Delaware Tribe of Indians, and the Stockbridge Munsee Band of Mohican Indians are the only Federally Recognized Delaware/Lenape entities in the United States and consultation must be made only with designated staff of these three tribes. We appreciate your cooperation in contacting the Delaware Nation Cultural Preservation Office to conduct proper Section 106 consultation. Should you have any questions, feel free to contact our offices at 405-247-2448 ext. 1403.



Erin Thompson  
Director of Historic Preservation  
Delaware Nation  
31064 State Highway 281  
Anadarko, OK 73005  
Ph. 405-247-2448 ext. 1403  
ethompson@delawarenation-nsn.gov



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739  
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



July 26, 2019

Connie Zeigler  
Green 3, LLC  
1104 Prospect Street  
Indianapolis, Indiana 46203

Federal Agency: Indiana Department of Transportation (“INDOT”),  
on behalf of Federal Highway Administration (“FHWA”)

Re: Indiana Department of Transportation’s finding of “no adverse effect,” on behalf of the Federal Highway Administration, for pavement replacement along SR 135 and SR 56 and replacement of Bridge No. 056-88-01478, in the City of Salem, Washington Township, Washington County, Indiana (Des. No. 160073, 1600875, and 1700168; DHPA No. 22240)

Dear Ms. Zeigler:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, the, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding that Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana Historic Preservation Officer (“Indiana SHPO”) has reviewed Green 3 LLC’s cover letter and review request submittal form, enclosing INDOT’s June 27, 2019 finding and documentation, which we received on June 28, 2019, for the pavement and bridge replacement project in the City of Salem, Washington Township, Washington County.

As previously indicated, in terms of archaeology, there are no currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places (“NRHP”) within the proposed project area; and it is our opinion that no further archaeological investigations appear necessary at this proposed project area. However, this identification is subject to the project activities remaining within areas disturbed by previous construction of a recent and non-historical nature. If archaeological deposits are encountered from the post-contact period, they will be evaluated regarding their eligibility for the NRHP in consultation with the staff of the Indiana SHPO. Please contact our office if such deposits are encountered. The archaeological recording must be done in accordance with the Secretary of the Interior’s “Standards and Guidelines for Archaeology and Historic Preservation” (48 F.R. 44716) and a report of the archaeological documentation must be submitted to our office for review and comment.

We agree with the individual findings that the Salem Downtown Historic District (NR-1345), North Main Street Historic District (IHSSI Site # 175-567-27001-050) which includes the Schulz/Gladden House at 505 N. Main Street (IHSSI Site #175-567-27029); Warder and Alice Stevens House at 1201 N. Main Street (Green3 # 175-567-G2); and Ray and Hester Ault House at 1116 W. Mulberry Street (Green3 #175-567-G29) are the only historic properties within the area of potential effects of this project and that they will not be adversely affected.

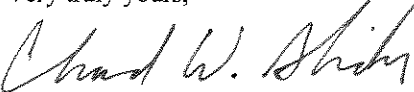
Accordingly, we concur with INDOT’s June 27, 2019, Section 106 finding, on behalf of FHWA, of No Adverse Effect for pavement replacement along SR 135 and SR 56 and replacement of Bridge No. 056-88-01478 (‘Small Town Pavement Replacement and Bridge Project’) in the City of Salem, Washington Township, Washington County.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The reviewers of this project on the Indiana SHPO staff are Wade Tharp for archaeology and Chad Slider for structures. If you have questions about the status of a review, about the review process, or about what to submit for review, please contact the assigned reviewer at INDOT's Cultural Resources Office.

If there is any future correspondence regarding the pavement and bridge replacement project in the City of Salem, Washington Township, Washington County (Des. Nos. 160073, 1600875, 1700168), please refer to DHPA No. 22240.

Very truly yours,



Beth K. McCord  
Deputy State Historic Preservation Officer

BKM:WTT:CWS:cws

emc: Anuradha Kumar, INDOT  
Shaun Miller, INDOT  
Susan Branigin, INDOT  
Mary Kennedy, INDOT  
Connie Zeigler, Green3

## Small Town Pavement Replacement and Bridge Project



Salem, Washington Township, Washington County, Indiana  
Des. Nos. 1600873, 1600875, 1700168

800.11(e) Documentation and Effects Finding  
June 27, 2019

Prepared for:  
Metric Environmental  
6971 Hillsdale Court  
Indianapolis, IN 46250

Connie Zeigler Historian/QP  
Green3 LLC  
1104 Prospect Street Indianapolis, IN 46203



p. 317.634.4110

f. 866.422.2046

email: [connie@green3studio.com](mailto:connie@green3studio.com)

**FEDERAL HIGHWAY ADMINISTRATION'S  
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND  
SECTION 106 FINDINGS AND DETERMINATIONS  
AREA OF POTENTIAL EFFECT  
ELIGIBILITY DETERMINATIONS  
EFFECT FINDING  
Small Town Pavement Replacement and Bridge Project  
SALEM, WASHINGTON COUNTY, INDIANA  
DES. Nos. 1600873, 1600875, 1700168  
DHPA NO. 22240**

**AREA OF POTENTIAL EFFECTS  
(Pursuant to 36 CFR Section 800.4(a)(1))**

The APE of the project includes all properties adjacent to the project and those with a proximate viewshed of the project. Forested areas, rolling landscape and bends in the road limited the APE. The APE is approximately 2.8 miles in length and approximately 0.14-mile wide at its widest point, near the eastern terminus of the project on SR 56. See Appendix A for a map of the APE.

**ELIGIBILITY DETERMINATIONS  
(Pursuant to 36 CFR 800.4(c)(2))**

- **Ray and Hester Ault House # 175-567-G29**, a one-story flat-roof Modern style house constructed of the unusual material, McCorkle block, is eligible under Criterion C for its use of unusual material and modern architecture, with a nod to International Style design.
- **Salem Downtown Historic District** (listed in the National Register (NRHP) in 1996), IHSSI #s 175-567-26001-275, has early commercial and governmental buildings and residential areas from the mid-nineteenth to mid-twentieth centuries. NRHP- Listed under Criteria A and C.
- **North Main Street Historic District** IHSSI #s 175-567-27001-050, is residential with houses dating from the 1850s to 1960s in styles ranging from Greek Revival to Ranch. The district is eligible under Criteria A, for its association with the development of Salem, and C, for its intact architecture.
- **Schulz/Gladden House** IHSSI # 175-567-27029, a Queen Anne style house was constructed by Martha Schultz, who later married Percey Gladden, owner of several saloons and the first movie theatre in Salem. The house is eligible under Criteria A, for its association with the social history of Salem, and C, for its intact Queen Anne architecture.
- **Warder and Alice Stevens House # 175-567-G2**, is a Free Classic house, outbuildings and landscape, constructed beginning 1891 for Warder W. Stevens and Alice Caspar Stevens, both of whom were active in local and state agriculture and Alice was a reformer, lecturer and writer. The house is eligible under Criteria B, for its association with Warder and Alice Stevens, both of whom are historically significant, and C, for its intake Free Classic architecture.

**EFFECT FINDING**

- **Ray and Hester Ault House** – The undertaking will have “No Adverse Effect” on the Ray and Hester Ault House.
- **Salem Downtown Historic District** – The undertaking will have “No Adverse Effect” on the NRHP-listed Salem Downtown Historic District.



- **North Main Street Historic District** – The undertaking will have “No Adverse Effect” on the North Main Street Historic District.
- **Schulz/Gladden House** – The undertaking will have “No Adverse Effect” on the Schulz/Gladden House.
- **Warder and Alice Stevens House** – The undertaking will have “No Adverse Effect” on the Warder and Alice Stevens House.

INDOT, acting on FHWA’s behalf, has determined a "No adverse effect" finding is appropriate for this undertaking.

INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

**SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)**

**Ray and Hester Ault House** - This undertaking will not convert property from the Ray and Hester Ault House, a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA’s behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required for **Ray and Hester Ault House**.

**Salem Downtown Historic District** - This undertaking will not convert property from the Salem Downtown Historic District, a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA’s behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required for **Salem Downtown Historic District**.

**North Main Street Historic District** - This undertaking will not convert property from the North Main Street Historic District, a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA’s behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required for **North Main Street Historic District**.

**Schulz/Gladden House** - This undertaking will not convert property from the Schulz/Gladden House, a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA’s behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required for **Schulz/Gladden House**.

**Warder and Alice Stevens House** - This undertaking will not convert property from the Warder and Alice Stevens House, a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA’s behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required for **Warder and Alice Stevens House**.

*Anuradha Kumar V.*

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Anuradha V. Kumar, for FHWA  
 Manager  
 INDOT Cultural Resources

06/27/2019

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Approved Date

**FEDERAL HIGHWAY ADMINISTRATION  
DOCUMENTATION OF SECTION 106 FINDING OF  
“NO ADVERSE EFFECT”  
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER  
PURSUANT TO 36 CFR 800.5 (c)**

**Small Town Pavement Replacement and Bridge Project  
SALEM, WASHINGTON COUNTY, INDIANA  
DES. Nos. 1600873, 1600875, 1700168  
DHPA NO. 22240**

**1. DESCRIPTION OF THE UNDERTAKING**

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), plans to proceed with the Small Town Pavement Replacement and Bridge Project (Des. Nos. 1600873, 1600875, 1700168). The project is located on Main St./State Road (SR) 135 and SR 56 in Salem, Washington Township, Washington County, Indiana.

The project consists of three parts. Pavement Replacement Project along SR 56 (Des. No. 1600873) will consist of a pavement replacement, storm sewers, new curb, gutter and sidewalks, and ramps in compliance with the Americans with Disabilities Act (ADA) along SR 56. The vertical alignment is being modified in order to best fit the existing corridor with some grade changes anticipated. The project will provide continuous sidewalk along SR 56 from the east end of the project to the extent sidewalk (Spring Grove Road). Continuous sidewalk will be constructed along one side of the roadway, and a graded grass area will be provided on the other side of the roadway. The project includes replacing the existing SR 56 bridge over Brock Creek (Des. No. 1700168). The new bridge is proposed to be a two-span (45 feet each), haunched, reinforced concrete slab structure that will match the existing bridge in total length. A minor profile grade will occur at the structure to satisfy the hydraulic and geometric requirements. The waterway opening will increase slightly from the existing since the waterway area blocked by the arches will be eliminated. The typical roadway section at the bridge will consist of two twelve-foot travel lanes with a two-foot curb offset (north side), a four-foot shoulder (south side), and a six-foot six-inch sidewalk along the north side. The railings are an open tube type that will provide a view up and down the stream. The proposed clear roadway width will be 30 feet and the total out-to-out bridge width will be 38 feet, 6 inches. The Pavement Replacement Project along Main St./SR 135 (Des. No. 1600875) will consist of milling 1.5 inches of Hot Mix Asphalt and replacing with 1.5 inches of HMA surface course and reconstructing curb ramps.

Permanent right-of-way acquired is anticipated to be 0.32 acre on SR 56 (31 parcels) and 0.1 acre (one parcel) along SR 135. Total temporary right-of-way will be 0.47 acre on SR 56 and 0.14 acre on SR 135. Temporary right-of-way will be required in the North Main Street Historic District in two locations. Please see Appendix E for preliminary plan sheets.

The area of potential effects (APE) as defined in 36 CFR 800.16(d), is “the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist.” The APE for this project is limited to resources with a proximate viewshed of the project. The APE of the project includes all properties adjacent to the project and those with a proximate viewshed of the project. Forested areas, rolling landscape and bends in the road limited the APE. The APE is approximately 2.8 miles in length and approximately 0.14-mile wide at its widest point near the eastern terminus of the project on SR 56. Please see Appendix A for a map of the APE.

## 2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

The NRHP and Indiana Register of Historic Sites and Structures (State Register) were consulted. The Salem Downtown Historic District (IHSSI #175-567-26001-26075) is listed on the NRHP and State Register; a small sliver of that district is within the APE. The online ArcGIS Indiana Historic Buildings, Bridges, and Cemeteries Map, which includes information from the Indiana Historic Sites and Structures Inventory (IHSSI), was consulted and checked against the *Washington County Interim Report* (2008).

One historic district was identified as eligible in the *Washington County Interim Report*: the North Main Street Historic District, numbered in the interim report as numbers 317[sic]-567-27001-050, but correctly numbered in the SHAARD data as #175-567-27001-050. One individually eligible resource is within the eligible North Main Street Historic District: Schulz/Gladden House, IHSSI #175-567-27006, rated Notable previously. Two eligible properties were not previously surveyed: the Warder and Alice Stevens House #175-567-G2 and the Ray and Hester Ault House, #175-567-G29. The *Indiana Historic Bridge Inventory, Volume 2: Listing of Historic and Non-Historic Bridges* (February 2009) by M & H Architecture, Inc. was reviewed; the project bridge under Des. No. 1700168 within the APE, INDOT Bridge No. 056-88-01478, NBI No. 20180, was noted as “Not eligible”. No historic bridges were identified within the APE.

Connie Zeigler, a Qualified Professional historian and architectural historian conducted site visits of the project area on October 19 (with Qualified Professional, Karen Wood) and November 2, 2017. Zeigler documented above-ground resources within the APE that are at least 50 years of age or that will be at least 50 years of age at the time of the project letting (expected 2020). The historian walked each street in the APE, taking photographs of all resources meriting a Contributing or higher rating. Non-contributing resources or those that did not meet the age requirements were noted but not documented other than in general streetscape photographs. Please see Appendix B for photos.

A Historic Property Report (HPR) was completed for the project (Zeigler, January 30, 2018, distributed February 27, 2018). The summary of the HPR is found in Appendix D.

On February 27, 2018, an early coordination letter was distributed to potential consulting parties inviting them to participate in the Section 106 process and review the HPR for this project. The State Historic Preservation Officer (SHPO) is an automatic consulting party; that office and others that accepted consulting party status are shown in boldface type below. The SHPO and the property owners at that time received a paper copy of the HPSR. Others were invited on INSCOPE <http://erms.indot.in.gov/Section106Documents/>. Below is the list of invited consulting parties. Those identified in bold print are participating consulting parties.

**State Historic Preservation Officer (SHPO) (automatic consulting party)**

**Indiana Landmarks, Southern Regional Office**

Mayor, City of Salem

**Washington County Historian**

**Washington County Historical Society**

Delaware Nation of Oklahoma

Eastern Shawnee Tribe of Oklahoma

**Miami Tribe of Oklahoma**

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potawatomi Indians

**Delaware Tribe of Indians, Oklahoma**

Chris Jackson, a Qualified Professional, conducted a Phase Ia archaeological investigation (Jackson August 14, 2018). It was submitted to the SHPO and distributed to tribes via INSCOPE on August 30, 2018. The

report located no archaeological sites within the project area and recommended that the project be allowed to proceed as planned. No further archaeological work was recommended. The SHPO staff replied to the archaeological report in a letter dated October 1, 2018, stating that “we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places (NRHP) within the project area; it is our opinion that no further archaeological investigations appear necessary at the proposed project area.” The summary of the archaeology report is found in Appendix D.

The State Historic Preservation Officer (SHPO) staff concurred with the recommendations of the HPR in a letter dated March 21, 2018, stating that “we concur with the consultant’s assessments of eligibility and ineligibility for the National Register of Historic Places.”

On March 12, 2018, Indiana Landmarks, Southern Regional Office, responded by letter accepting consulting party status and agreeing with the eligibility recommendations of the HPR.

On March 14, 2018, John Kiser, of the Washington County Historical Society, phoned Green 3 to be sure that the consultant was aware of stone walls, hitching posts, stone steps and brick sidewalks in the area.

On March 15, 2018, the Washington County historian confirmed by email that he and the Washington County Historical Society would like to be consulting parties. The historian mentioned interest in the preservation of hitching posts and carriage steps along the route and asked to be notified of any public meetings over the project or any proposed plans for the restoration of “historical artifacts along the route(s).” He noted that: “Several residents have contacted me asking if I knew if they should plan to remove their old posts prior to the state project.” Green 3’s historian responded by email on the same date stating “Please tell residents NOT to remove any of those features. They are considered contributing to the North Main Street Historic District. No work will take place until after we have the plans and know for sure what will be done with the sidewalks and streets.”

### 3. DESCRIBE AFFECTED HISTORIC PROPERTIES

- **Ray and Hester Ault House** IHSSI # 175-567-G29, a one-story flat-roof Modern style house constructed of the unusual material, McCorkle block is eligible under Criterion C for its use of unusual material and modern architecture with a nod to International Style design.
- **Salem Downtown Historic District** (listed 1996), IHSSI #s 175-567-26001-275, has early commercial and governmental buildings and residential areas from the mid-nineteenth to mid-twentieth centuries. The Salem Downtown Historic District is NRHP-listed under Criteria A and C.
- **North Main Street Historic District** IHSSI #s 175-567-27001-050, is residential with houses dating from the 1850s to 1960s in styles ranging from Greek Revival to Ranch. The district is eligible under Criteria A, for its association with the development of Salem, and C, for its intact architecture.
- **Schulz/Gladden House** IHSSI # 175-567-27029, a Queen Anne style house was constructed by Martha Schultz, who later married Percy Gladden, owner of several saloons and the first movie theatre in Salem. The house is eligible under Criteria A, for its association with the social history of Salem, and C, for its intact Queen Anne architecture.
- **Warder and Alice Stevens House** IHSSI # 175-567-G2, is a Free Classic house, outbuildings and landscape, constructed beginning 1891 for Warder W. Stevens and Alice Caspar Stevens, both of whom were active in local and state agriculture and Alice was a reformer, lecturer and writer. The house is eligible under Criteria B, for its association with Warder and Alice Stevens, both of whom are historically significant, and C, for its intake Free Classic architecture.

#### 4. DESCRIBE THE UNDERTAKING'S EFFECT ON HISTORIC PROPERTIES

**Ray and Hester Ault House.** While the project will be visible from the property, the project will not require right-of-way or have any direct impacts at this location. The slight changes in viewshed that may be created from pavement replacement adjacent to the property and/or driveway reconstruction on the opposite side of SR 56 will not in any way impact the integrity of this property. The project will have No Adverse Effect on the Ray and Hester Ault House.

**Salem Downtown Historic District.** Incidental construction may occur along SR 56 adjacent to the district in front of one property—a modern church with parking lot – which is not a contributing resource in the historic district and there are no other elements that contribute to the district (such as historic sidewalks) at this location. The project will have No Adverse Effect on the NRHP-listed Salem Downtown Historic District.

**North Main Street Historic District.** A small amount of temporary right-of-way will be required at two locations where brick will be removed from sidewalk and curb ramp sections for construction of ADA-compliant ramps within the district. The temporary right-of-way will be needed only from areas currently in a transportation use. The project will occur only on the existing roadway and at specific sidewalk and curb locations and will not impact historic hitching posts, tree lawns, or historic curbs. The project will not impact historic sidewalks at any location other than those described below. At the northeast corner of Main St./SR 135 and SR 56: approximately 266 square feet of bricks will be removed from the sidewalk within the right-of-way to construct new ADA-compliant ramps. A two-foot section of ADA Detectable Warning Surface (see Detectable Warning Surface photo in Appendix X) will be installed in the new ramp, the rest of the brick will be re-laid if possible or new brick that matches the existing in size and color will be laid if the existing brick is too damaged to reuse at this location. The new curb will be concrete, as is the existing curb; the new ramp will be constructed entirely with brick. An existing brick wall on the property at the northeast corner of Main St./SR 135 and SR 56 will not be affected. The brick will be re-laid in the same pattern as it is currently. It should be noted that most of the bricks at this location were previously removed and bricks were re-laid as part of an INDOT project in 1999. At that time a sidewalk ramp was constructed at this location. The 1999 project plans (see Appendix E) called for the brick to be removed, salvaged and reinstalled. However, the existing bricks are laid in a different pattern than the rest of the historic sidewalk and they appear a slightly different color than the rest of the sidewalk; that may be due to exposure of a different brick face when the sidewalk was re-laid, or perhaps to replacement of some damaged brick with different bricks at the time of construction. This project work will occur adjacent to a contributing property.

At the intersection of Main St./SR 135 and Salem Ave., approximately two feet on each side of the intersection in each quad will be replaced with ADA Detectable Warning Surface (bricks) at all four quadrants. On the east side of Main St./SR 135, temporary right-of-way will be needed for the construction; no temporary or permanent right-of-way will be needed on the west side of Main St./SR 135. Although there will be a minor loss of these brick sidewalk sections (approximately two feet in each quad), and a two-foot section of limestone edging at two of the locations, as an entire unit the sidewalk in the historic district has already lost integrity due to the replacement of many sections of sidewalk with numerous non-historic materials, including asphalt, pavers, new bricks, and long sections of concrete. The rest of the existing historic brick sidewalk (which comprises a slight majority of the entire sidewalk) will remain intact. This small loss will not constitute an adverse effect to the district.

Project commitments will include the reinstallation of bricks, either reusing the existing bricks, if they are reusable or installing bricks that match the existing bricks in color and size at the corner of Main/SR 135

and SR 56, excepting the location where two-feet of ADA Detectable Warning Surface is used, in order to minimize impacts. Project commitments will also include avoiding impacting all hitching posts and limestone steps that are in North Main Street Historic District or replacing any impacted element back in its current location. The project will have No Adverse Effect on the North Main Street Historic District.

**Schulz/Gladden House.** There will be no direct impacts and little if any visual impact from the street pavement replacement. There is no historic brick sidewalk near the house and no sidewalk work will occur near the concrete sidewalk or the stonewall that is on the property side of the sidewalk. No other contributing features will be affected by the pavement replacement. The project will have No Adverse Effect on the Schulz/Gladden House.

**Warder and Alice Stevens House.** The project will have no direct impact at this location. Nor will it affect the viewshed from the house or any features that contribute to the property's significance. The project may be visible from the front lawn of the property where it is adjacent to SR 135, but due to the nature of the project, which is the replacement of road surface, the visual effects won't impact the historic features or setting of the property. The project will have No Adverse Effect on the Stevens house.

## **5. EXPLAIN APPLICATION OF CRITERIA OF ADVERSE EFFECT – INCLUDE CONDITIONS OR FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS**

According to 36 CFR 800.5(a)(1) "An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling or association."

**Ray and Hester Ault House.** Per 800.5(a)(2)(i), the undertaking will not result in the "Physical destruction of or damage to all or part of the property;" there will be no right-of-way acquired from the property. See plan sheet E1 in Appendix E)

Per 36 CFR 800.5(a)2(ii), the undertaking will not cause the "Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties and/or other applicable guidelines."

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the "Removal of the property from its historic location."

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in the "Change of the character of the property's use of or physical features within the property's setting that contribute to its historic significance." There will be no direct impacts and the project will have little to no impact on the property's setting, which has always been adjacent to a state highway. Project elements, such as the reconstruction of a driveway on the opposite side of SR 56 and the repaving of SR 56 will not change any features that contribute to its historic significance.

Per 36 CFR 800.5(a)2(v), the undertaking will not result in the "Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features." The project will not introduce elements that diminish the historic architecture or materials of the house, which make it eligible for the NRHP.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...”

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...”

**Salem Downtown Historic District.** Per 800.5(a)2(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property;” there will be no physical damage to the Salem Downtown Historic District as a result of this project. No right-of-way will be taken from the property boundary. See plan sheet E2 in Appendix E.

Per 36 CFR 800.5(a)2(ii), the undertaking will cause no “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines.”

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the “Removal of the property from its historic location.”

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in the “Change of the character of the property’s use of or physical features within the property’s setting that contribute to its historic significance.”

Per 36 CFR 800.5(a)2(v), the undertaking will not result in the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The project will only be visible from properties that are considered non-contributing to the historic district.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...”

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...”

**North Main Street Historic District.** Per 800.5(a)2(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property;” there will be no physical damage to the North Main Street Historic District as a result of this project. Small sections of brick sidewalk will be removed and replaced with modern brick at the intersections of Salem Avenue and Main St./SR 135, and the sidewalk at the corner of Salem Ave. and Main St./SR 135 will be removed and then re-laid with the existing or matching brick, except the two-foot section of ADA Detectable Warning Surface. However, the sidewalk on the east side of Main St./SR 135 as a whole has already undergone many alterations and these small changes will not rise to the level of adverse effect. See plan sheet E2-E5 in Appendix E

Per 36 CFR 800.5(a)2(ii), the undertaking will cause no “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines.” The installation of ADA-compliant ramps at the sidewalk intersection of Salem Ave. and Main St./SR 135 will be consistent with the Secretary’s Standards of Rehabilitation and with the guidance as per Preservation Brief 32 “Making Historic Properties Accessible,” which states the following: “when new features are incorporated for accessibility, historic materials and

features should be retained whenever possible. Accessibility modifications should be in scale with the historic property, visually compatible, and, whenever possible, reversible. . . The design of new features should also be differentiated from the design of the historic property so that the evolution of the property is evident.” At the intersection of Main St./SR 135 and SR 56 the historic bricks will be reused wherever possible and any necessary newer brick will match in color and size the existing bricks. The new ADA-compliant Detectable Warning Surface that will be added at the curb ramp and at the sidewalk intersection at Salem Ave. and Main St./SR 135, although modern brick, will be visually compatible with the brick sidewalks where it will be added, but also differentiated from the historic design with the use of new brick in the warning surface.

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the “Removal of the property from its historic location.”

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in the “Change of the character of the property’s use of or physical features within the property’s setting that contribute to its historic significance,” as only small sections of the sidewalk will be changed and the project commitments will include replacement of the existing bricks at the intersection of Main St./SR 135 and SR 56 with the same bricks where possible or with bricks in the same color and size. The only noticeable changes will be at the locations of the two-foot wide modern brick Detectable Warning Surface for the ramp at Main St./SR 135 and SR 56 and at the two-foot wide sections at the four corners of Salem Ave. and Main St./SR 135. These changes are small compared to the many altered sections of sidewalk that already exist.

Per 36 CFR 800.5(a)2(v), the undertaking will not result in the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” Only small sections of brick in the already altered sidewalk will be changed. The bricks at the corner of Main St./SR 135 and SR 56 will be re-laid in the same pattern as currently exists and will, therefore, look similar. Overall the visual changes will be so minor that the integrity of the district will not be diminished.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...”

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...”

**Schulz/Gladden House.** Per 800.5(a)(2)(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property.” There will be no direct impacts to the property. See plan sheet E3 in Appendix E.

Per 36 CFR 800.5(a)2(ii), the undertaking will not cause “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines.”

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the “Removal of the property from its historic location.”

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in the “Change of the character of the property’s use of or physical features within the property’s setting that contribute to its historic significance.” There will be no direct impacts and no impacts to historic physical features of the house or its setting as the only project element that will occur adjacent to the house will be the replacement of the street pavement.



Per 36 CFR 800.5(a)2(v), the undertaking will not result in the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” While the street pavement will be replaced in front of the house, this should cause little, if any, visual changes to the property and none that would impact its significant historic features.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...”

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...”

**Warder and Alice Stevens House.** Per 800.5(a)2(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property;” there will be no right-of-way acquired from the property and no direct impacts. See plan sheets E6 and E7 in Appendix E.

Per 36 CFR 800.5(a)2(ii), the undertaking will not cause “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines.”

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the “Removal of the property from its historic location.”

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in the “Change of the character of the property’s use of or physical features within the property’s setting that contribute to its historic significance.” There will be no direct impacts on the property nor any change in its setting that will impact its historic integrity.

Per 36 CFR 800.5(a)2(v), the undertaking will not result in the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The project will be barely visible, if at all, from the house and not highly visible from the front yard and the nature of the project, replacement of street pavement, will not introduce a visual change that will impact the property’s significance in any way.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...”

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...”

## 6. SUMMARY OF CONSULTING PARTIES AND PUBLIC VIEWS

On February 27, 2018, an early coordination letter was distributed to potential consulting parties inviting them to participate in the Section 106 process and review the Historic Property Report (HPR; Zeigler, 1/30/2018) for this project.

The State Historic Preservation Officer (SHPO) staff concurred with the recommendations of the HPR in a letter dated March 21, 2018, stating that “we concur with the consultant’s assessments of eligibility and ineligibility for the National Register of Historic Places.”

On February 27, 2018, the Miami Tribe of Oklahoma responded affirmatively to the consulting party invitation by email.

On March 12, 2018, Indiana Landmarks, Southern Regional Office, responded by letter accepting consulting party status and agreeing with the eligibility recommendations of the HPR.

On March 14, 2018, John Kiser, of the Washington County Historical Society, phoned Green 3 to be sure that the consultant was aware of stone walls, hitching posts, stone steps and brick sidewalks in the area.

On March 14, 2018, the Delaware Nation of Oklahoma responded by email concurring with the proposed plan and accepting consulting party status.

On March 15, 2018, the Washington County historian confirmed by email that he and the Washington County Historical Society would like to be consulting parties. The historian mentioned interest in the preservation of hitching posts and carriage steps along the route and asked to be notified of any public meetings over the project or any proposed plans for the restoration of “historical artifacts along the route(s).” He noted that: “Several residents have contacted me asking if I knew if they should plan to remove their old posts prior to the state project.” Green 3’s historian responded by email on the same date stating “Please tell residents NOT to remove any of those features. They are considered contributing to the North Main Street Historic District. No work will take place until after we have the plans and know for sure what will be done with the sidewalks and streets.”

A Phase Ia archaeological investigation (Jackson August 14, 2018) was completed and submitted to the SHPO and distributed to tribes via INSCOPE on August 30, 2018. The report located no archaeological sites within the project area and recommended that the project be allowed to proceed as planned. No further archaeological work was recommended. The SHPO staff replied to the archaeological report in a letter dated October 1, 2018, stating that “we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places (NRHP) within the project area; it is our opinion that no further archaeological investigations appear necessary at the proposed project area.”

On April 12, 2019, an effects letter was distributed to consulting parties, explaining the effects and efforts to minimize the effects of the project. The letter recommended a No Adverse Effects finding.

In a letter dated May 13, 2019, the SHPO concurred with the recommendation of No Adverse Effects, stating that “we agree that the proposed project does not appear to adversely affect the integrity of the National Register-listed or -eligible properties within the APE.”

No other consulting comments were received. See Appendix C for copies of all consulting party correspondence.

## **APPENDICES**

A – Maps

B – General Photographs

C – List of Consulting Parties and Consulting Party Correspondence

D – Historic Property Report and Archaeology Report Summaries

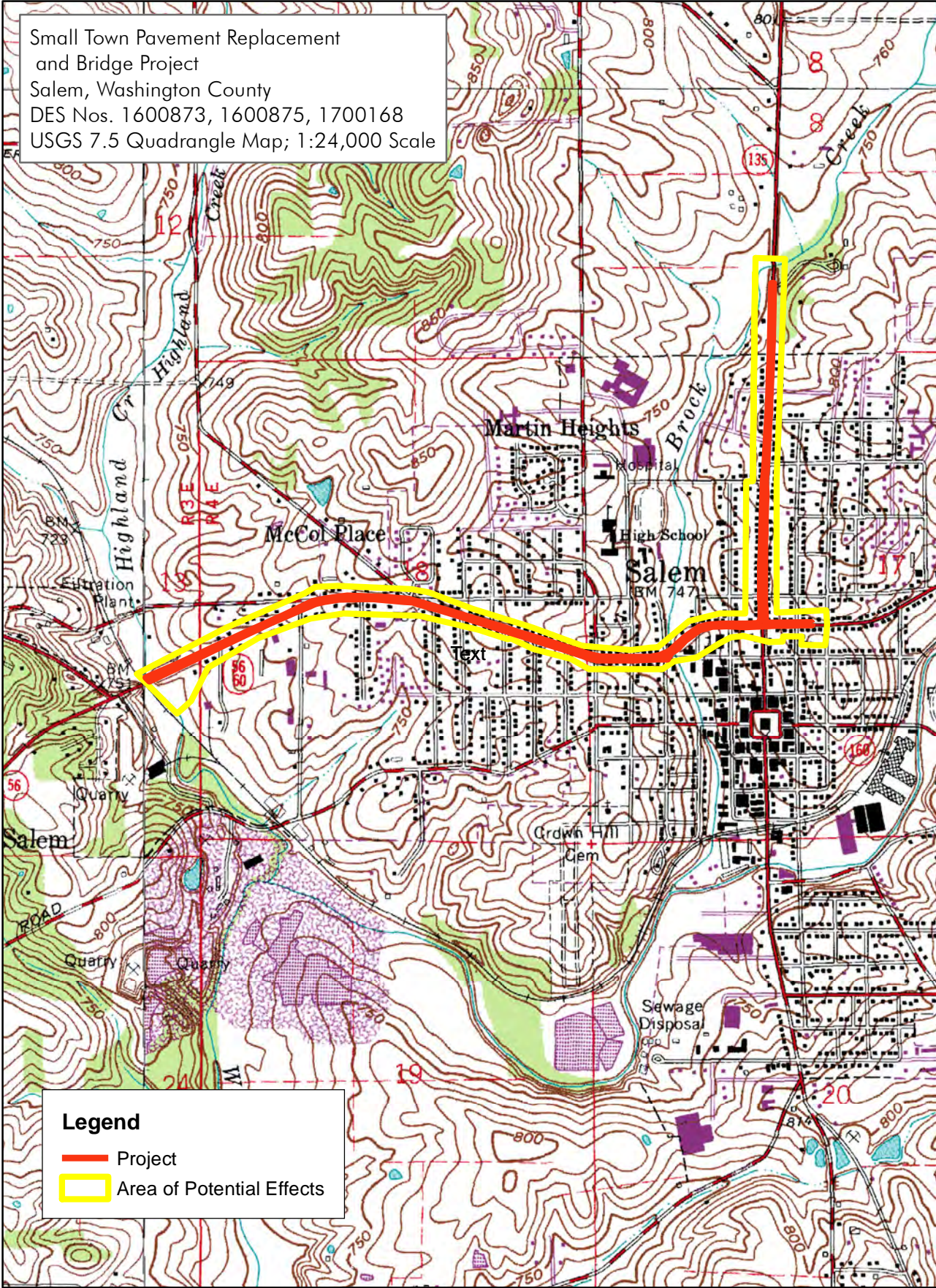
Appendices E and F were intentionally omitted. Please refer to Appendix B in the CE document.

E – Plan Sheets showing historic properties and 1999 brick installation

F – Preliminary Plans

# Topographic Map

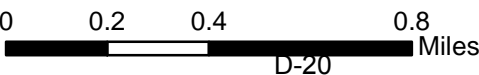
Small Town Pavement Replacement  
and Bridge Project  
Salem, Washington County  
DES Nos. 1600873, 1600875, 1700168  
USGS 7.5 Quadrangle Map; 1:24,000 Scale



## Legend

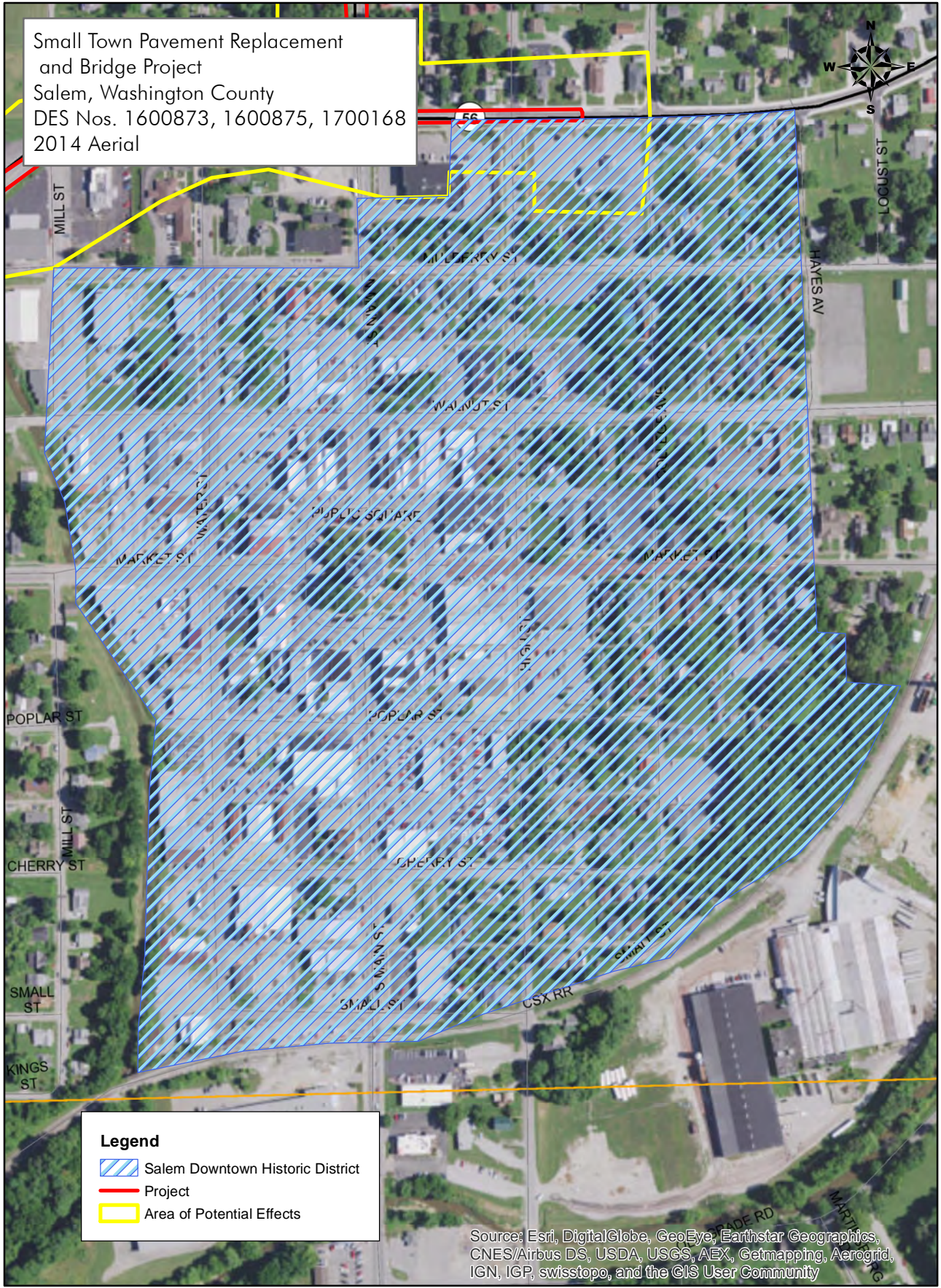
- Project
- Area of Potential Effects

USGS Quads: Salem, Becksmill  
County: Washington

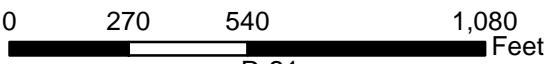


Green3 Studio

# Salem Downtown Historic District Boundaries



Township: Washington  
 County: Washington



# Warder and Mary Alice Stevens Property Historic Boundary

Small Town Pavement Replacement  
and Bridge Project  
Salem, Washington County  
DES Nos. 1600873, 1600875, 1700168  
2014 Aerial

135

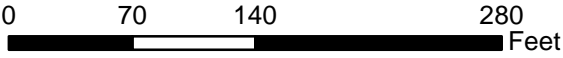


**Legend**

- Project
- Area of Potential Effects
- Warder and Mary Alice Stevens Property

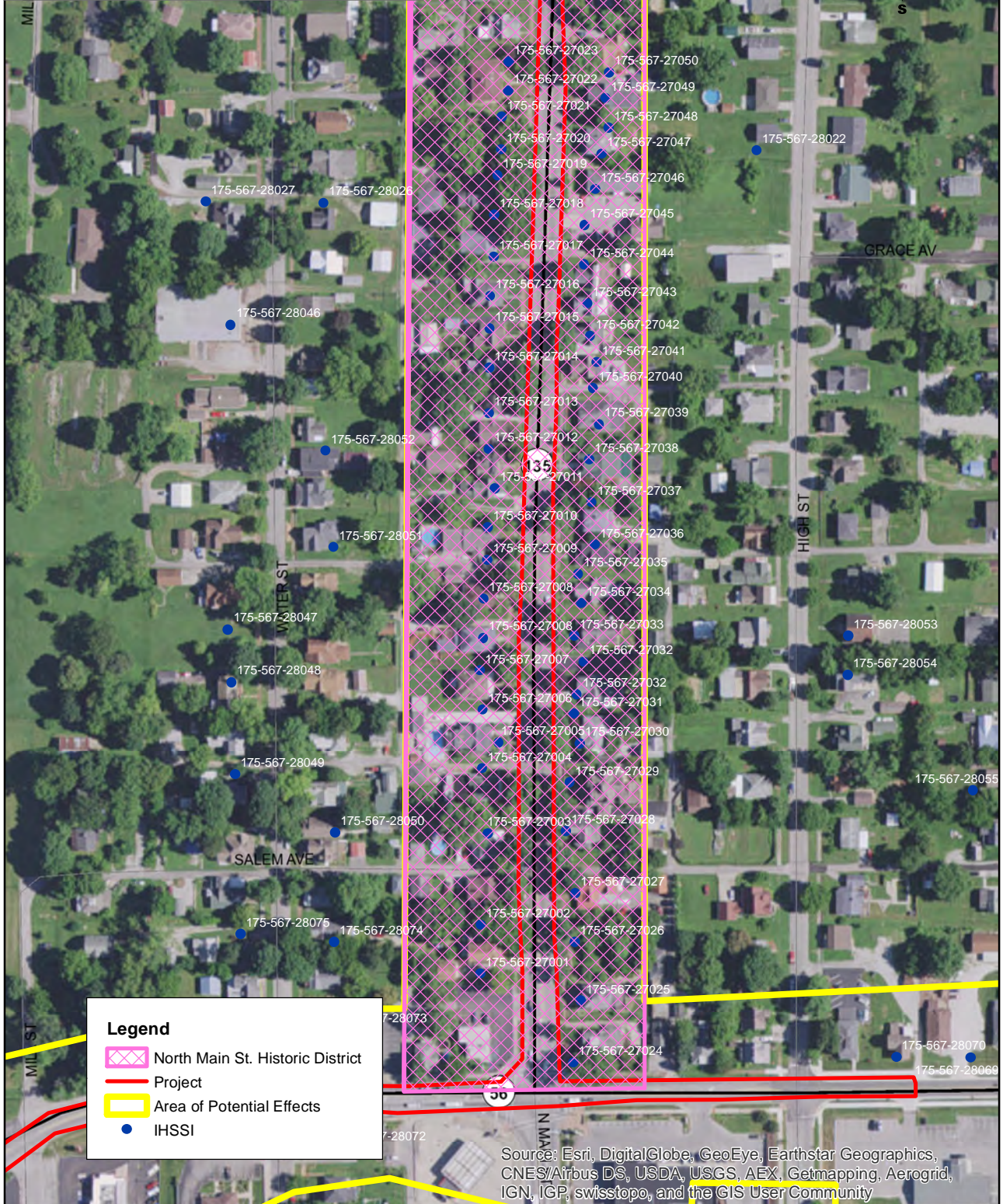
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Township: Washington  
County: Washington

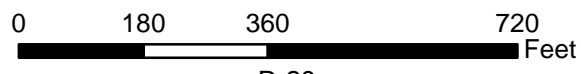


# North Main Street Historic District Boundaries

Small Town Pavement Replacement  
and Bridge Project  
Salem, Washington County  
DES Nos. 1600873, 1600875, 1700168  
2014 Aerial



Township: Washington  
County: Washington



●●● Green3 Studio

# Schulz/Gladden House Boundary

Small Town Pavement Replacement  
and Bridge Project  
Salem, Washington County  
DES Nos. 1600873, 1600875, 1700168  
2014 Aerial

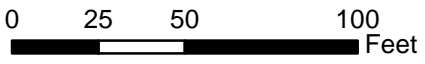


**Legend**

- Schulz-Gladden House
- Project
- Area of Potential Effects
- North Main St. Historic District

Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community

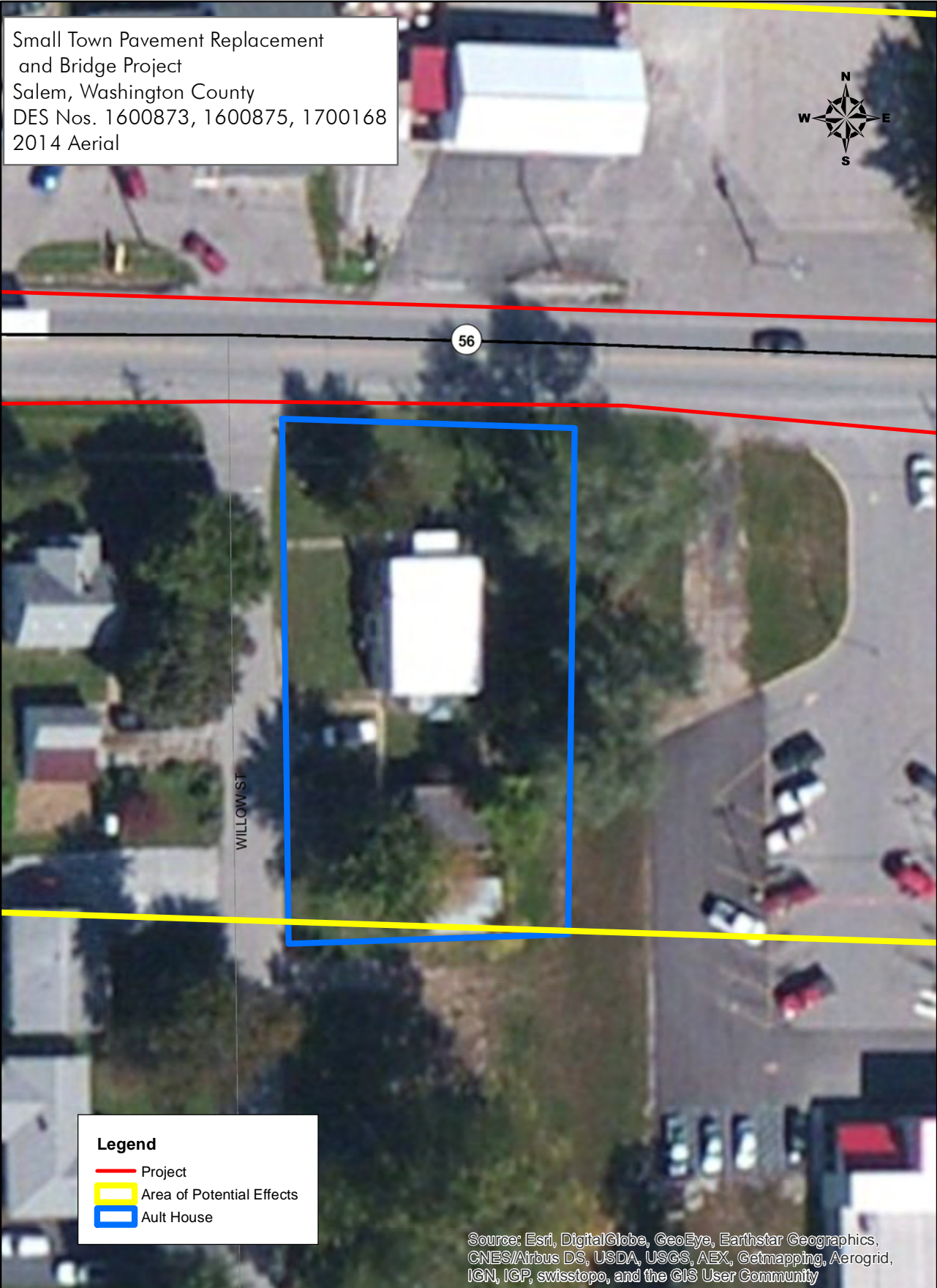
Township: Washington  
County: Washington





# Ault House Historic Boundary

Small Town Pavement Replacement  
and Bridge Project  
Salem, Washington County  
DES Nos. 1600873, 1600875, 1700168  
2014 Aerial

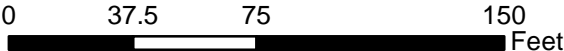


**Legend**

- Project
- Area of Potential Effects
- Ault House

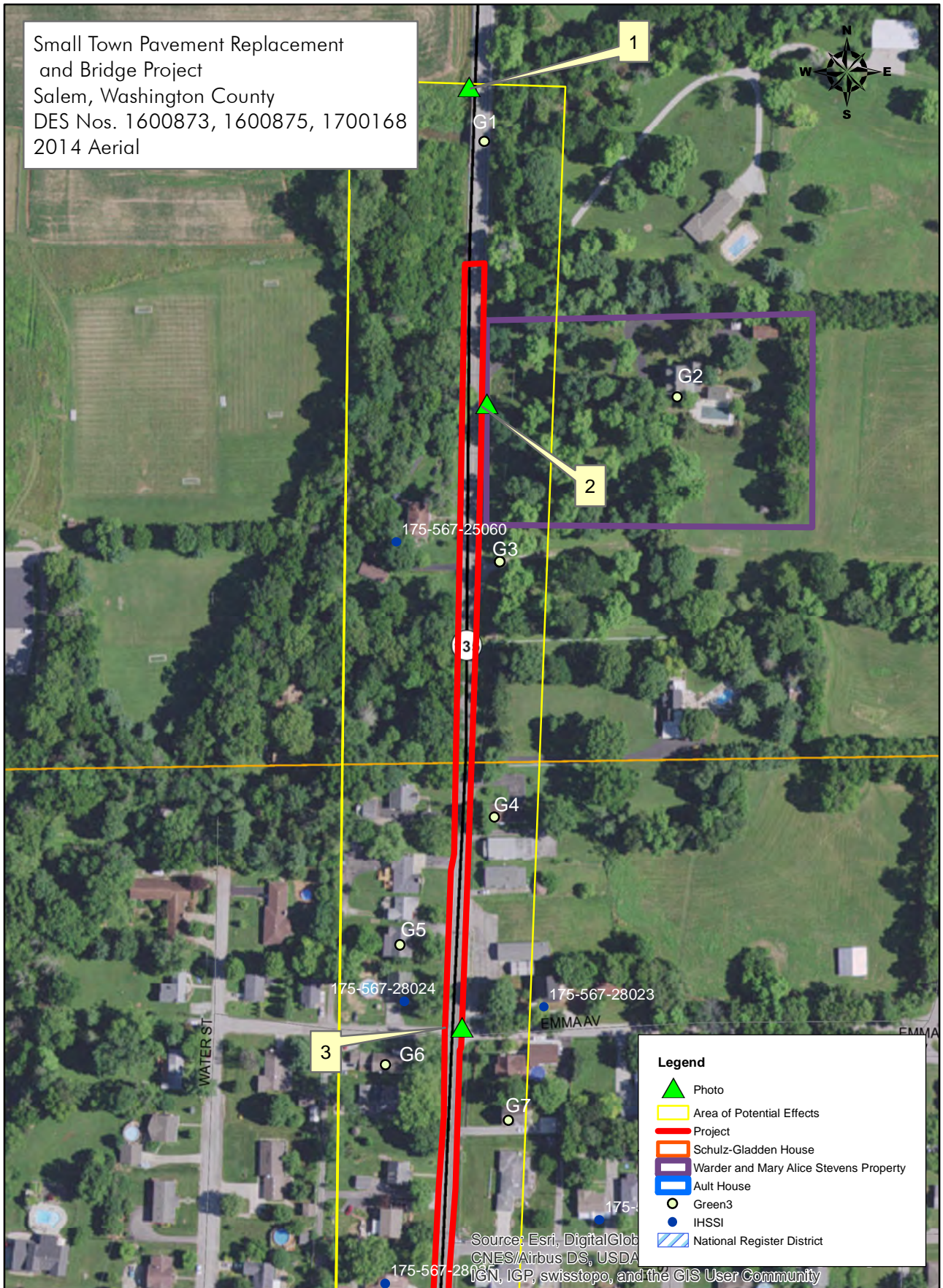
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Township: Washington  
County: Washington

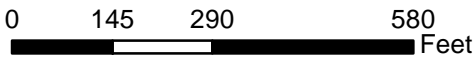


# Photo Locations 1

Small Town Pavement Replacement  
and Bridge Project  
Salem, Washington County  
DES Nos. 1600873, 1600875, 1700168  
2014 Aerial

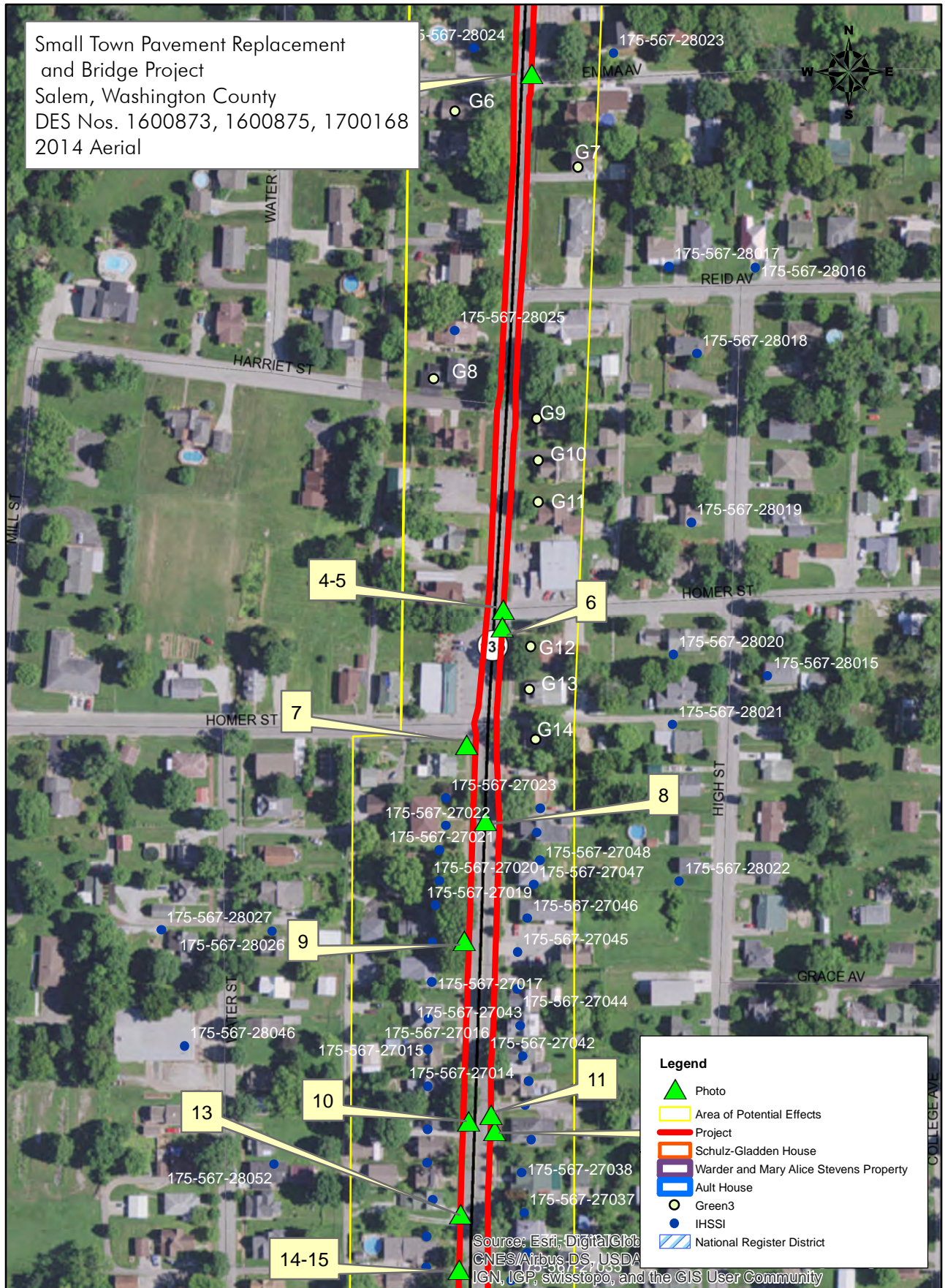


Township: Washington  
County: Washington



# Photo Locations 2

Small Town Pavement Replacement  
and Bridge Project  
Salem, Washington County  
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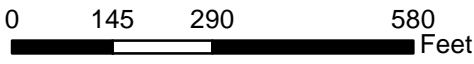


**Legend**

- ▲ Photo
- Area of Potential Effects
- Project
- Schulz-Gladden House
- Warder and Mary Alice Stevens Property
- Ault House
- Green3
- IHSSI
- National Register District

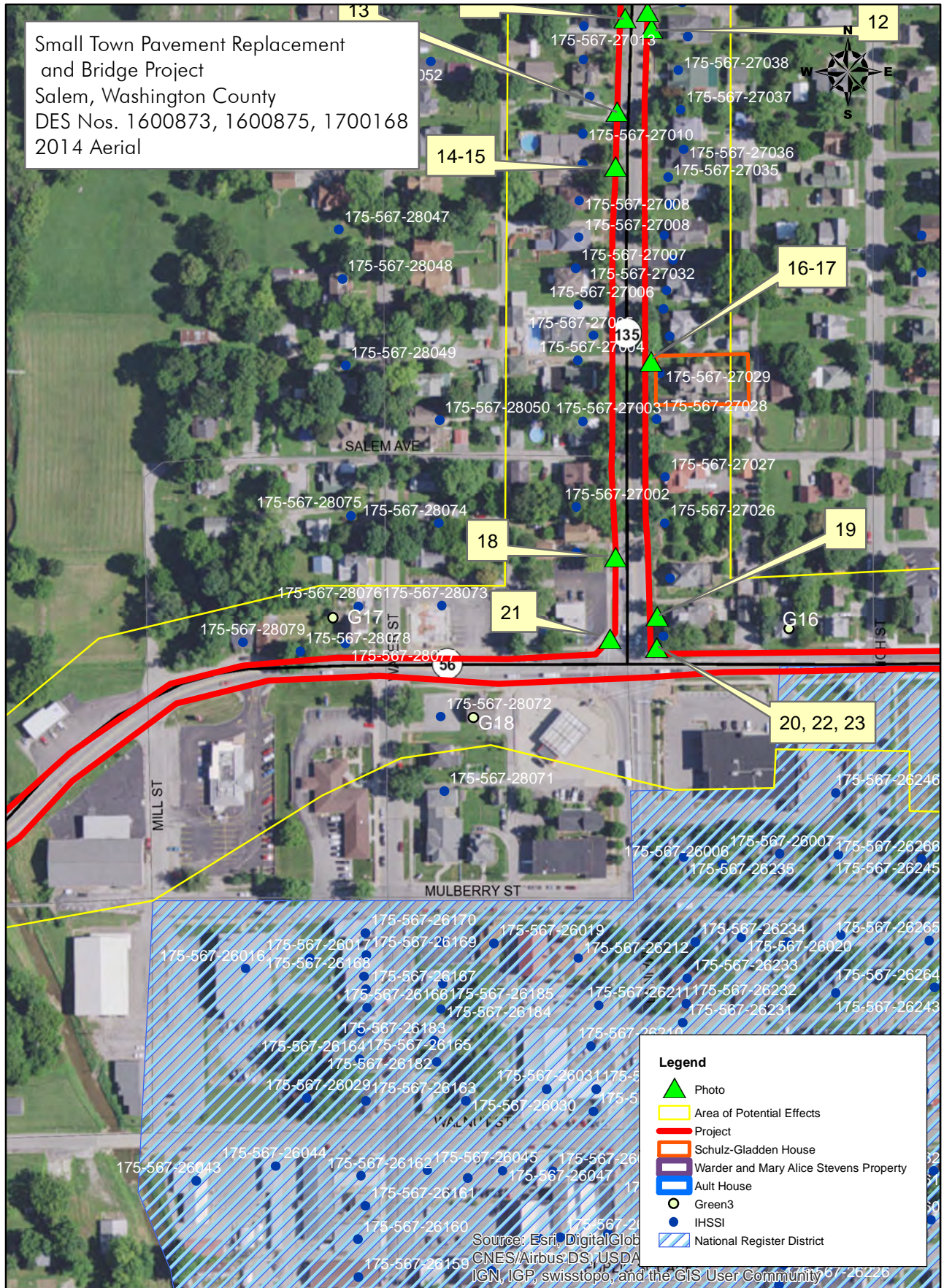
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Township: Washington  
County: Washington

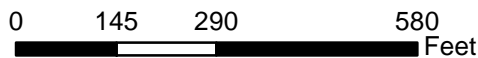


# Photo Locations 3

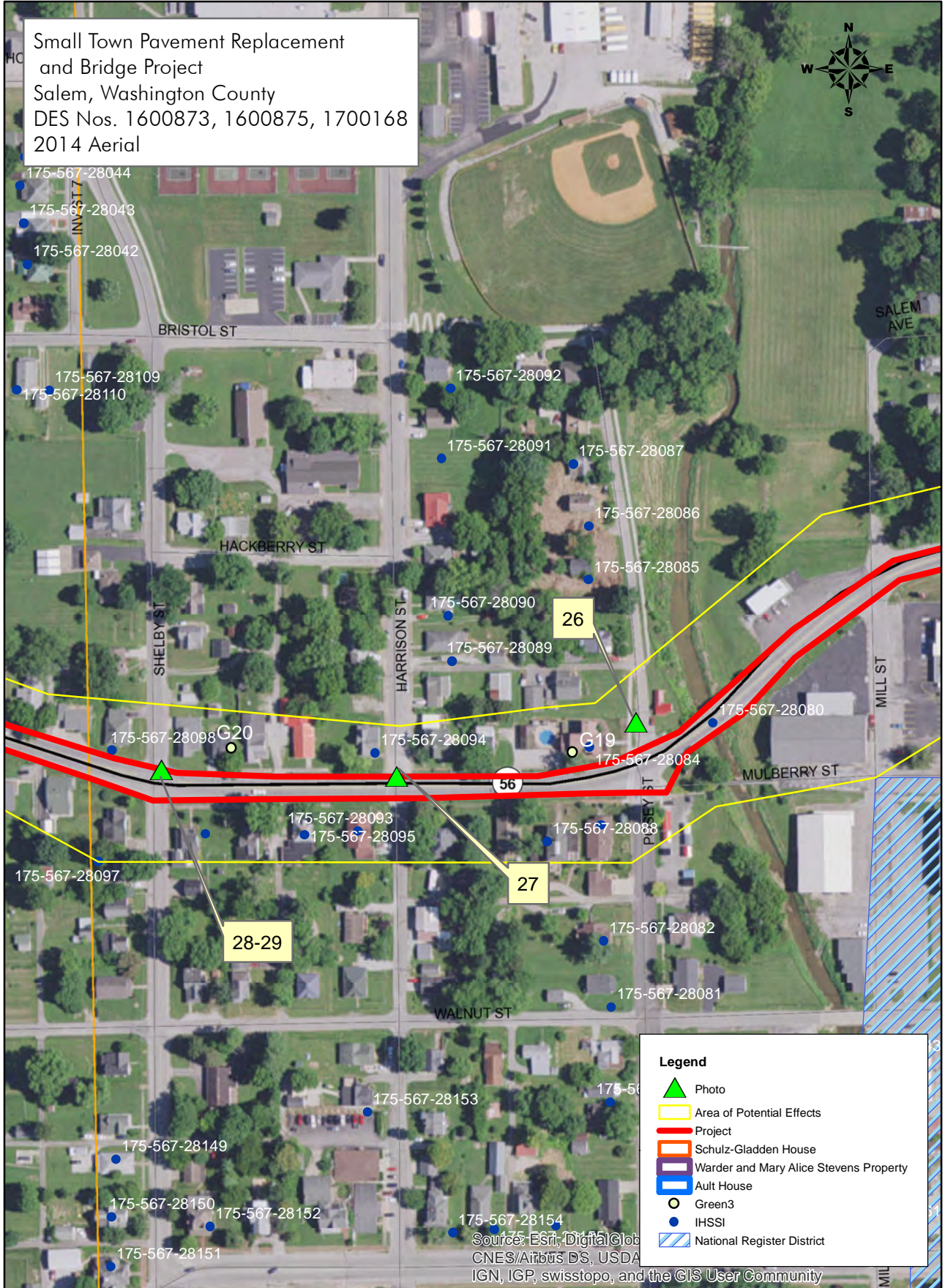
Small Town Pavement Replacement  
and Bridge Project  
Salem, Washington County  
DES Nos. 1600873, 1600875, 1700168  
2014 Aerial



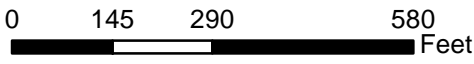
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County: Washington



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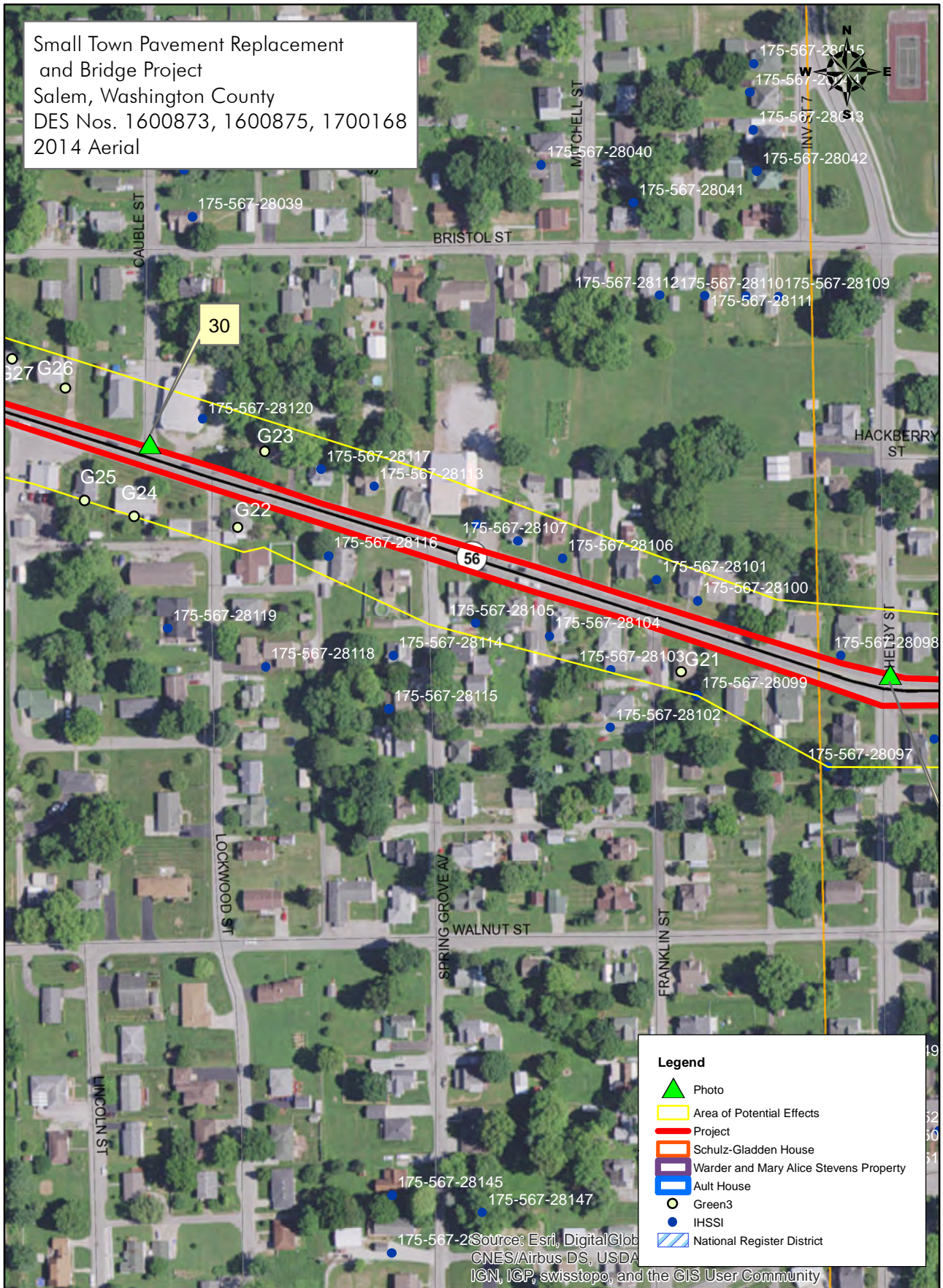


Township: Washington  
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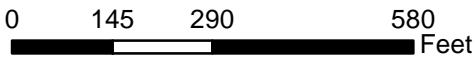


# Photo Locations 5

Small Town Pavement Replacement  
and Bridge Project  
Salem, Washington County  
DES Nos. 1600873, 1600875, 1700168  
2014 Aerial

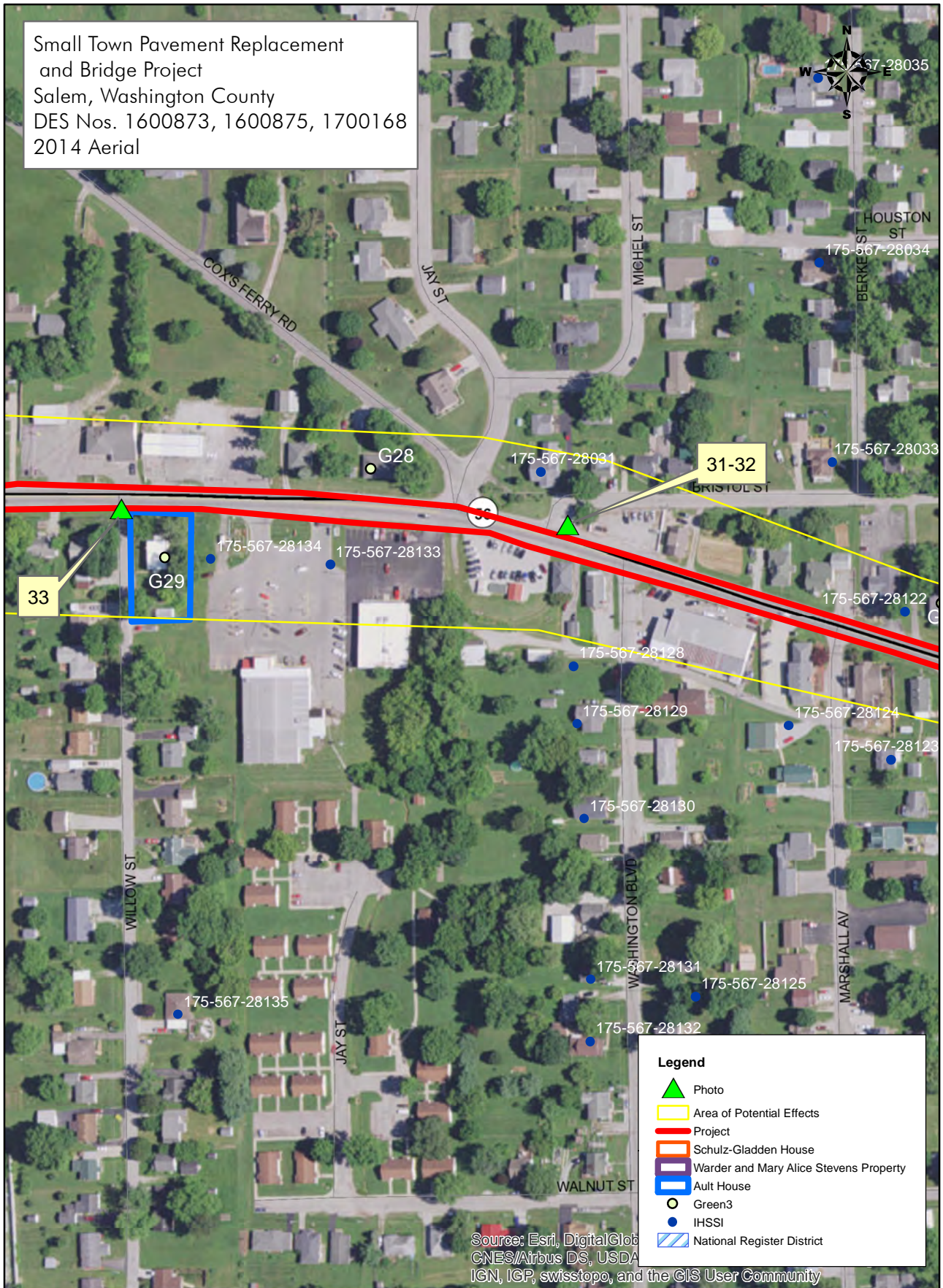


Township: Washington  
County: Washington



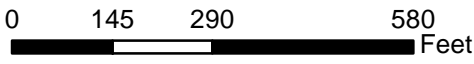
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Small Town Pavement Replacement  
and Bridge Project  
Salem, Washington County  
DES Nos. 1600873, 1600875, 1700168  
2014 Aerial



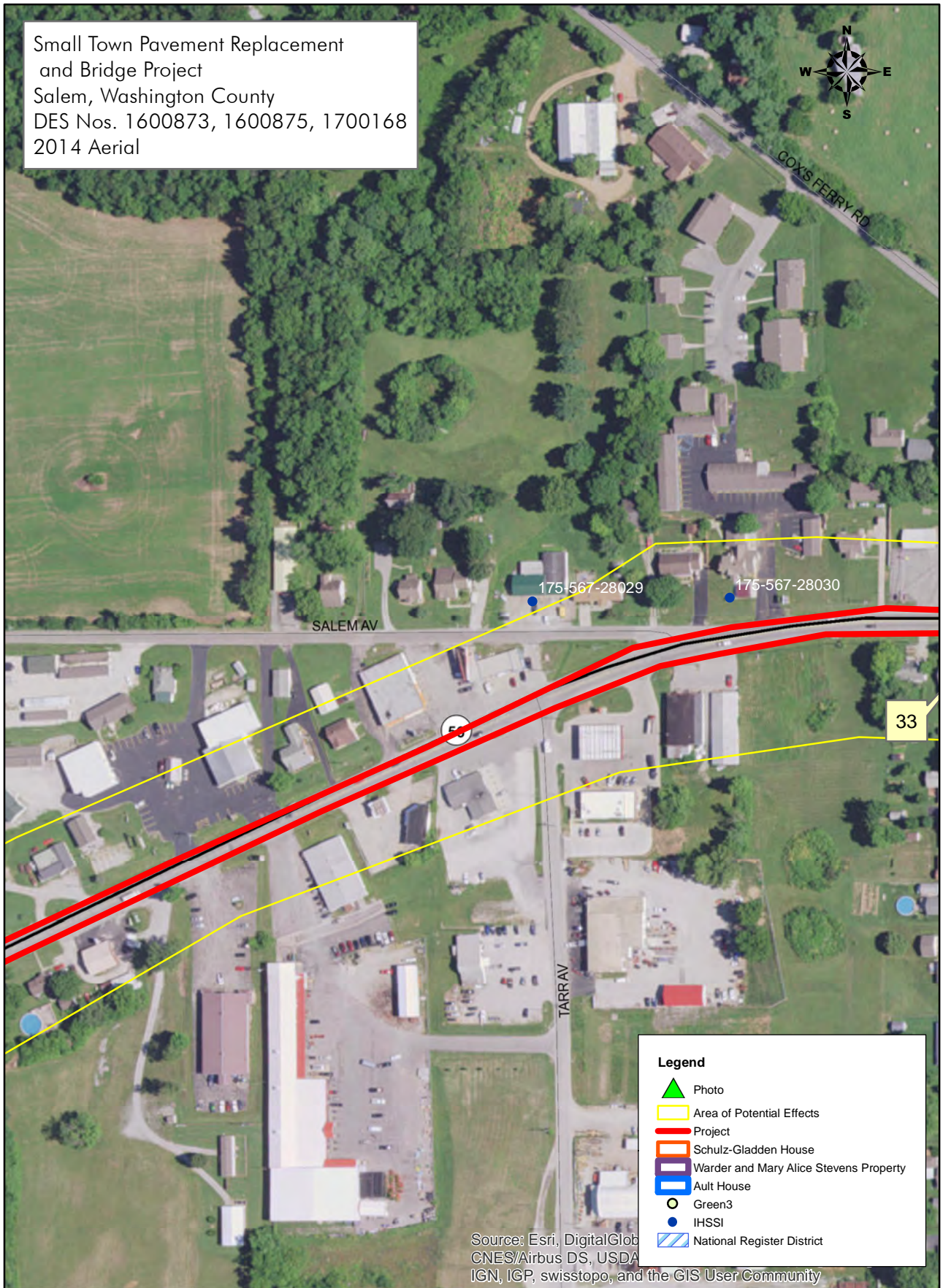
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Township: Washington  
County: Washington



# Photo Locations 7

Small Town Pavement Replacement  
and Bridge Project  
Salem, Washington County  
DES Nos. 1600873, 1600875, 1700168  
2014 Aerial

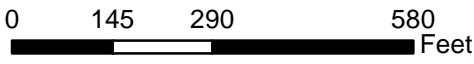


**Legend**

- ▲ Photo
- Area of Potential Effects
- Project
- Schulz-Gladden House
- Warder and Mary Alice Stevens Property
- Ault House
- Green3
- IHSSI
- National Register District

Source: Esri, DigitalGlobe, CNES/Airbus DS, USDA, IGN, IGP, swisstopo, and the GIS User Community

Township: Washington  
County: Washington





# Photo Locations 8

Small Town Pavement Replacement  
and Bridge Project  
Salem, Washington County  
DES Nos. 1600873, 1600875, 1700168  
2014 Aerial

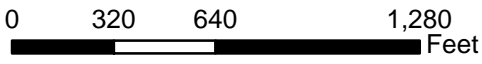


**Legend**

- Photo
- Area of Potential Effects
- Project
- Schulz-Gladden House
- Warder and Mary Alice Stevens Property
- Ault House
- Green3
- IHSSI
- National Register District

Source: Esri, DigitalGlobe, CNES/Airbus DS, USDA, IGN, IGP, swisstopo, and the GIS User Community

Township: Washington  
County: Washington





1. Looking south on SR 135 from end of APE



2. Looking south at concrete ditch on property of 1201 Main St., G-2



3. Looking south at Main Street from Emma Street



4. Looking north on SR135-Main Street, from Homer Street



5. Looking south on SR135-Main Street from Homer Street.



6. Looking south at sidewalks starting at Homer Street



7. Looking south at North Main Street from west side of Homer Street



8. Looking south at North Main Street from 700 block



9. Showing sidewalk treatments near 701 N. Main Street



10. Looking south at herringbone pattern brick sidewalk, west side of 600 block North Main



11. Looking south at sidewalk and retaining wall east side of 600 block North Main



12. Looking south at stone and concrete curbs and gutter by street 600 block of N. Main in HD



13. South at east side 500 block N. Main from front of 602 N. Main in HD



14. Southeast at 500 block North Main Street in North Main Street HD





15. Looking south at west side of 500 block N. Main Street in North Main Street HD



16. Looking south in front of 505 Main Street in North Main Street HD, 503 Main,



17. South at altered sidewalk in front of 505 N. Main Street



18. Looking north on west side of N. Main Street, 404 Main, McMurrin House, 175-567-27001



19. Looking south at stone slab steps in front of N. 401 Main



20. North from 401 North Main at walk, slabs, hitching post and North Main Street



21. Looking south from 404 North Main at intersection with SR 56



22. Looking west from intersection of SR 56 and SR 135



23. Looking east from SRs 56 and 135 intersection, south end of North Main Street HD



24. Looking west from the east end of the APE on Hackberry St.



25. Looking south from east end of APE on Hackberry Street



26. Looking southwest at 400 block W. Mulberry Street



27. Looking east on SR 56-Mulberry Street at intersection of Harrison and Mulberry



28. Looking east at Mulberry-SR 56- from Shelby Street



29. Looking south at Shelby Street from Mulberry-SR 56 intersection



30. Looking west at Mulberry-SR 56- from Franklin Street





31. Looking east fom Bristol and Mulberry



32. Looking west at Bristol and Mulberry



33. Looking east on W. Mulberry-SR 56- from Willow Street



34. Looking east at SR 56 from end of APE



## INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

February 27, 2018

This letter was sent to the listed parties.

Re: Small Town Pavement Replacement and Bridge Project Des Nos. 1600873, 1600875, 1700168, Salem, Washington County, Indiana

Dear Consulting Party,

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration, proposes to proceed with the Small Town Pavement Replacement and Bridge Project Des Nos. 1600873, 1600875, and 1700168. Green 3, LLC is under contract with INDOT to advance the environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking located on State Roads (SR) 135 and 56 in Salem, Washington Township, Washington County, Indiana. Specifically, the project is located in the Salem and Becks Mill quads (7.5) map, Sections 17 and 18, Township 2 North, Range 4 East and Section 13, Township 2 North, Range 3 East.

The purpose of the project is to improve the pavement condition, improve safety on the road and bridge, and provide ADA compliance on the sidewalk. It is not known at this time if right-of-way will be acquired for the project. The need for the project DES. Nos. 1600873 and 1600875 is due to the poor pavement conditions of SR 56 and SR 135, as well as areas where sidewalks exist that do not comply with ADA requirements. In respect to DES. No. 1700168, INDOT Bridge No. 056-88-01478, NBI No. 20180, is narrow, and in poor condition. The intent of this project is to remove completely the three-span arch structure and replace it with a new bridge structure. The proposed new footprint should be similar in size to the existing structure. The bituminous wearing surface of the bridge has numerous cracks. The curb and copings have exposed rebar and the drains are clogged. The arches have wide cracks and are scaling with efflorescence. There is cracking, scaling and efflorescence in all the spandrel walls. The west-side abutment footing is exposed and the bridge seats are cracking. One of the pier caps has exposed rebar. Project plans are not yet available.

Land use around the project is generally urban with some commercial properties intermingled on North Main Street; land use includes residential, commercial and some light industry on SR 56. Land use is not anticipated to change because of this project.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2(c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: Protecting Historic Properties: A Citizen's Guide to Section 106 Review available online at <http://www.achp.gov/citizensguide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains one resource listed in the National Register of Historic Places (NRHP), the Salem Downtown Historic District Indiana Historic Structures Survey (IHSSI) #s 175-567-26001-275. A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, four above-ground resources are recommended as eligible for listing in the NRHP: North Main Street Historic District, #s 175-567-27001-050; Warder and Alice Stevens House #175-567-G2; Schulz/Gladden House, #175-567-27029; and Ray and Hester Ault House, #175-567-G29.

With regards to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards will prepare an archaeological study for INDOT's review, which will be forwarded to the SHPO, and released on IN SCOPE (for tribal contacts only), upon INDOT approval.

The Historic Property Report is available for review online in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard copy of this material, please respond to this email with your request within seven (7) days.

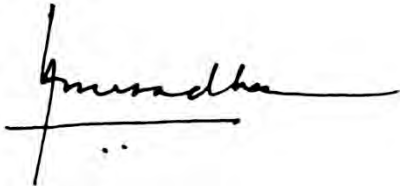
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Connie Zeigler of Green 3, LLC at (317) 634-4110 or e-mail to [connie@green3studio.com](mailto:connie@green3studio.com). All future responses regarding the proposed project should be forwarded to Green 3, LLC at the following address:

Connie Zeigler  
Architectural Historian/Historian  
Green 3 LLC  
1104 Prospect Street  
Indianapolis, IN 46203  
[connie@green3studio.com](mailto:connie@green3studio.com)

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317- 233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Sincerely,

A handwritten signature in black ink, appearing to read "Anuradha", with a horizontal line underneath and a vertical line extending downwards from the left side.

Anuradha V. Kumar, Manager  
Cultural Resources Office  
Environmental Services

Enclosures: **The Topographic Project Area Map was intentionally omitted from this letter. Please refer to page D-20 in this document.**  
Topographic Project Area Map  
Historic Properties Report (this report is on the INSCOPE site for those receiving this letter by email)

Distribution List:  
C: Indiana State Historic Preservation Officer  
Federal Highway Administration  
INDOT-CRO  
Indiana Landmarks, Southern Regional Office  
Mayor, City of Salem  
Washington County Historian  
Washington County Historical Society

Delaware Nation of Oklahoma  
Eastern Shawnee Tribe of Oklahoma  
Miami Tribe of Oklahoma  
Peoria Tribe of Indians of Oklahoma  
Pokagon Band of Potawatomi Indians  
Delaware Tribe of Indians, Oklahoma

## Connie Zeigler

---

**From:** Connie Zeigler  
**Sent:** Tuesday, February 27, 2018 11:21 AM  
**To:** Allen, Michelle (FHWA); jeremy.elliott9@outlook.com; info@johnhaycenter.org; south@indianalandmarks.org; joy.bierly@cityofsalemin.com  
**Cc:** Branigin, Susan; Miller, Shaun (INDOT)  
**Subject:** FHWA Project: Des. Nos. 1600873, 1600875, 1700168; Small Town Replacement and Bridge Project, Washington County, Indiana  
**Attachments:** Small Town Pavement Replacement and Bridge Project\_des1600873\_1600875\_170016\_ECL\_2018-2-27.pdf

**Des. No.:** 1600873, 1600875, 1700168

**Project Description:** Pavement Replacement and Bridge project

**Location:** Salem, Washington County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the Small Town Pavement Replacement and Bridge Project, Des. Nos. **1600873, 1600875, 1700168**

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Indiana State Historic Preservation Officer (automatic consulting party)  
Indiana Landmarks, Southern Regional Office  
Mayor, City of Salem  
Washington County Historian  
Washington County Historical Society

Delaware Nation of Oklahoma  
Eastern Shawnee Tribe of Oklahoma  
Miami Tribe of Oklahoma  
Peoria Tribe of Indians of Oklahoma  
Pokagon Band of Potawatomi Indians  
Delaware Tribe of Indians, Oklahoma

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Please review the letter [and any other document(s) currently available: HPR, archaeology report, etc.] located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party in the time allotted, the project will proceed consistent with

the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

Connie Zeigler  
Architectural Historian

[connie@green3studio.com](mailto:connie@green3studio.com)

**g r e e n 3**



**HISTORIC FONTANA SQUARE  
1104 PROSPECT STREET  
INDIANAPOLIS, IN 46203**

**p 317.634.4110  
f 866.477.2046 . .**

## Connie Zeigler

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**From:** Branigin, Susan <SBranigin@indot.IN.gov>  
**Sent:** Tuesday, February 27, 2018 12:44 PM  
**To:** bbarnes@estoo.net; dhunter@miamination.com; lpappenfort@peoriatribe.com; jason.wesaw@pokagonband-nsn.gov; kpenrod@delawarenation.com; lheady@delawaretribe.org  
**Cc:** 'michelle.allen@dot.gov'; Miller, Shaun (INDOT); Erin Mulryan; Connie Zeigler  
**Subject:** Des. Nos. 1600873, 1600875, 1700168: Small Town Replacement and Bridge Project, Washington County, Indiana  
**Attachments:** Small Town Pavement Replacement and Bridge Project\_des1600873\_1600875\_170016\_ECL\_2018-2-27.pdf

**Des. No.:** 1600873, 1600875, 1700168  
**Project Description:** Pavement Replacement and Bridge project  
**Location:** Salem, Washington County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the Small Town Pavement Replacement and Bridge Project, Des. Nos. **1600873, 1600875, 1700168**

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

- Indiana State Historic Preservation Officer (automatic consulting party)
- Indiana Landmarks, Southern Regional Office
- Mayor, City of Salem
- Washington County Historian
- Washington County Historical Society
  
- Delaware Nation of Oklahoma
- Eastern Shawnee Tribe of Oklahoma
- Miami Tribe of Oklahoma
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians
- Delaware Tribe of Indians, Oklahoma

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the letter [and any other document(s) currently available: HPR, archaeology report, etc.] located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party in the time allotted, the project will proceed consistent with



the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

**Susan R. Branigin**

*History Team Lead*

*Cultural Resources Office*

*Environmental Services*

100 N. Senate Ave., Rm. 642

Indianapolis IN 46204

**Office:** (317) 232-2969

**Email:** [sbranigin@indot.in.gov](mailto:sbranigin@indot.in.gov)





# Miami Tribe of Oklahoma

3410 P St. NW, Miami, OK 74354 • P.O. Box 1326, Miami, OK 74355  
Ph: (918) 541-1300 • Fax: (918) 542-7260  
www.miamination.com



February 27, 2018

Shaun Miller  
Archaeological Team Lead  
Cultural Resources Office  
Indiana DOT  
575 North Pennsylvania Street  
Indianapolis, IN 46204

Re: Des. Nos. 1600873, 1600875, 1700168: Small Town Pavement Replacement and Bridge Project, Washington County, Indiana – Comments of the Miami Tribe of Oklahoma

Dear Mr. Miller:

Aya, kikwehsitoole – I show you respect. My name is Diane Hunter, and I am the Tribal Historic Preservation Officer for the Federally Recognized Miami Tribe of Oklahoma. In this capacity, I am the Miami Tribe's point of contact for all Section 106 issues.

The Miami Tribe offers no objection to the above-mentioned project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, as this site is within the aboriginal homelands of the Miami Tribe, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery. In such a case, please contact me at 918-541-8966 or by email at [dhunter@miamination.com](mailto:dhunter@miamination.com) to initiate consultation.

The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project. In my capacity as Tribal Historic Preservation Officer I am the point of contact for consultation.

Respectfully,

Diane Hunter  
Tribal Historic Preservation Officer

Response from Indiana Landmarks, Southern Regional Office

March 12, 2018

Connie Zeigler  
Architectural Historian/Historian  
Green 3 LLC  
1104 Prospect Street  
Indianapolis, IN 46203

RE: Small Town Pavement Replacement and Bridge Project Des Nos. 1600873, 1600875, 1700168 -  
Salem, Washington County, Indiana

Dear Ms. Zeigler:

Thank you for your February 27 email and the information about this proposed project.

We have reviewed the information provided, and Indiana Landmarks concurs with the findings that the Warder and Alice Stevens House (#175-567-G2), the Schulz/Gladden House (#175-567-27029), the Ray and Hester Ault House (#175-567-G29), and the North Main Street Historic District (IHSSI #s 175-567-27001-050) are eligible for listing in the National Register of Historic Places.

Our office would like to be a consulting party for this project, and be provided additional information as the plans for the work are developed.

Thank you again for the opportunity to participate in this consultation process. Please don't hesitate to contact me should you have any questions or need any additional information.

Sincerely,

A handwritten signature in blue ink that reads "Laura Renwick". The signature is written in a cursive, flowing style.

Laura Renwick  
Community Preservation Specialist

---

**From:** "Miller, Shaun (INDOT)" <[smiller@indot.IN.gov](mailto:smiller@indot.IN.gov)>  
**Date:** Wednesday, March 14, 2018 at 10:31 AM  
**To:** Erin Mulryan <[erin@green3studio.com](mailto:erin@green3studio.com)>  
**Cc:** "Branigin, Susan" <[SBranigin@indot.IN.gov](mailto:SBranigin@indot.IN.gov)>  
**Subject:** FW: Des. Nos. 1600873, 1600875, 1700168: Small Town Replacement and Bridge Project, Washington County, Indiana

Erin,

Please see below response from the Delaware Nation regarding subject project.

Thank you,

Shaun Miller  
Archaeological Team Lead  
INDOT, Cultural Resources Office  
[smiller@indot.in.gov](mailto:smiller@indot.in.gov)  
(317) 233-6795

---

**From:** Kimberly Penrod [<mailto:kpenrod@delawarenation.com>]  
**Sent:** Wednesday, March 14, 2018 9:59 AM  
**To:** Branigin, Susan <[SBranigin@indot.IN.gov](mailto:SBranigin@indot.IN.gov)>  
**Subject:** RE: Des. Nos. 1600873, 1600875, 1700168: Small Town Replacement and Bridge Project, Washington County, Indiana

**\*\*\*\* This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. \*\*\*\***

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Susan,

The protection of our tribal cultural resources and tribal trust resources will take all of us working together. We look forward to working with you and your agency. With the information you have submitted we can concur at present with this proposed plan.

As with any new project, we never know what may come to light until work begins. The Delaware Nation asks that you keep us up to date on the progress of this project and if any discoveries arise please contact us immediately.

Our department is trying to go as paper free as possible. If it is at all feasible for your office to send email correspondence we would greatly appreciate.

If you need anything additional from me please do not hesitate to contact me.

*Respectfully,*

*Kim Penrod  
Delaware Nation  
Director, Cultural Resources/106  
Archives, Library and Museum*

31064 State Highway 281  
PO Box 825  
Anadarko, OK 73005  
(405)-247-2448 Ext. 1403 Office  
(405)-924-9485 Cell  
[kpenrod@delawarenation.com](mailto:kpenrod@delawarenation.com)

*Unless someone like you cares a whole awful lot, nothing is going to get better. It's not. ~Dr. Seuss*

CONFIDENTIALITY NOTE:

This e-mail (including attachments) may be privileged and is confidential information covered by the Electronic Communications Privacy Act 18 U.S.C. 2510-2521 and any other applicable law, and is intended only for the use of the individual or entity named herein. If the reader of this message is not the intended recipient, or the employee or agent responsible to deliver it to the intended recipient, you are hereby notified that any retention, dissemination, distribution or copying of this communication is strictly prohibited. Although this e-mail and any attachments are believed to be free of any virus or other defect that might affect any computer system in to which it is received and opened, it is the responsibility of the recipient to ensure that it is virus free and no responsibility is accepted by Delaware Nation or the author hereof in any way from its use. If you have received this communication in error, please immediately notify us by return e-mail. Thank you.

---

**From:** Branigin, Susan [<mailto:SBranigin@indot.IN.gov>]  
**Sent:** Tuesday, February 27, 2018 11:44 AM  
**To:** [bbarnes@estoo.net](mailto:bbarnes@estoo.net); [dhunter@miamination.com](mailto:dhunter@miamination.com); [lpappenfort@peoriatribe.com](mailto:lpappenfort@peoriatribe.com); [jason.wesaw@pokagonband-nsn.gov](mailto:jason.wesaw@pokagonband-nsn.gov); Kimberly Penrod <[kpenrod@delawarenation.com](mailto:kpenrod@delawarenation.com)>; [lheady@delawaretribe.org](mailto:lheady@delawaretribe.org)  
**Cc:** 'michelle.allen@dot.gov' <[michelle.allen@dot.gov](mailto:michelle.allen@dot.gov)>; Miller, Shaun (INDOT) <[smiller@indot.IN.gov](mailto:smiller@indot.IN.gov)>; Erin Mulryan <[erin@green3studio.com](mailto:erin@green3studio.com)>; Connie Zeigler <[connie@green3studio.com](mailto:connie@green3studio.com)>  
**Subject:** Des. Nos. 1600873, 1600875, 1700168: Small Town Replacement and Bridge Project, Washington County, Indiana

**Des. No.:** 1600873, 1600875, 1700168  
**Project Description:** Pavement Replacement and Bridge project  
**Location:** Salem, Washington County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the Small Town Pavement Replacement and Bridge Project, Des. Nos. **1600873, 1600875, 1700168**

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

- Indiana State Historic Preservation Officer (automatic consulting party)
- Indiana Landmarks, Southern Regional Office
- Mayor, City of Salem
- Washington County Historian

Memo to File

Date: March 14, 2018

From: Connie Zeigler

Phone Log of conversation between John Kiser, Washington County Historical Society and Connie Zeigler, Green 3, LLC

Mr. Kiser left a message and I called him back. He wanted to ask how best to respond to the Early Coordination Letter in respect to being sure that we are aware of the stone walls, hitching posts and stone steps in the APE.

I told him that, to my knowledge, those elements occur only in the North Main Street Historic District and that they are considered contributing elements, just as a house is a contributing resource. So they will be considered when we begin to look at effects.

He asked about the brick sidewalks, and I told him that, as of right now, there are not available plans so we don't yet know what will be done, but that it was likely at least some sidewalks will be affected since they are barely passable in some areas. He told me that some of the brick sidewalks are maintained and are at least 100 years old. I agreed with him.

He asked what we needed from him at this point. I asked him to respond by email or surface mail to say that he wants to be a consulting party. He can include information about historic elements in his letter/email if he wishes or he can just say that he wants to be a consulting party and that will ensure that he continues to receive the documents and can respond to them and that he will be invited to a consulting party meeting if there is one held.

## Connie Zeigler

---

**From:** Connie Zeigler  
**Sent:** Thursday, March 15, 2018 3:53 PM  
**To:** 'Jeremy Elliott'  
**Subject:** RE: FHWA Project: Des. Nos. 1600873, 1600875, 1700168; Small Town Replacement and Bridge Project, Washington County, Indiana

Jeremy,

Thank you for your response. I will be sure to note in the file that both you and the Washington County Historical Society want to be consulting parties. That will ensure that you receive all correspondence and documentation moving forward. And that you will be invited to a consulting party if it appears that might be an adverse effect on historic properties.

Please tell residents to NOT remove any of those features. They are considered contributing to the North Main Street Historic District. No work will take place until after we have the plans and know for sure what will be done with the sidewalks and streets. Once we know that, we will draft an effects letter and then most likely meet in Salem to discuss the project with consulting parties. If people start removing those features, even for safekeeping, then they are no longer considered contributing to the district, and the protections of the historic district's eligibility won't apply to them. There will be plenty of time to know what might happen before any work commences. I have already noted the steps and hitching posts in the historic property report and will be addressing them as we move forward.

Don't hesitate to call or email if you have further questions. Thank you for accepting consulting party status.

Connie Zeigler  
Architectural Historian

[connie@green3studio.com](mailto:connie@green3studio.com)



---

**From:** Jeremy Elliott <jeremy.elliott9@outlook.com>  
**Sent:** Thursday, March 15, 2018 11:15 AM  
**To:** Connie Zeigler <connie@green3studio.com>  
**Subject:** Re: FHWA Project: Des. Nos. 1600873, 1600875, 1700168; Small Town Replacement and Bridge Project, Washington County, Indiana

Hello Connie,

Due to the historical significance of Salem's North Main Street/Hwy. 135, both myself and the Washington County Historical Society would like to respectfully ask to be included in the consulting process of this improvement project. Personally, I am interested in the preservation of the old hitching-posts and carriage steps along this route. Several residents have contacted me asking if I knew if they should plan to remove their old posts prior to the state project. If you could please inform us of any public meetings over this project or any proposed plans for the restoration of historical artifacts along the route(s), it would be appreciated.

Thank you for the notification,  
Jeremy Elliott  
Washington County Historian  
The John Hay Center  
307 E. Market St.  
Salem, IN. 47167  
(812) 883-6495

On Feb 27, 2018, at 11:20 AM, Connie Zeigler <[connie@green3studio.com](mailto:connie@green3studio.com)> wrote:

**Des. No.: 1600873, 1600875, 1700168**  
**Project Description: Pavement Replacement and Bridge project**  
**Location: Salem, Washington County, Indiana**

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the Small Town Pavement Replacement and Bridge Project, Des. Nos. **1600873, 1600875, 1700168**

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Indiana Landmarks, Southern Regional Office  
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Washington County Historian  
Washington County Historical Society

Delaware Nation of Oklahoma  
Eastern Shawnee Tribe of Oklahoma  
Miami Tribe of Oklahoma  
Peoria Tribe of Indians of Oklahoma  
Pokagon Band of Potawatomi Indians  
Delaware Tribe of Indians, Oklahoma

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Please review the letter [and any other document(s) currently available: HPR, archaeology report, etc.]located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource



impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party in the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

Connie Zeigler  
Architectural Historian

[connie@green3studio.com](mailto:connie@green3studio.com)

<image001.png>

<Small Town Pavement Replacement and Bridge Project\_des1600873\_1600875\_170016\_ECL\_2018-2-27.pdf>



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739  
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



March 21, 2018

Connie Zeigler  
Green 3, LLC  
1104 Prospect Street  
Indianapolis, Indiana 46203

Federal Agency: Indiana Department of Transportation (“INDOT”),  
on behalf of Federal Highway Administration (“FHWA”)

Re: Historic property report (Zeigler, 1/30/2018) for pavement replacement along SR 135 and SR 56  
and replacement of Bridge No. 056-88-01478 in Salem, Washington County, Indiana  
(Des. No. 160073, 1600875, 1700168; DHPA No. 22240)

Dear Ms. Zeigler:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, the, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding that Implementation of the Federal Aid Highway Program In the State of Indiana,” we have reviewed the information dated February 27, 2018 and received on February 28, 2018 for the pavement and bridge replacement project in Salem, Washington Township of Washington County.

We are not aware of any parties who should be invited to participate in this Section 106 consultation, beyond those whom INDOT has already invited.

The area of potential effects (“APE”) identified in the historic property report appears to be of adequate size to encompass the geographic area in which direct or indirect effects could occur.

In regard to buildings and structures, for purposes of this Section 106 review, we concur with the consultant’s assessments of eligibility and ineligibility for the National Register of Historic Places. We agree that the Salem Downtown Historic District is listed on the National Register, and that the following properties appear to meet the criteria of eligibility: North Main Street Historic District (IHSSI Site # 175-567-27001-050); Schulz/Gladden House at 505 N. Main Street (IHSSI Site #175-567-27029), located within the North Main Street Historic District; Warder and Alice Stevens House at 1201 N. Main Street (Green3 # 175-567-G2); and Ray and Hester Ault House at 1116 W. Mulberry Street (Green3 #175-567-G29).


Additionally, we agree that no other properties within the area of potential effects appear to be eligible for inclusion in the National Register of Historic Places. We also note that within the project area, Washington County Bridge No.056-88-01478 (NBI No.20180) was previously determined not eligible for the National Register in the Indiana Historic Bridge Inventory (Mead & Hunt, 2009); the Indiana SHPO concurs with that assessment.

The above comments are strictly in regards to historic structures, since no information has been provided thus far regarding archaeology.

The reviewers of this project on the Indiana SHPO staff are Wade Tharp for archaeology and Chad Slider for structures. If you have questions about the status of a review, about the review process, or about what to submit for review, please contact the assigned reviewer at INDOT’s Cultural Resources Office.

In all future correspondence regarding the pavement and bridge replacement project in Salem, Washington Township of Washington County (Des. Nos. 160073, 1600875, 1700168), please refer to DHPA No. 22240.

Very truly yours,



Mitchell K. Zoll  
Deputy State Historic Preservation Officer

MKZ:CWS:MKZ:cws

emc: Anuradha Kumar, INDOT  
Shaun Miller, INDOT  
Susan Branigin, INDOT  
Mary Kennedy, INDOT  
Connie Zeigler, Green3



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739  
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



October 1, 2018

Susan Castle  
Metric Environmental  
6971 Hillsdale Court  
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation ("INDOT"),  
on behalf of Federal Highway Administration ("FHWA")

Re: Indiana archaeological short report (Jackson, 08/14/2018), for pavement replacement project along SR 135 and SR 56 and replacement project of Bridge No. 056-88-01478, in the City of Salem, in Washington Township, Washington County, Indiana (Des. Nos. 1600073, 1600874, and 1700168; DHPA No. 22240)

Dear Ms. Castle:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO staff" or "INDNR-DHPA") has reviewed the Indiana archaeological short report (Jackson, 08/14/2018), which was submitted along with your August 30, 2018, review request submittal form, both of which we received on August 31, 2018.

Based on the submitted information and documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the proposed project area; and it is our opinion that no further archaeological investigations appear necessary at this proposed project area. However, this identification is subject to the project activities remaining within areas disturbed by previous construction of a recent and non-historical nature. If archaeological deposits are encountered from the post-contact period, they will be evaluated regarding their eligibility for the NRHP in consultation with the staff of the Indiana SHPO. Please contact our office if such deposits are encountered. The archaeological recording must be done in accordance with the Secretary of the Interior's "Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716) and a report of the archaeological documentation must be submitted to our office for review and comment.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The archaeological reviewer for this project on the Indiana SHPO staff is Wade Tharp, and the structures reviewer is Chad Slider. If you have questions about the status of our review of a submission, about the kind of information to submit, or about a procedural issue, please contact an INDOT Cultural Resources staff member who is assigned to this project.

Susan Castle  
October 1, 2018  
Page 2

If there is any future correspondence regarding the pavement replacement project along SR 135 and SR 56 and the replacement project of Bridge No. 056-88-01478, in the City of Salem, in Washington Township, Washington County, Indiana (Des. Nos. 1600073, 1600874, and 1700168), please refer to DHPA No. 22240.

Very truly yours,



Christopher A. Smith  
Deputy Director  
Indiana Department of Natural Resources

CAS:WTT:wt

emc: Robert Dirks, PE, FHWA  
Michelle Allen, FHWA  
Anuradha Kumar, INDOT  
Shaun Miller, INDOT  
Susan Branigin, INDOT  
Mary Kennedy, INDOT  
Shirley Clark, INDOT  
Connie Zeigler, Green 3, L.L.C.  
Christopher Jackson, Green 3, L.L.C.  
Chad W. Slider, Indiana SHPO staff  
Wade T. Tharp, Indiana SHPO staff



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

April 11, 2019

This letter was sent to the listed parties.

Re: Small Town Pavement Replacement and Bridge Project Des Nos. 1600873, 1600875, 1700168, (DHPA No. 22240) Salem, Washington County, Indiana– Effects Letter

Dear Consulting Party,

Pursuant to Section 106 of the National Historic Preservation Act (1966) and 36 CFR Part 800 (Revised January 2001 and incorporating amendments effective August 5, 2004), federal agencies are required to take into account the effects of their undertakings on historic and archaeological properties. The Indiana Department of Transportation (INDOT) with funding from the Federal Highway Administration (FHWA) plans to proceed with the Small Town Pavement Replacement and Bridge Project (Des. Nos. 1600873, 1600875, 1700168). Green 3, LLC is under contract with INDOT to advance the environmental documentation for the referenced project.

The project consists of three parts. Pavement Replacement Project along SR 56 (Des. No. 1600873) will consist of a pavement replacement, storm sewers, new curb, gutter and sidewalks along SR 56. The vertical alignment is being modified in order to best fit the existing corridor with some grade changes anticipated. The project is proposed to provide continuous sidewalk along SR 56 from the east end of the project to the extent sidewalk (Spring Grove Road). Continuous sidewalk will be provided along one side of the roadway, and a graded grass area on the other side of the roadway. The project includes replacing the existing SR 56 bridge over Brock Creek (Des. No. 1700168). The new bridge is proposed to be a 2-span (45 feet each), haunched, reinforced concrete slab structure that will match the existing bridge in total length. A minor profile grade will occur at the structure to satisfy the hydraulic and geometric requirements. The waterway opening will increase slightly from the existing since the waterway area blocked by the arches will be eliminated. The typical roadway section at the bridge will consist of two 12-foot travel lanes with a two-foot curb offset (north side), a four-foot shoulder (south side), and a six-foot six-inch sidewalk along the north side. The railings are open tube type that will provide a view up and down the stream. The proposed clear roadway width will be 30 feet and the total out-to-out bridge width will be 38 feet, 6 inches. Pavement Replacement Project along SR 135 (Des. No. 1600875) will consist of milling 1.5" of HMA and replacing with a 1.5" HMA surface course and reconstructing curb ramps.

Permanent right-of-way acquired is anticipated to be 0.32 acre on SR 56 (31 parcels) and 0.1 acre (one parcel) along SR 135. Total temporary right-of-way will be 0.47 acre on SR 56 and 0.14 acre on SR 135. Temporary right-of-way will be required in the North Main Street Historic District in two locations. Please see Appendix E for preliminary plan sheets.

On February 27, 2018, an early coordination letter was distributed to potential consulting parties inviting them to participate in the Section 106 process and review the Historic Property Report (HPR; Zeigler, 1/30/2018) for this project. The HPR recognized that one resource in the project's area of potential effects (APE) is listed on the National Register of Historic Places (NRHP): the Salem Downtown Historic District, Indiana Historic Structures Survey (IHSSI) #s 175-567-26001-275. Four resources were recommended eligible for the NHRP: the North Main Street Historic District, IHSSI #s 175-567-27001-050; the Schulz/Gladden House, #175-567-27029, which is within the North Main Street Historic District and also recommended individually eligible; and two resources that were not previously surveyed: the Warder and Alice Stevens House #175-567-G2 and the Ray and Hester Ault House, #175-567-G29.

The State Historic Preservation Officer (SHPO) staff concurred with the recommendations of the HPR in a letter dated March 21, 2018, stating that “we concur with the consultant’s assessments of eligibility and ineligibility for the National Register of Historic Places.”

Other consulting party comments on the HPR are included below. Copies of all consulting party correspondence are found in CP Correspondence attached.

On February 27, 2018, the Miami Tribe of Oklahoma responded affirmatively to the consulting party invitation by email.

On March 12, 2018, Indiana Landmarks, Southern Regional Office, responded by letter accepting consulting party status and agreeing with the eligibility recommendations of the HPR.

On March 14, 2018, John Kiser, of the Washington County Historical Society, phoned Green 3 to be sure that the consultant was aware of stone walls, hitching posts, stone steps and brick sidewalks in the area.

On March 14, 2018, the Delaware Nation of Oklahoma responded by email concurring with the proposed plan and accepting consulting party status.

On March 15, 2018, the Washington County historian confirmed by email that he and the Washington County Historical Society would like to be consulting parties. The historian mentioned interest in the preservation of hitching posts and carriage steps along the route and asked to be notified of any public meetings over the project or any proposed plans for the restoration of “historical artifacts along the route(s).” He noted that: “Several residents have contacted me asking if I knew if they should plan to remove their old posts prior to the state project.” Green 3’s historian responded by email on the same date stating “Please tell residents NOT to remove any of those features. They are considered contributing to the North Main Street Historic District. No work will take place until after we have the plans and know for sure what will be done with the sidewalks and streets.”

A Phase Ia archaeological investigation (Jackson August 14, 2018) was completed and submitted to the SHPO and distributed to tribes via INSCOPE on August 30, 2018. The report located no archaeological sites within the project area and recommended that the project be allowed to proceed as planned. No further archaeological work was recommended. The SHPO staff replied to the archaeological report in a letter dated October 1, 2018, stating that “we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places (NRHP) within the project area; it is our opinion that no further archaeological investigations appear necessary at the proposed project area.”

No other consulting comments were received.

### **Project Effects**

According to CFR 800.5(a)(1), “an adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property’s location, design, setting, materials, workmanship, feeling, or association.”

In respect to the Ray and Hester Ault House, there will be NO Adverse Effect to the historic property. While the project will be visible from the property, the project will not require right-of-way or have any direct impacts at this location. The slight changes in viewshed that may be created from pavement replacement adjacent to the property and/or driveway reconstruction on the opposite side of SR 56 will not in any way impact the integrity of this property. See plan sheet 2 in Maps and Plans attached.

**Ray and Hester Ault House.** Per 800.5(a)(2)(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property;” there will be no right-of-way acquired from the property.

Per 36 CFR 800.5(a)2(ii), the undertaking will not cause the “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines.”

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the “Removal of the property from its historic location.”

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in the “Change of the character of the property’s use or physical features within the property’s setting that contribute to its historic significance.” There will be no direct impacts and the project will have little to no impact on the property’s setting, which has always been adjacent to a state highway. Project elements, such as the reconstruction of a driveway on the opposite side of SR 56 and the repaving of SR 56 will not change any features that contribute to its historic significance.

Per 36 CFR 800.5(a)2(v), the undertaking will not result in the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The project will not introduce elements that diminish the historic architecture or materials of the house, which make it eligible for the NRHP.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...”

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...”

In respect to the Salem Downtown Historic District, there will be No Adverse Effect to the historic property as only incidental construction may occur along SR 56 adjacent to the district in front of one property—a modern church with parking lot – which is not a contributing resource in the historic district and there are no other elements that contribute to the district (such as historic sidewalks) at this location. See plan sheet 4 in Maps and Plans attached.

**Salem Downtown Historic District.** Per 800.5(a)2(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property;” there will be no physical damage to the Salem Downtown Historic District as a result of this project. No right-of-way will be taken from the property boundary.

Per 36 CFR 800.5(a)2(ii), the undertaking will cause no “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines.”

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the “Removal of the property from its historic location.”

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in the “Change of the character of the property’s use or physical features within the property’s setting that contribute to its historic significance.”

Per 36 CFR 800.5(a)2(v), the undertaking will not result in the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The project will only be visible from properties that are considered non-contributing to the historic district.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...”

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...”

In respect to the North Main Street Historic District, there will be No Adverse Effect to the historic property. A small amount of temporary right-of-way will be required at two locations where brick will be removed from sidewalk and curb ramp sections for construction of ADA-compliant ramps within the district. The project will occur only on the existing



roadway and at specific sidewalk and curb locations and will not impact historic hitching posts, tree lawns, or historic curbs. The project will not impact historic sidewalks at any location other than those described below. At the northeast corner of Main and Hackberry: approximately 266 square feet of bricks will be removed from the sidewalk within the right-of-way to construct new ADA-compliant ramps. A two-foot section of ADA Detectable Warning Surface (see Detectable Warning Surface photo on page DWS-1 attached). will be installed in the new ramp, the rest of the brick will be relaid if possible or new brick that matches the existing in size and color will be laid if the existing brick is too damaged to reuse at this location. The new curb will be concrete, as is the existing curb; the new ramp will be constructed entirely with brick. An existing brick wall on the property at this location will not be affected. The brick will be relaid in the same pattern as it is currently. It should be noted that most of the bricks at this location were previously removed and bricks were re-laid as part of an INDOT project in 1999. At that time a sidewalk ramp was constructed at this location. The 1999 project plans (see attached pages) called for the brick to be removed, salvaged and reinstalled. However, the existing bricks are laid in a different pattern than the rest of the historic sidewalk and they appear a slightly different color than the rest of the sidewalk; that may be due to exposure of a different brick face when the sidewalk was relaid, or perhaps to replacement of some damaged brick with different bricks at the time of construction. This project work will occur adjacent to a contributing property.

At the intersection of Main Street and Salem Avenue, approximately two feet on each side of the intersection in each quad will be replaced with ADA Detectable Warning Surface (bricks) at all four quadrants. On the east side of Main, temporary right-of-way will be needed for the construction; no temporary or permanent right-of-way will be needed on the west side of Main. Although there will be a minor loss of these brick sidewalk sections (approximately two feet in each quad), and a two-foot section of limestone edging at two of the locations, as an entire unit the sidewalk in the historic district has already lost integrity due to the replacement of many sections of sidewalk with numerous non-historic materials, including asphalt, pavers, new bricks, and long sections of concrete. The rest of the existing historic brick sidewalk (which comprises a slight majority of the entire sidewalk) will remain intact. This small loss will not constitute an adverse effect to the district.

Project commitments will include the reinstallation of bricks, either reusing the existing bricks, if they are reusable or installing bricks that match the existing bricks in color and size at the corner of SR 135/North Main and SR 56 (Hackberry), excepting the location where two-feet of ADA Detectable Warning Surface is used, in order to minimize impacts. Project commitments will also include avoiding impacting all hitching posts and limestone steps that are in North Main Street Historic District or replacing any impacted element back in its current location.

**North Main Street Historic District.** Per 800.5(a)(2)(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property;” there will be no physical damage to the North Main Street Historic District as a result of this project. Small sections of brick sidewalk will be removed and replaced with modern brick at the intersections of Salem and SR 135 and the sidewalk at the corner of Salem and SR 135 will be removed and then relaid with the existing or matching brick, excepting the two-foot section of ADA Detectable Warning Surface; however, the sidewalk on the east side of SR 135 as a whole has already undergone many alterations and these small changes will not rise to the level of adverse effect. See attached plan sheets 6 and 7.

Per 36 CFR 800.5(a)2(ii), the undertaking will cause no “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines.” The installation of ADA-compliant ramps at the sidewalk intersection of Salem Avenue and SR 135 will be consistent with the Secretary’s Standards of Rehabilitation and with the guidance as per Preservation Brief 32 “Making Historic Properties Accessible,” which states the following: “when new features are incorporated for accessibility, historic materials and features should be retained whenever possible. Accessibility modifications should be in scale with the historic property, visually compatible, and, whenever possible, reversible. . . The design of new features should also be differentiated from the design of the historic property so that the evolution of the property is evident.” At the intersection of SR 135 and SR 56 the historic bricks will be reused wherever possible and any necessary newer brick will match in color and size the existing bricks. The new ADA-compliant Detectable Warning Surface that will be added at the curb ramp and at the sidewalk intersection at Salem Avenue and SR 135, although modern brick, will be visually compatible

with the brick sidewalks where it will be added, but also differentiated from the historic design with the use of new brick in the warning surface.

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the “Removal of the property from its historic location.”

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in the “Change of the character of the property’s use of or physical features within the property’s setting that contribute to its historic significance,” as only small sections of the sidewalk will be changed and the project commitments will include replacement of the existing bricks at the intersection of SR 135 and SR 56 with the existing bricks where possible or with bricks in the same color and size. The only noticeable change will be at the locations of the two-foot wide modern brick Detectable Warning Surface for the ramp at SR 135 and SR 56 and at the two-foot wide sections at the four corners of Salem Avenue and SR 135. These changes are small compared to the many altered sections of sidewalk that already exist.

Per 36 CFR 800.5(a)2(v), the undertaking will not result in the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” Only small sections brick in the already altered sidewalk will be changed. The bricks at the corner of SR 135 and SR 56 will be relaid in the same pattern as currently exists and will, therefore, look similar. Overall the visual changes will be so minor that the integrity of the district will not be diminished.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...”

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...”

In respect to the Schulz/Gladden House, there will be No Adverse Effect to the historic property. There will be no direct impacts and little if any visual impact from the street pavement replacement. There is no historic brick sidewalk near the house and no sidewalk work will occur near the concrete sidewalk or the stonewall that is on the property side of the sidewalk. No other contributing features will be affected by the pavement replacement. See plan sheet 9 in Maps and Plans attached.

**Schulz/Gladden House.** Per 800.5(a)2(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property.” There will be no direct impacts to the property.

Per 36 CFR 800.5(a)2(ii), the undertaking will not cause “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines.”

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the “Removal of the property from its historic location.”

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in the “Change of the character of the property’s use of or physical features within the property’s setting that contribute to its historic significance.” There will be no direct impacts and no impacts to historic physical features of the house or its setting as the only project element that will occur adjacent to the house will be the replacement of the street pavement.

Per 36 CFR 800.5(a)2(v), the undertaking will not result in the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” While the street pavement will be replaced in front of the house, this should cause little, if any, visual changes to the property and none that would impact its significant historic features.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...”

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...”

In respect to the Warder and Alice Stevens House, there will be No Adverse Effect to the historic property as the project will not have any direct impact at this location. Nor will it affect the viewshed from the house or any features that contribute to the property's significance. The project may be visible from the front lawn of the property where it is adjacent to SR 135, but due to the nature of the project, which is the replacement of road surface, the visual effects won't impact the historic features or setting of the property.

**Warder and Alice Stevens House.** Per 800.5(a)(2)(i), the undertaking will not result in the "Physical destruction of or damage to all or part of the property;" there will be no right-of-way acquired from the property and no direct impacts. See plan sheets 11 and 12 in Maps and Plans attached.

Per 36 CFR 800.5(a)2(ii), the undertaking will not cause "Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties and/or other applicable guidelines."

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the "Removal of the property from its historic location."

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in the "Change of the character of the property's use of or physical features within the property's setting that contribute to its historic significance." There will be no direct impacts on the property nor any change in its setting that will impact its historic integrity.

Per 36 CFR 800.5(a)2(v), the undertaking will not result in the "Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features." The project will be barely visible, if at all, from the house and not highly visible from the front yard and the nature of the project, replacement of street pavement, will not introduce a visual change that will impact the property's significance in any way.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the "Neglect of a property which causes its deterioration..."

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the "Transfer, lease, or sale of property out of Federal ownership or control..."

Efforts to minimize the effects of the project will include the replacement of brick with existing or similar brick at the intersection of SR 135 and SR 56, aside from a two-foot section of Detectable Warning Surface. At this intersection the existing brick will be reused if and where possible. If any bricks must be replaced, bricks of the same color and size will be used. The bricks will be relaid to match the existing pattern at this intersection. Project commitments will include the replacement of brick with the same or matching brick in the same pattern at the intersection of SR 135 and SR 56. Commitments will also include that historic hitching posts and limestone steps within the North Main Street Historic District will not be disturbed.

We look forward to comment and input from consulting parties regarding the effects of the project on the North Main Street Historic District. Consulting parties may respond to this letter by email, phone or standard mail. Consulting parties may request a consulting party meeting, during which project details, historic properties and effects will be discussed.

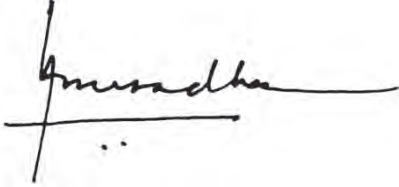
Please review the information and comment within thirty (30) calendar days of receipt. For questions concerning specific project details, you may contact Connie Zeigler of Green 3 at 317-634-4110 or [connie@Green3studio.com](mailto:connie@Green3studio.com). All future responses regarding the proposed project should be forwarded to Green 3 at the following address:

Connie Zeigler  
Architectural Historian  
Green 3, LLC  
1104 Prospect St.  
Indianapolis, IN 46203

[connie@Green3studio.com](mailto:connie@Green3studio.com)

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317- 233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager  
Cultural Resources Office  
Environmental Services

Enclosures:

Consulting Party Correspondence  
Historic Property Boundary Maps and plan sheets showing boundaries  
1999 INDOT Plan Sheets  
Images of locations where curb ramps will be constructed  
Preliminary Plans

Distribution List:

SHPO  
FWHA  
Indiana Landmarks, Southern Regional Office  
Washington County Historian  
Washington County Historical Society  
Delaware Nation of Oklahoma  
Miami Tribe of Oklahoma

## Connie Zeigler

---

**From:** Connie Zeigler  
**Sent:** Thursday, April 11, 2019 3:48 PM  
**To:** Slider, Chad (DNR); Tharp, Wade; Jeremy Elliott; info@johnhaycenter.org; south@indianalandmarks.org  
**Cc:** Ross, Anthony; Kumar, Anuradha; Branigin, Susan; Erin Mulryan; Karen Wood  
**Subject:** FHWA Project: Des. No. 1600873, 1600875, 1700168; Small Town Pavement Replacement Project, Salem, Washington County, Indiana

**Des. No.:** 1600873, 1600875, 1700168

**Project Description:** Small Town Pavement Replacement Project

**Location:** SRs 56 and 135, Salem, Washington County Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the Small Town Pavement Replacement and Bridge Project (Des. Nos. 1600873, 1600875, 1700168).

As part of Section 106 of the National Historic Preservation Act, an effects letter and plans have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

Connie Zeigler  
Architectural Historian

[connie@green3studio.com](mailto:connie@green3studio.com)

g r e e n • 3



Historic Fountain Square  
1104 Prospect Street  
Indianapolis, IN 46203

P 317.634.4110  
F 866.477.2046 . .

## Connie Zeigler

---

**From:** Ross, Anthony <ARoss3@indot.IN.gov>  
**Sent:** Friday, April 12, 2019 8:25 AM  
**To:** 'dhunter@miamination.com'; kpenrod@delawarenation.com  
**Cc:** Connie Zeigler; Miller, Shaun (INDOT); Kumar, Anuradha; Branigin, Susan; Karen Wood; Erin Mulryan; Prince, Greg; Dye, David; Allen, Michelle (FHWA)  
**Subject:** FHWA Project: Des. No. 1600873, 1600875, 1700168; Small Town Pavement Replacement Project, Salem, Washington County, Indiana

**Des. No.:** 1600873, 1600875, 1700168

**Project Description:** Small Town Pavement Replacement Project

**Location:** SRs 56 and 135, Salem, Washington County Indiana

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Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

**Anthony Ross, Ph.D.**

*Historian*

*Cultural Resources Office*

*Environmental Services*

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

**Office:** (317) 234-0142

**Email:** [aross3@indot.in.gov](mailto:aross3@indot.in.gov)



**\*\* Historic Property Report (HPR) guidelines can be found [here](#)**



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739  
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



May 13, 2019

Connie Zeigler  
Green 3, LLC  
1104 Prospect Street  
Indianapolis, Indiana 46203

Federal Agency: Indiana Department of Transportation (“INDOT”),  
on behalf of Federal Highway Administration (“FHWA”)

Re: Additional project information for pavement replacement along SR 135 and SR 56 and  
replacement of Bridge No. 056-88-01478 in Salem, Washington County, Indiana  
(Des. No. 160073, 1600875, 1700168; DHPA No. 22240)

Dear Ms. Zeigler:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, the, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding that Implementation of the Federal Aid Highway Program In the State of Indiana,” we have reviewed the information dated April 11, 2019 and received on April 16, 2019 for the pavement and bridge replacement project in Salem, Washington Township of Washington County.

In terms of archaeology, we previously stated that there are no currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places (“NRHP”) within the proposed project area; and it is our opinion that no further archaeological investigations appear necessary at this proposed project area. However, this identification is subject to the project activities remaining within areas disturbed by previous construction of a recent and non-historical nature. If archaeological deposits are encountered from the post-contact period, they will be evaluated regarding their eligibility for the NRHP in consultation with the staff of the Indiana SHPO. Please contact our office if such deposits are encountered. The archaeological recording must be done in accordance with the Secretary of the Interior’s “Standards and Guidelines for Archaeology and Historic Preservation” (48 F.R. 44716) and a report of the archaeological documentation must be submitted to our office for review and comment.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to DNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

In regard to buildings and structures, we note that the Washington County Bridge No.056-88-01478 (NBI No.20180) was previously determined not eligible for the National Register. We previously agreed with the consultant’s assessment that the Salem Downtown Historic District (NR-1345), North Main Street Historic District (IHSSI Site # 175-567-27001-050) which includes the Schulz/Gladden House at 505 N. Main Street (IHSSI Site #175-567-27029); Warder and Alice Stevens House at 1201 N. Main Street (Green3 # 175-567-G2); and Ray and Hester Ault House at 1116 W. Mulberry Street (Green3 #175-567-G29) are the only historic resources within the area of potential effects for the undertaking.

Thank you for providing the effects analysis and project plans for review. It is our understanding that no right of way will be required of the Ray and Hester Ault House, Warder and Alice Stevens House or the Salem Downtown Historic District, and that

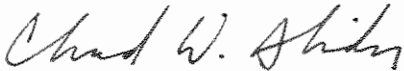
these properties will not be adversely affected by project activities. However, there will be minor impacts within the North Main Street District to include permanent loss of some brick and limestone for installation of curb ramps. The minimal alteration appears to be acceptable in consideration of INDOT's proposed project commitments to protect historic features, replace any removed or damaged brick in-kind, and avoid and (as necessary) reinstall the hitching posts and limestone steps. Regarding the Schulz/Gladden House, which is located within the North Main Street Historic District and is also considered individually eligible for the National Register, it will not be directly impacted by project activities.

Based on the plans and information provided, we agree that the proposed project does not appear to adversely affect the integrity of the National Register-listed or -eligible properties within the APE. Unless another consulting party expresses concerns about the project's effects, it might now be appropriate for INDOT to make a finding of effect for this undertaking.

The reviewers of this project on the Indiana SHPO staff are Wade Tharp for archaeology and Chad Slider for structures. If you have questions about the status of a review, about the review process, or about what to submit for review, please contact the assigned reviewer at INDOT's Cultural Resources Office.

In all future correspondence regarding the pavement and bridge replacement project in Salem, Washington Township of Washington County (Des. Nos. 160073, 1600875, 1700168), please refer to DHPA No. 22240.

Very truly yours,



Beth K. McCord  
Deputy State Historic Preservation Officer

BKM:CWS:cws

emc: Anuradha Kumar, INDOT  
Shaun Miller, INDOT  
Susan Branigin, INDOT  
Mary Kennedy, INDOT  
Connie Zeigler, Green3



# Small Town Pavement Replacement and Bridge Project

## HISTORIC PROPERTY REPORT

Salem, Washington Township, Washington County, Indiana

Des. Nos. 1600873, 1600875, 1700168



January 30, 2018

Prepared for: Metric Environmental  
6971 Hillside Court  
Indianapolis, IN 46250

By:

Connie Zeigler  
Green3 LLC  
Historic Fountain Square  
1104 Prospect Street  
Indianapolis, IN 46203



p. 317.634.4110 f. 866.422.2046 connie@green3studio.com

## Executive Summary

The Indiana Department of Transportation (INDOT) with funding from the Federal Highway Administration (FHWA) and plan to proceed with the Small Town Pavement Replacement and Bridge Project (Des. Nos. 1600873, 1600875, 1700168). The project is located adjacent on State Road (SR) 135 and SR 56 in Salem, Washington Township, Washington County, Indiana.

The area of potential effects (APE) of the project includes all properties adjacent to the project and those with a proximate viewshed of the project. Development and the limited nature of the project restricted the APE to properties directly adjacent to the project.

Connie Zeigler, a qualified professional who meets the Secretary of the Interior Standards, evaluated properties in the APE and prepared the Historic Property Report. There is one property listed on the National Register of Historic Places (NRHP) in the APE: the Salem Downtown Historic District (listed 1996), Indiana Historic Structures Survey (IHSSI) #s 175-567-26001-275. Four resources are recommended eligible for the NHRP in this report. One of these was identified in the *Washington County Interim Report* as the North Main Street Historic District, IHSSI #s 175-567-27001-050. The Schulz/Gladden House, #175-567-27029, which is within the North Main Street Historic District and previously rated Notable is recommended eligible. Two resources that were not previously surveyed: the Warder and Alice Stevens House #175-567-G2 and the Ray and Hester Ault House, #175-567-G29.



**A Phase Ia Archaeological Investigation for the Proposed Small Town Pavement Replacement and Bridge Project (Des. 1600873, 1600875, and 1700168) on State Roads 56 and 135 in the City of Salem, Washington Township, Washington County, Indiana**  
ARCHAEOLOGICAL SHORT REPORT

August 14, 2018

Prepared for:  
Metric Environmental  
6971 Hillside Court  
Indianapolis, Indiana 46250



Christopher Jackson, M.S., RPA  
Archaeologist, Historian/QP  
Green3 LLC  
Historic Fountain Square  
1104 Prospect Street  
Indianapolis, IN 46203

p. 317.634.4110

f. 866.422.2046

e. [chris@green3studio.com](mailto:chris@green3studio.com)

the State Road 56 section of the project area that is located in Section 18 was prior to the Second World War was rural in nature with only a couple of homesteads/farmsteads situated along the road (D.J. Lake and Company 1878; Indiana Highway Survey Commission 1936; U.S. Post Office Department 1910). The northern city limits has basically stayed the same.

A review of the historic aerial photographs indicated that the western growth of the city occurred during the 1960s and 1970s. Prior to 1960, the western section of the project area was rural with very little commercial and/or residential development.

Known Cultural Manifestations and/or Additional Information:

An examination of the local and regional archaeological records indicates that this area has been utilized throughout prehistory. One of the most prominent sites is the Ana Lynn site (12-Ws-284), a Late Prehistoric village site that is situated east of Salem. In regards to the project area, the data indicates that this area would have been more heavily utilized during the Archaic period of prehistory.

**FIELD INVESTIGATION:** (check all that apply)

Field Investigation Date(s) (month, day, year): July 27, 2018

Field Supervisor: Christopher Jackson

Field Crew: Jennifer Carroll

Surface Visibility: 0 percent

Factors Affecting Visibility: commercial and residential development

Visual Walkover  Pedestrian Survey  Shovel Test  Screened  Mesh Size

Interval 5 m  10 m  15 m  Other (describe below)

Number of Shovel Test Units Excavated: 0

Describe Methods: The methods utilized by this investigation entailed visual inspection. Visual inspection consisted of visually inspecting all of the project area and determining visually undisturbed areas, or those areas that had the potential for intact/undisturbed soils. In those areas that were visually disturbed, photographs were taken. If any area had been encountered that was either undisturbed or had the possibility for intact soils, shovel probes would have been excavated.

Attach photographs documenting disturbances below

Describe Disturbances: existing right-of-way, drainage ditches, man-made landforms, commercial development, residential development, existing sidewalks

Comments:

**RESULTS**

- Archaeological records check has determined that the project area does not have the potential to contain archaeological resources.
- Archaeological records check has determined that the project area has the potential to contain archaeological resources.
- Phase Ia reconnaissance has located no archaeological resources in the project area.
- Phase Ia reconnaissance has identified landforms conducive to buried archaeological deposits.

Actual Area Surveyed hectares:  acres:

Nearly all of the project area is situated within the existing right-of-way (see attached construction drawings). Visual examination of the right-of-way indicated that it is disturbed via the existing highways/roads, drainage ditches, curbs, etc.

Visual inspection of those areas that were located outside of the existing right-of-way indicated that they have been disturbed and thus have an extremely limited possibility for archaeological resources. The disturbance entailed drives or man-made landforms.

Because no areas were encountered that either were visually undisturbed or had the possibility for intact soils, no shovel probes were excavated, and no sites were documented.

Comments:

### RECOMMENDATION

- The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.
- The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.
- The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.
- The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.
- The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

Other Recommendations/Commitments:

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

### Attachments

- Figure showing project location within Indiana.
- USGS topographic map showing the project area (1:24,000scale).
- Aerial photograph showing the project area, land use and survey methods.
- Photographs of the project area.
- Project plans (if available)

Other Attachments:

References Cited: Adderley, Anthony W.  
2013 Archaeological Literature Review and Field Reconnaissance Short Report: Proposed Salem High School Spoil Site for INDOT Contract R-31768 in Washington County, Indiana. Access Cultural and Environmental Solutions. Submitted to Dave O'Mara Contractors.

ATTACH COPY OF ADVERTISEMENT HERE

Prescribed by State Board of Accounts

General Form No. 99P (Rev. 2009A)

Indiana Dept. of Transportation

To: The Salem Leader

Washington County, Indiana.

PUBLISHER'S CLAIM

LINE COUNT

Display Master (Must not exceed two actual lines, neither of which shall total more than four solid lines of the type in which the body of the advertisement is set) -- number of equivalent lines	.....
Head -- number of lines	.....
Body -- number of lines	<b>114</b> .....
Tail -- number of lines	.....
Total number of lines in notice	<b>114</b> .....

COMPUTATION OF CHARGES

<b>114</b> lines, ..... columns wide equals ..... equivalent lines at <b>.3636</b> cents per line	<b>\$41.45</b> .....
Additional charges for notices containing rule or tabular work (50 per cent of above amount)	.....
Charge for extra proofs of publication (\$1.00 for each proof in excess of two)	.....
<b>TOTAL AMOUNT OF CLAIM</b>	<b>\$41.45</b> .....

Width of single column in picas **7p10** Size of type ..... **7** point.

Number of insertions **1**.....

Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width and type size, which was duly published in said paper **1**..... times. The dates of publication being as follows:

**July 2**

Additionally, the statement checked below is true and correct:

- ..... Newspaper does not have a Web site.
- ..... Newspaper has a Web site and this public notice was posted on the same day as it was published in the newspaper.
- ..... Newspaper has a Web site, but due to technical problem or error, public notice was posted on .....
- ..... Newspaper has a Web site but refuses to post the public notice.

Date: **July 2, 2019**.....

NANCY GROSSMAN  
Title: PUBLISHER

**Public Notice  
Des. No. 1600873,  
1600875, 1700168**

The Indiana Department of Transportation (INDOT) is planning to undertake a pavement replacement project funded in part by the Federal Highway Administration. The proposed undertaking is located on State Roads (SR) 135 and 56 in Salem, Washington Township, Washington County, Indiana.

The project consists of three parts. The Pavement Replacement Project along SR 56 (Des. No. 1600873) will consist of a pavement replacement, storm sewer replacement, and installation of new curbs, gutters, sidewalks, and curb ramps in compliance with the Americans with Disabilities Act (ADA). The project includes replacing the existing SR 56 bridge over Brock Creek (Des. No. 1700168). The Pavement Replacement Project along Main St./SR 135 (Des. No. 1600875) will consist of milling 1.5 inches of Hot Mix Asphalt and replacing with 1.5 inches of HMA surface course and reconstructing curb ramps.

Permanent right-of-way acquired is anticipated to be 0.32 acre on SR 56 and 0.1 acre along SR 135. Total temporary right-of-way will be 0.47 acre on SR 56 and 0.14 acre on SR 135.

Properties listed in or eligible for the National Register of Historic Places (NRHP) located within the Area of Potential Effects (APE) include Salem Downtown Historic District, Warder and Alice Stevens House (1201 N. Main Street), North Main Street Historic District, Schulz/Gladden House (505 North Main Street), Ray and Hester Ault House (1116 W. Mulberry Street).

The proposed action impacts properties listed in or eligible for the NRHP. The Indiana Department of Transportation (INDOT), on behalf of the FHWA, has issued a "No Adverse Effect" finding for the project because the project will not diminish the integrity of the characteristics that qualify the historic properties within the APE for inclusion in the NRHP. In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(2), the documentation specified in 36 CFR 800.11(e) is available for inspection in the office of Green 3, LLC. Additionally, this documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents>. This documentation serves as the basis for the "No Adverse Effect" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Connie Zeigler, Green 3, LLC, 1104 Prospect Street, Indianapolis, IN 46203; 317-908-6046; [connie@green3studio.com](mailto:connie@green3studio.com) no later than August 1, 2019.

In accordance with the "Americans with Disabilities Act", if you have a disability for which INDOT needs to provide accessibility to the document(s) such as interpreters or readers, please contact Rickie Clark at 317-232-6601 or [rclark@indot.in.gov](mailto:rclark@indot.in.gov).

L27c1 hspaxlp

## Susan Castle

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**Subject:** FW: FHWA Project: Des. No. 1600873, 1600875, and 1700168 ; Small Town Pavement Replacement and Bridge Project on State Roads 56 and 135, Washington County, Indiana

---

**From:** Moffatt, Charles D

**Sent:** Thursday, October 24, 2019 4:11 PM

**To:** Larry Heady <[lheady@delawaretribe.org](mailto:lheady@delawaretribe.org)>; [thpo@estoo.net](mailto:thpo@estoo.net); Diane Hunter <[dhunter@miamination.com](mailto:dhunter@miamination.com)>; 'lpappenfort@peoriatribe.com' <[lpappenfort@peoriatribe.com](mailto:lpappenfort@peoriatribe.com)>; [Matthew.Bussler@pokagonband-nsn.gov](mailto:Matthew.Bussler@pokagonband-nsn.gov); [dkelly@delawarenation-nsn.gov](mailto:dkelly@delawarenation-nsn.gov); [ethompson@delawarenation-nsn.gov](mailto:ethompson@delawarenation-nsn.gov)

**Cc:** Miller, Shaun (INDOT) <[smiller@indot.IN.gov](mailto:smiller@indot.IN.gov)>; Allen, Michelle (FHWA) <[michelle.allen@dot.gov](mailto:michelle.allen@dot.gov)>

**Subject:** FHWA Project: Des. No. 1600873, 1600875, and 1700168 ; Small Town Pavement Replacement and Bridge Project on State Roads 56 and 135, Washington County, Indiana

Des. No.: 1600873, 1600875, and 1700168  
Project Description: Small Town Pavement Replacement and Bridge Project on State Roads 56 and 135  
Location: Salem, Washington Township, Washington County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the Small Town Pavement Replacement and Bridge Project on State Roads 56 and 135, Washington County, Indiana; Des. No. 1600873, 1600875, and 1700168

Notification in an email about this project was sent on 2/27/2018 and the archaeological report has been prepared was posted within IN SCOPE in August 2018, However, I failed to notify you about the report at that time. As consulting parties the Delaware Nation and Miami Tribe of Oklahoma received notice of an effects letter the finding in separate emails (4/12/2019 and 6/27/2019).

The project is located within the town of Salem, and the Archaeologists found the area it be completely disturbed. If you wish to review this documentation it is located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). Please provide by comment Monday, October 28.

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

I apologize for my oversight and the short notice,

David Moffatt  
Archaeologist  
Environmental Services  
Cultural Resources Office  
Indiana Department of Transportation  
317-233-3703



**APPENDIX E**  
**Red Flag and Hazardous Materials**



# INDIANA DEPARTMENT OF TRANSPORTATION

## *Driving Indiana's Economic Growth*

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204-2216 (317) 232-5348 FAX: (317) 233-4929

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

Date: August 27, 2018

To: Site Assessment & Management  
Environmental Services  
Indiana Department of Transportation  
100 N Senate Avenue, Room N642  
Indianapolis, IN 46204

From: Kennita Jones  
Metric Environmental  
6971 Hillside Court  
Indianapolis, Indiana  
Kennitaj@metricenv.com

Re: RED FLAG INVESTIGATION  
Des. No. 1600873, Road & Sidewalk Reconstruction, State Road (SR) 56  
Des. No. 1600875, Mill and Resurface, SR 135  
Des. No. 1700168, Bridge Replacement, Bridge No. 056-88-01478, SR 56 over Brock Creek  
Salem, Washington County, Indiana

### **NARRATIVE**

The City of Salem, a Local Public Agency (LPA), is proposing to proceed with a small town reconstruction project along SR 56 and SR 135, Salem, Washington County, Indiana, Sections 8, 13, 17, and 18, Township 2 North, Range 4 East.

The proposed project will include the removal and replacement of pavement, sidewalks and curbs and establish new storm water drainage patterns along SR 56, from 0.9 mile east of West Junction of SR 60 to 0.8 mile east of SR 135 (High Street). Sidewalks replaced along SR 56 will have Americans with Disabilities Act (ADA) compliant ramps at public road intersections. Curb ramps will be excavated and installed at a maximum depth of approximately 1 foot below ground surface (bgs). Storm sewers being replaced along SR 56 will be excavated to a depth ranging between 3 feet and 18 feet below ground surface (bgs). Only resurfacing will be conducted on SR 56 from Water Street to High Street. The proposed improvements along SR 135 from SR 56 to 0.72 mile north of SR 56 includes mill and resurface. The bridge on SR 56 over Brock Creek will be replaced. The total project limits is approximately 2.17 miles. It is anticipated that less than 10 acres of right-of-way will be required.

The SR 56 bridge (Bridge ID#: 056-88-01478) is a concrete cast-in-place, two lane arch bridge structure constructed in 1933 with a maximum span of 45 feet and a structure length of 95 feet. The SR 56 bridge replacement over Brock Creek will consist of the complete removal and replacement of the three span arch structure. The proposed new footprint will be similar in size to the existing structure. According to the most recent bridge inspection dated September 15, 2015, the bituminous wearing surface of the bridge has numerous cracks. The curb and copings have exposed rebar and the drains are clogged. The arches have wide cracks and are scaling with efflorescence. There is cracking, scaling and efflorescence in all the spandrel walls. The west side abutment footing is exposed, and the bridge

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seats are cracking. Pier cap #2 has cracking, scaling and spalling with exposed rebar. Complete replacement of the structure is anticipated to prolong the use of this structure another 75 years.

Due to the tight urban conditions and full pavement replacement, it is recommended to close SR 56 and SR 135 to through traffic. The SR 56 detour would consist of US 150, and SR 135 (south leg, including the Salem Bypass). The SR 135 detour would consist of SR 56, SR 39, and SR 250. These state detours would not be in effect concurrently. Preliminary coordination with the city of Salem has helped identify local detours to use during construction. During the design process, the Designer will detail possible segments for construction for both SR 56 and SR 135 that ensure access, while maintaining constructability.

## SUMMARY

<b>Infrastructure</b>			
Indicate the number of items of concern found within 0.5 mile and an explanation why each item within the 0.5-mile search radius will/will not impact the project. If there are no items, please indicate N/A:			
Religious Facilities	<b>14</b>	Recreational Facilities	<b>4</b>
Airports	<b>1</b>	Pipelines	<b>1</b>
Cemeteries	<b>6</b>	Railroads	<b>1</b>
Hospitals	<b>1</b>	Trails	<b>8</b>
Schools	<b>4</b>	Managed Lands	<b>2</b>

Religious Facilities: Fourteen religious facilities are located within the 0.5-mile search radius. Two features are mapped adjacent to the project area, Assembly of God and The Church of Jesus Christ of Latter-day Saints, a review of Google Maps shows that both features are located outside of the 0.5-mile search radius. Therefore, the closest confirmed religious facility is the Christian Life Baptist Church which is located adjacent to the western project limits on SR 56. Coordination with Christian Life Baptist Church will occur.

Airports: One public airport was identified within 3.8 miles of the project area. The airport Salem Municipal is located approximately 0.9 miles southwest of the project area. Coordination with INDOT Aviation will occur.

Cemeteries: Six cemeteries are depicted within the 0.5-mile search radius; however upon further review, it appears as though only one cemetery is located within the 0.5-mile search radius. The cemetery, Crown Hill Cemetery is located 0.28 mile south of the project area. No impact is expected.

Hospitals: One hospital is located within the 0.5-mile search radius. The facility, Washington County Memorial Hospital, is depicted approximately 0.16 mile north of the project area; however upon further review, the facility is physically located approximately 0.34 mile north of the project area. No impact is expected.

Schools: Four schools are located within the 0.5-mile search radius. The closest feature, Salem High School, is located approximately 0.20 miles north of the project area. No impact is expected.

Recreations Facilities: Four recreational facilities are located within the 0.5-mile search radius. The closest facility, Depaw Park, is owned by the Washington County Commissioners and is located approximately 0.09 miles south of the project area. No impact is expected.

Pipelines: One pipeline is located within the 0.5-mile search radius. The pipeline, owned by Midwest Natural Gas Corp, carries natural gas. The pipeline intersects the project area. Coordination with INDOT Utilities and Railroads will occur.

Railroads: One railroad segment is located within the 0.5-mile search radius. Louisville & Nashville Railroad is located approximately 0.08 mile west of the project area. No impact is expected.

Trails: Eight trail segments are located within the 0.5-mile search radius. The following two (2) potential trails are located in the project area. Coordination with the Salem Park and Recreation Board will occur.

- Salem Community Trail extension south from schools, is a potential urban trail managed by the Salem Park and Recreation Board that intersects the project area at Posey Street.
- Salem Community Trail Courthouse, Fairgrounds, Community Park Trail, is a potential asphalt/concrete urban trail managed by the Salem Park and Recreation Board that intersects and runs adjacent to the project area at SR 135.

Managed Lands: Two managed lands are located within the 0.5-mile search radius. The closest managed land, Christian Church Playground, is managed by the Salem Park Board and is located approximately 0.22 miles southeast of the project area. No impact is expected.

<b>Water Resources</b>			
Indicate the number of items of concern found within 0.5 mile and an explanation why each item within the 0.5-mile search radius will/will not impact the project. If there are no items, please indicate N/A:			
NWI - Points	<b>1</b>	NWI - Wetlands	<b>6</b>
Karst Springs	<b>N/A</b>	IDEM 303d Listed Lakes	<b>N/A</b>
Canal Structures – Historic	<b>N/A</b>	Lakes	<b>4</b>
NWI - Lines	<b>7</b>	Floodplain - DFIRM	<b>28</b>
IDEM 303d Listed Rivers and Streams (Impaired)	<b>N/A</b>	Cave Entrance Density	<b>3</b>
Rivers and Streams	<b>16</b>	Sinkhole Areas	<b>N/A</b>
Canal Routes - Historic	<b>N/A</b>	Sinking-Stream Basins	<b>N/A</b>
Urbanized Area Boundary (UAB)	<b>1</b>		

NWI Points: One NWI point is identified within the 0.5-mile search radius. The NWI point is located approximately 0.17 miles north of the project area. No impact is expected.

NWI Lines: Seven NWI Lines are identified within the 0.5-mile search radius. The closest NWI line intersects the project area. A Waters of the U.S. Report will be prepared and coordination with INDOT ES Ecology and Waterway Permitting will occur.

Rivers and Streams: Sixteen river and stream segments are identified within the 0.5-mile search radius. The closest feature, Brock Creek intersects the project area. A Waters of the U.S. Report will be prepared and coordination with INDOT ES Ecology and Waterway Permitting will occur.

Urbanized Area Boundary (UAB): This project lies within the Washington County, Indiana UAB. Post construction Storm Water Quality Best Management Practices (BMPs) may need to be considered. An early coordination letter with topographic and aerial maps showing the project area should be sent to the Soil and Water Conservation Department.

NWI – Wetlands: Six NWI wetlands are located within the 0.5-mile search radius. The closest feature is located approximately 0.07 mile north of the project area. No impact is expected.

Lakes: Four lakes are located within the 0.5-mile search radius. The closest feature is located approximately 0.20 miles north of the project area. No impact is expected.

Floodplains: Twenty-eight floodplain features are located within the 0.5-mile search radius. The closest feature, classified as AE, the 100-year floodplain intersects the project area. Coordination with INDOT ES Ecology and Waterway Permitting will occur.

Cave Entrance Density: Three cave entrance density areas are located within the 0.5-mile search radius. One cave entrance density location intersects the project area. Coordination with INDOT Ecology and Waterway Permitting will occur.

<b>Mining/Mineral Exploration</b>			
Indicate the number of items of concern found within 0.5 mile and an explanation why each item within the 0.5-mile search radius will/will not impact the project. If there are no items, please indicate N/A:			
Petroleum Wells	<b>6</b>	Mineral Resources	<b>N/A</b>
Mines – Surface	<b>N/A</b>	Mines – Underground	<b>N/A</b>

Petroleum Wells: Six petroleum wells are located within the 0.5-mile search radius. One well, operated by Duncan and operated by Morris, is a presumed plugged well located adjacent to the project area. Coordination with IDNR Oil and Gas Division will occur.

<b>Hazardous Material Concerns</b>			
Indicate the number of items of concern found within 0.5 mile and an explanation why each item within the 0.5-mile search radius will/will not impact the project. If there are no items, please indicate N/A:			
Brownfield Sites	<b>2</b>	Restricted Waste Sites	<b>N/A</b>
Corrective Action Sites (RCRA)	<b>N/A</b>	Septage Waste Sites	<b>N/A</b>
Confined Feeding Operations	<b>1</b>	Solid Waste Landfills	<b>N/A</b>
Construction Demolition Waste	<b>N/A</b>	State Cleanup Sites	<b>N/A</b>
RCRA Generators	<b>4</b>	Tire Waste Sites	<b>N/A</b>
Infectious/Medical Waste Sites	<b>N/A</b>	Waste Transfer Stations	<b>N/A</b>
Lagoon/Surface Impoundments	<b>N/A</b>	RCRA Waste Treatment, Storage, and Disposal Sites (TSDs)	<b>N/A</b>
Leaking Underground Storage Tanks (LUSTs)	<b>9</b>	Underground Storage Tanks (USTs)	<b>18</b>
Manufactured Gas Plant Sites	<b>N/A</b>	Voluntary Remediation Program	<b>N/A</b>
NPDES Facilities	<b>2</b>	Superfund	<b>N/A</b>
NPDES Pipe Locations	<b>2</b>	Institutional Control Sites	<b>1</b>
Open Dump Sites	<b>N/A</b>		

Brownfields Sites: Two brownfields sites are located within the 0.5-mile search radius. The closest Brownfields site is discussed below.

- Salem City (AI ID#:60805, Regulatory ID#: 4030018) is located at 108 High Street, approximately 0.22 miles southeast of the project area. According to the Indiana Department of Environmental Management Virtual Filing Cabinet (IDEM VFC), a Phase II conducted at the site in 2003 revealed soils onsite impacted with total petroleum hydrocarbons (TPH) at concentrations below the applicable IDEM RISC screening criteria. Arsenic was detected in one soil samples at a concentration above applicable IDEM remediation closure

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guide (IDEM RCG) screening levels (SLs). Volatile organic compounds (VOCs) were encountered at concentrations below applicable IDEM RCG screening criteria. Additional information indicating any additional subsurface investigations conducted since 2003, were not encountered during this investigation. No impact is expected.

Confined Feeding Operations: One confined feeding operation is located within the 0.5-mile search radius. The facility (AI ID#:60900, Regulatory ID#: 3342) 1307 North State Road 135, is located approximately 0.21 mile north of the project area. No impact is expected.

RCRA Generators: Four RCRA generator facilities are located within the 0.5-mile search radius. The closest facility, Graves Body Shop (AI ID#: 58627, Regulatory ID#: IND981193956) is located at 201 South Water Street, approximately 0.21 mile south of the project area. According to the IDEM VFC, the facility updated their waste handler status to Conditionally Exempt Small Quantity (CESQG) in 2002. Additional documentation on the facility was not encountered during this investigation. No impact is expected.

Underground Storage Tanks: Eighteen UST sites are located within the 0.5-mile search radius. Of the eighteen, seven (7) of the sites are located adjacent to the project area and are summarized below.

- Markers Marathon (AI ID#: 59377, Regulatory ID#: 14337), located at 400 North Main Street, adjoins the project area along SR 56. According to the IDEM VFC, three (3) USTs were removed from the site in August of 1991. Confirmatory soil samples collected at the site were non-detect for benzene, toluene, ethylbenzene, and xylenes (BTEX). No impact is expected.
- Simpson's Service (AI ID#: 59191, Regulatory ID#: 20682) located adjoining the project area along SR 56 at 301 West Hackberry Street. See the LUST Section below for site summary and recommendation.
- City Garage (AI ID#: 60127, Regulatory ID#:12913) located adjoining the project area along SR 56 at Mulberry & Cauble Street. According to the IDEM VFC, one (1) 2,000-gallon UST was removed from the site in March 2001. Confirmatory soil sample analytical results identified TPH concentrations, which were below applicable IDEM residential direct contact SLs. Groundwater was not encountered and was not sampled. Additional information indicating any spills, or leaks associated with the facility was not located during this investigation. No impact is expected.
- Abandoned facility (Regulatory ID#: 0731021UR) adjoins the project area along SR 56. A review of the IDEM VFC database with this facility name and regulatory ID number did not yield any documentation. No impact is expected.
- Casey's General Store (AI ID#: 60969, Regulatory ID#: 24586) 1216 Mulberry Street adjoins the project area along SR 56. According to the IDEM VFC, the most recent Underground Storage Tank Report, dated June 3, 2016, listed two (2) registered fiberglass USTs at the site. The facility was found to be in compliance with operating, maintenance, and financial responsibility requirements set forth in Indiana's UST Rule 329 IAC 9. No impact is expected.
- Cooper's Transmission Service (AI ID#: 60190, Regulatory ID#: 17526) 1117 West Mulberry Street adjoins the project area along SR 56. According to the IDEM VFC, two (2) USTs were removed from the site in 1991. Confirmatory soil analytical results identified TPH concentrations, which were below applicable IDEM RCG SLs. The USTs were installed in 1968. In addition to petroleum contamination, it is likely that lead could be encountered within the soil and/or groundwater. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary.
- Eddie Gilstrap Motors Incorporated (AI ID#: 58913, Regulatory ID#: 7670) 1008 West Mulberry Street adjoins the project area along SR 56. According to the IDEM VFC, four USTs were registered at the site in 1986. Information regarding the current status of the USTs, or any spills or releases associated with the USTs, was not located during this investigation. A Phase II Environmental Site Assessment (ESA) is recommended.

Leaking Underground Storage Tanks (LUSTs): Nine LUST facilities are identified within the 0.5-mile search radius. Of the nine, five (5) facilities are mapped adjacent to the project area.

- An unnamed facility (Regulatory ID#: 17787) adjoins the project area along SR 56 at 301 West Hackberry Street. A review of the IDEM VFC identified the facility as Collette Family Market/Simpson's Service. According IDEM VFC files, three (3) USTs were removed from the site in 2009. Confirmatory soil sample analytical results identified benzene concentrations above applicable IDEM RCG soil migration to groundwater SLs. All other COCs were below laboratory detection limits or below applicable IDEM RCG SLs. Impacted soil at the facility appeared to be confined to the former UST pit. A release was reported to IDEM and the facility was assigned incident # 201001518. IDEM issued a NFA for the facility in a letter dated January 26, 2010. No impact is expected.
- Salem Tobacco Road (AI ID#: 58985, Regulatory ID#:6588) adjoins the project area along SR 56 at 1305 West Mulberry Street. According to the IDEM VFC, the facility was previously a filling station. Since 2006 several subsurface investigations and monitoring activities have been conducted at the site. The most recent quarterly groundwater monitoring report, dated November 17, 2017, reported concentrations of benzene and naphthalene above the applicable IDEM RCG residential groundwater screening criteria. Residual adsorbed and dissolved chemicals of concern (CoCs) remain on-site along the property boundary and have not been fully delineated. It is likely that residual petroleum CoCs extend into SR 56. If excavation occurs in this area, proper removal and disposal of soil and/or groundwater will be necessary.
- Swifty #142 / Sunoco/ LLB3 Food Mart, LLC. (AI ID#: 11342, Regulatory ID#: 238) adjoins the project area along SR 56 at 306 North Main Street. According to the IDEM VFC files, four USTs consisting of two (2) 10,000-gallon gasoline, one (1) 12,000 gallon gasoline, and one (1) 6,000 gallon diesel UST have been registered at the site since 1990. In 2000, a subsurface investigation measured organic vapors with a photoionization detector above 100 parts per million. The most recent inspection, conducted July 7, 2016, reported the facility as nonoperational. According to the inspection report, the four USTs were still present, the facility had no power, no leak detection or inventory control activities were being conducted, and no corrosion protection records were available. IDEM and the State of Indiana have filed a legal case against the facility in an Agreed Order dated July 13, 2017. A Phase II ESA is recommended.
- Crystal Flash Petroleum #28 (AI ID#: 60170) 311 North Main Street. Although the icon associated with the site is mapped approximately 0.05 mile south of the project area, the site is actually located on the southwest corner of SR 56 and SR 135 and is currently operating as a CVS Pharmacy. According to the IDEM VFC, the IDEM issued a No Further Action Approval Determination Pursuant to IDEM RISC Guidance letter, dated January 26, 2017. It appears as though samples collected from soil borings and monitoring wells along the property boundary, and adjoining the project area, were below detection limits. No impact is expected.
- Simpsons Service (AI ID#: 59191, Regulatory ID#: 17787) adjoins the project area along SR 56 at 301 West Hackberry Street. According to the IDEM VFC files, three (3) USTs were removed from the site in 2009. Confirmatory soil sample analytical results identified concentrations of benzene above applicable IDEM RCG soil migration to groundwater SLs. All other COCs were either below laboratory detection limits or applicable IDEM RCG SLs. Impacted soil encountered at the facility appeared to be confined to the former UST pit. A release was reported to IDEM and the facility was assigned incident # 201001518. IDEM issued an NFA for the facility in a letter dated January 26, 2010. No impact is expected.

NPDES Facilities: Two NPDES facilities are located within the 0.5-mile search radius. The closest facility, Jean Extrusions Inc. (Permit ID#: IN0001589) is located approximately 0.23 mile south of the project area at 201 Jeans Road. No impact is expected.

NPDES Pipe Locations: Two NPDES pipe locations are located within the 0.5-mile search radius. The closest facility, Hanover Waste Water Treatment Plant (Unique ID#: IN0020702002Z, NPDES ID#: IN0020702) is located approximately 0.11 mile south of the project area. No impact is expected.

Institutional Control Site: One institutional control site is located within the 0.5-mile search radius. The facility, Tobacco Road (FID #:18762) is located approximately 0.40 mile south of the project area at 308 South Main Street. According to the IDEM VFC files, an Environmental Restrictive Covenant (ERC) was recorded on the property deed on November 14, 2006. No impact is expected.

## **Ecological Information**

The Washington County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is attached with ETR species highlighted. Coordination with USFWS and IDNR will occur.

Due to the nature of project activities, this project will fall under the guidelines set forth under USFWS Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013. No further coordination is necessary.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The project area is located in a predominately residential area. The September 15, 2015 inspection report for Bridge #056-88-01478 contains no information about whether bats are present or absent in (or on) the bridge. Additional investigation to confirm the presence or absence of bats in on the bridge will be necessary. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects" dated October 25, 2017.

An inquiry using the USFWS Information for Planning and Consultation (IPaC) website did not indicate the presence of the federally endangered species, the Rusty Patched Bumble bee, in or within 0.5 mile of the project area. No impact is expected.

## **RECOMMENDATIONS**

**INFRASTRUCTURE**: The Salem Municipal is a public airport located outside the 0.5-mile search radius but is situated within 3.8 miles of the project area. Coordination with INDOT Aviation will occur.

Pipelines: One pipeline which carries natural gas intersects the project area. Coordination with INDOT Utilities and Railroads will occur.

The presence of the following infrastructure facilities will require coordination with the Salem Park and Recreation Board.

- Salem Community Trail extension south from schools, is a potential urban trail managed by the Salem Park and Recreation Board that intersects the project area at Posey Street.
- Salem Community Trail Courthouse, Fairgrounds, Community Park Trail, is a potential asphalt/concrete urban trail managed by the Salem Park and Recreation Board that intersects and runs adjacent to the project area at SR 135.

**WATER RESOURCES**: The project lies within the Salem, Indiana UAB. Coordination with the City of Salem MS4 Coordinator will occur.

The presence of the following water resources will require the preparation of a Waters of the U.S. Report and coordination with INDOT ES Ecology and Waterway Permitting:

- NWI line intersects the project area.
- River and stream segment, Brock Creek intersects the project area.
- Floodplain classified as AE, the 100-year floodplain intersects the project area.



- One cave entrance density area intersects the project area.

MINING/MINERAL EXPLORATION: The presence of the following mining/mineral exploration feature will require coordination with IDNR Oil and Gas Division:

- One well, operated by Duncan and operated by Morris, is a presumed plugged well located adjacent to the project area.

HAZMAT CONCERNS: The presence of the following hazardous material concerns will require additional investigation:

- Cooper’s Transmission Service (AI ID#: 60190, Regulatory ID#: 17526) 1117 West Mulberry Street adjoins the project area along SR 56. According to the IDEM VFC, two (2) USTs were removed from the site in 1991. Confirmatory soil analytical results identified TPH concentrations, which were below applicable IDEM RCG SLs. The USTs were installed in 1968. In addition to petroleum contamination, it is likely that lead could be encountered within the soil and/or groundwater. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary.
- Eddie Gilstrap Motors Incorporated (AI ID#: 58913, Regulatory ID#: 7670) 1008 West Mulberry Street adjoins the project area along SR 56. According to the IDEM VFC, four USTs were registered at the site in 1986. Information regarding the current status of the USTs, or any spills or releases associated with the USTs, was not located during this investigation. A Phase II Environmental Site Assessment (ESA) is recommended.
- Salem Tobacco Road (AI ID#: 58985, Regulatory ID#:6588) adjoins the project area along SR 56 at 1305 West Mulberry Street. According to the IDEM VFC, the facility was previously a filling station. Since 2006 several subsurface investigations and monitoring activities have been conducted at the site. The most recent quarterly groundwater monitoring report, dated November 17, 2017, reported concentrations of benzene and naphthalene above the applicable IDEM RCG residential groundwater screening criteria. Residual adsorbed and dissolved chemicals of concern (CoCs) remain on-site along the property boundary and have not been fully delineated. It is likely that residual petroleum CoCs extend into SR 56. If excavation occurs in this area, proper removal and disposal of soil and/or groundwater will be necessary.
- Swifty #142 / Sunoco/ LLB3 Food Mart, LLC. (AI ID#: 11342, Regulatory ID#: 238) adjoins the project area along SR 56 at 306 North Main Street. According to the IDEM VFC files, four USTs consisting of two (2) 10,000-gallon gasoline, one (1) 12,000 gallon gasoline, and one (1) 6,000 gallon diesel UST have been registered at the site since 1990. In 2000, a subsurface investigation measured organic vapors with a photoionization detector above 100 parts per million. The most recent inspection, conducted July 7, 2016, reported the facility as nonoperational. According to the inspection report, the four USTs were still present, the facility had no power, no leak detection or inventory control activities were being conducted, and no corrosion protection records were available. IDEM and the State of Indiana have filed a legal case against the facility in an Agreed Order dated July 13, 2017. A Phase II ESA is recommended.

ECOLOGICAL INFORMATION: Coordination with USFWS and IDNR will occur. Additional investigation to confirm the presence or absence of bats in on the bridge will be necessary. Preparation of the Scoping Worksheet for the Range-Wide Programmatic Information Consultation for Indiana Bat and Northern Long-Eared Bat will be required. If applicable, preparation of the Project Submittal Form for the Range-wide Programmatic Consultation for Indiana Bat and Northern Long-Eared Bat will be required.

CULTURAL RESOURCES: Due to the scope, full Section 106 Coordination will be necessary. Coordination will occur with INDOT ES Cultural Resources.

INDOT Environmental Services concurrence: Nicole Fohey-Breting August 28, 2018  
 (Signature)

Prepared by:  
Kennita Jones  
Environmental Geologist  
Metric Environmental

**Graphics:**

A map for each report section with a 0.5-mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

GENERAL SITE MAP SHOWING PROJECT AREA: YES

INFRASTRUCTURE: YES

WATER RESOURCES: YES

URBANIZED AREA BOUNDARY: YES

MINING/MINERAL EXPLORATION: YES

HAZMAT CONCERNS: YES

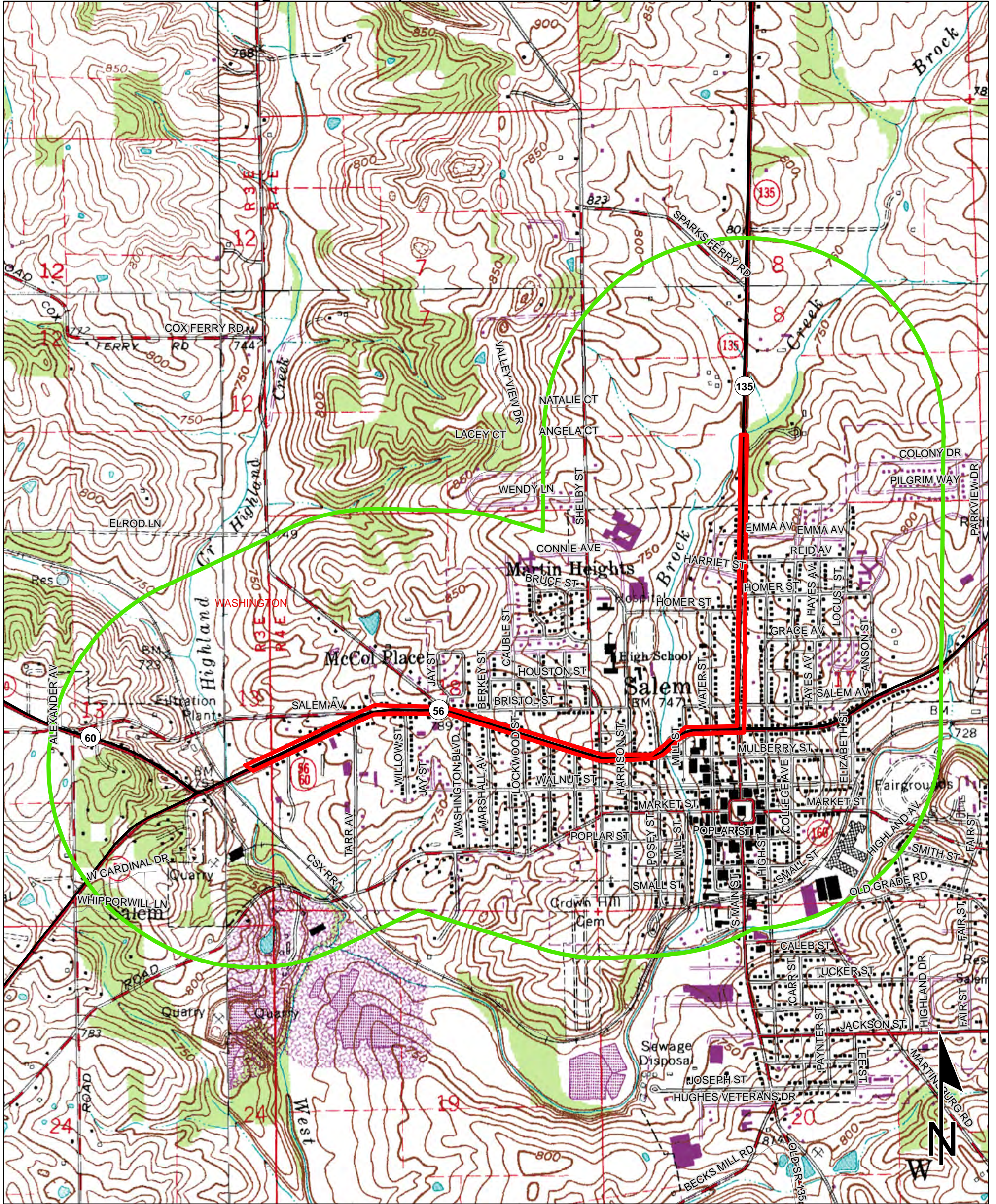
# Red Flag Investigation - Site Location Map

S.R. 56, 0.9 mile east of west Junction of S.R. 60 to S.R. 135 (High Street) Des No. 1600873

S.R. 135, S.R. 56 to 0.72 mile north of S.R. 56 Des No. 1600875

Bridge Replacement, S.R. 56 over Brock Creek, 0.20 mile west of S.R. 135 Des. No. 1700168

Washington Township, Salem, Washington County, Indiana



Sources: 0.3 0.15 0 0.3 Miles  
**Non Orthophotography**  
 Data - Obtained from the State of Indiana Geographical Information Office Library  
**Orthophotography** - Obtained from Indiana Map Framework Data ([www.indianamap.org](http://www.indianamap.org))  
 Map Projection: UTM Zone 16 N Map Datum: NAD83  
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

**SALEM QUADRANGLE  
 INDIANA  
 7.5 MINUTE SERIES  
 (TOPOGRAPHIC)**

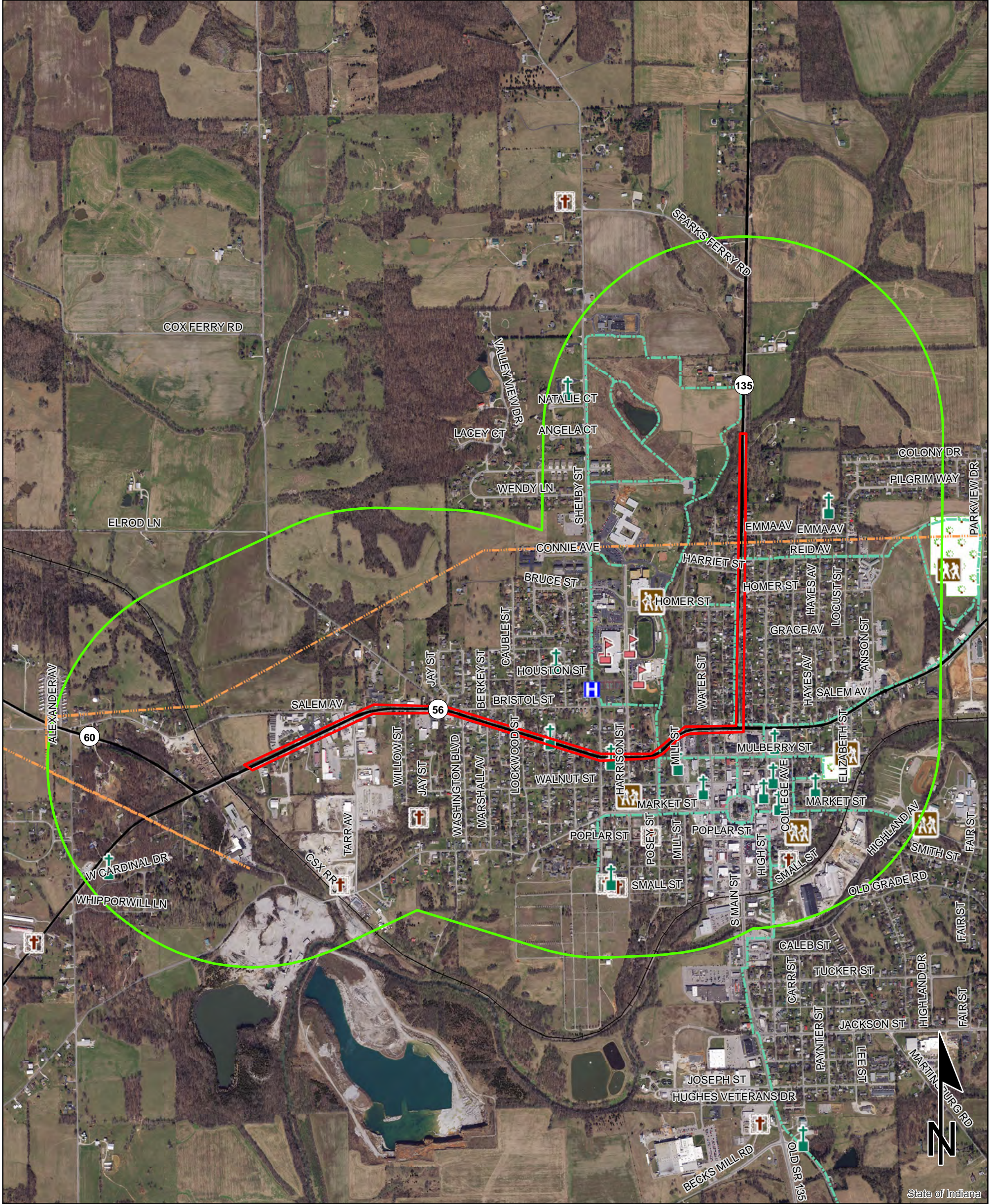
# Red Flag Investigation - Infrastructure

S.R. 56, 0.9 mile east of west Junction of S.R. 60 to S.R. 135 (High Street) Des No. 1600873

S.R. 135, S.R. 56 to 0.72 mile north of S.R. 56 Des No. 1600875

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	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Toll
	Hospital		Trails		Interstate
	School		Managed Lands		State Route
			County Boundary		US Route
					Local Road

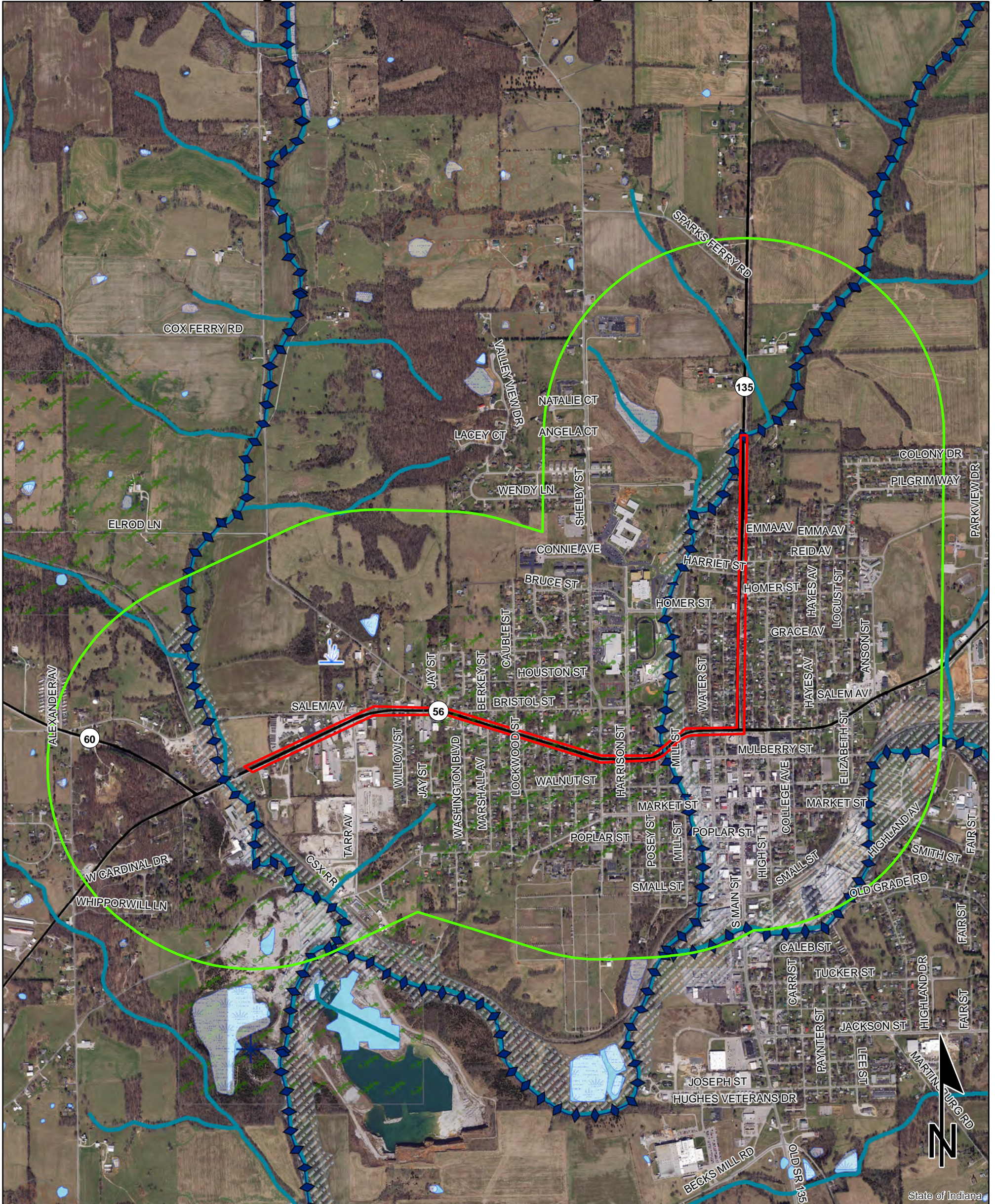
# Red Flag Investigation - Water Resources

S.R. 56, 0.9 mile east of west Junction of S.R. 60 to S.R. 135 (High Street) Des No. 1600873

S.R. 135, S.R. 56 to 0.72 mile north of S.R. 56 Des No. 1600875

Bridge Replacement, S.R. 56 over Brock Creek, 0.20 mile west of S.R. 135 Des. No. 1700168

Washington Township, Salem, Washington County, Indiana



Sources: 0.3 0.15 0 0.3 Miles

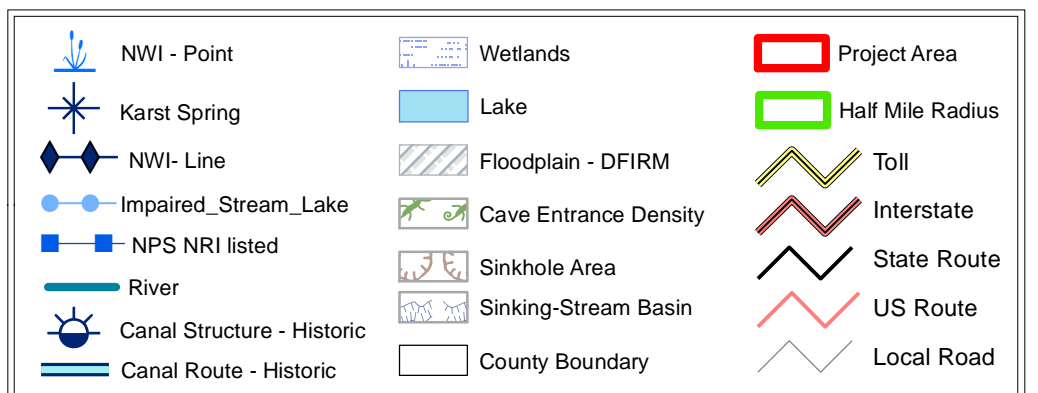
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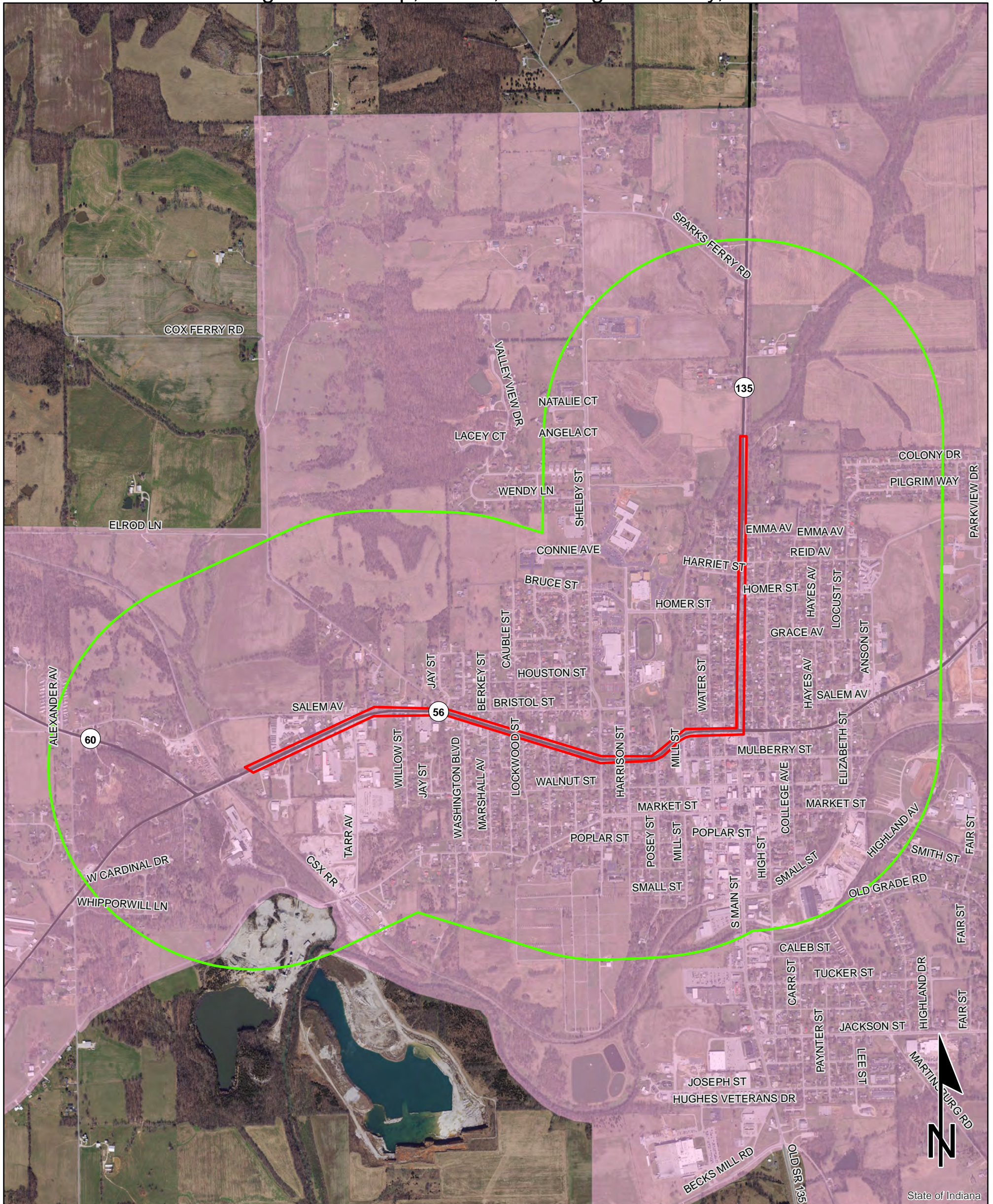
# Red Flag Investigation - Urbanized Area Boundary

S.R. 56, 0.9 mile east of west Junction of S.R. 60 to S.R. 135 (High Street) Des No. 1600873

S.R. 135, S.R. 56 to 0.72 mile north of S.R. 56 Des No. 1600875

Bridge Replacement, S.R. 56 over Brock Creek, 0.20 mile west of S.R. 135 Des. No. 1700168

Washington Township, Salem, Washington County, Indiana



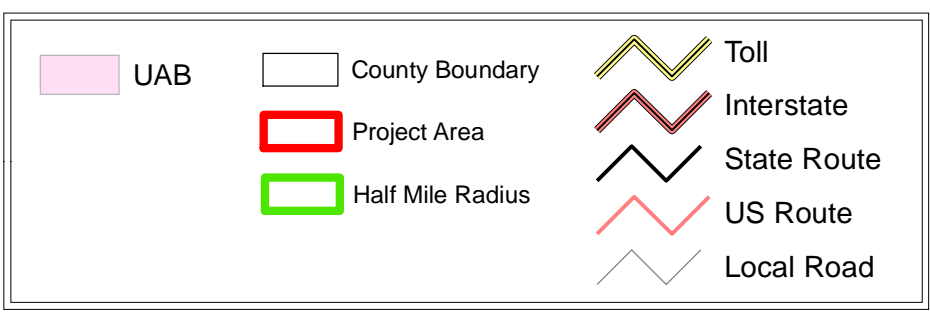
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**Non Orthophotography**

**Data** - Obtained from the State of Indiana Geographical Information Office Library

**Orthophotography** - Obtained from Indiana Map Framework Data ([www.indianamap.org](http://www.indianamap.org))

**Map Projection:** UTM Zone 16 N **Map Datum:** NAD83

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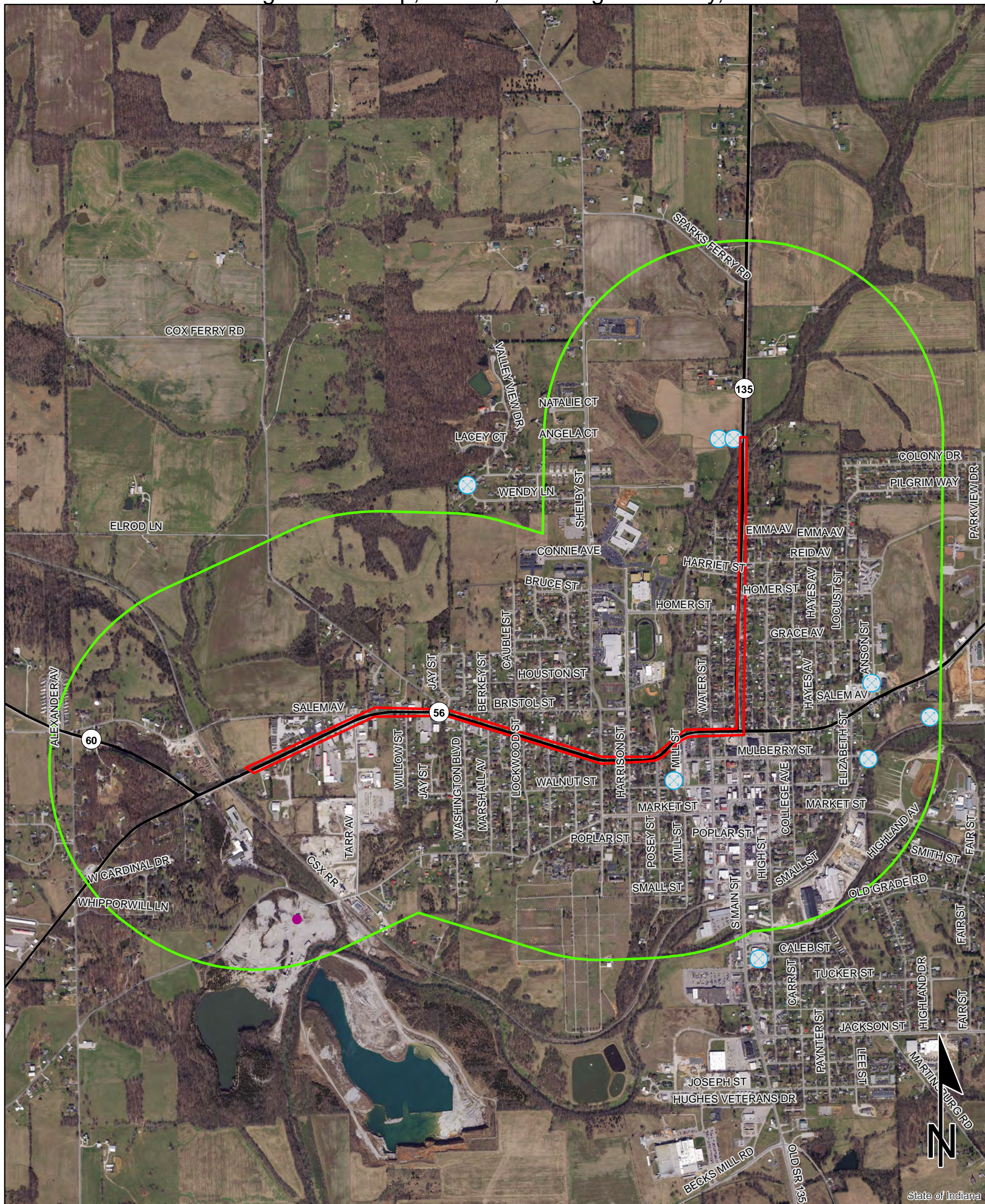
# Red Flag Investigation - Mining and Mineral Resources

S.R. 56, 0.9 mile east of west Junction of S.R. 60 to S.R. 135 (High Street) Des No. 1600873

S.R. 135, S.R. 56 to 0.72 mile north of S.R. 56 Des No. 1600875

Bridge Replacement, S.R. 56 over Brock Creek, 0.20 mile west of S.R. 135 Des. No. 1700168

Washington Township, Salem, Washington County, Indiana



0.3 0.15 0 0.3 Miles

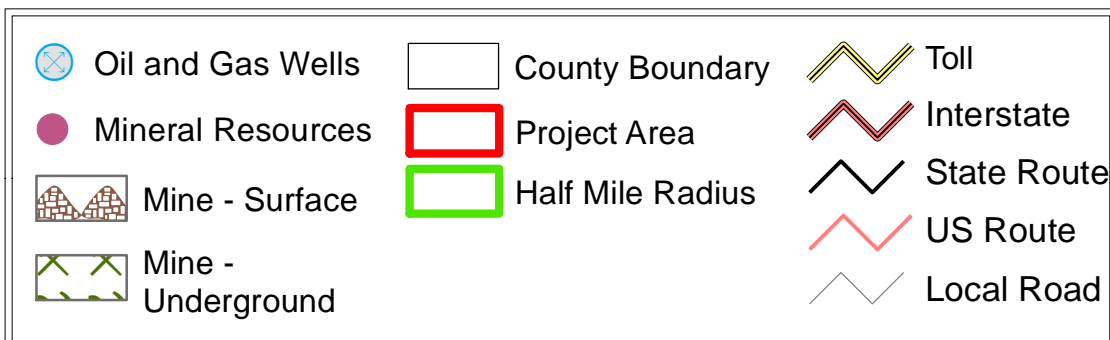
**Sources:**  
**Non Orthophotography**

**Data** - Obtained from the State of Indiana Geographical Information Office Library

**Orthophotography** - Obtained from Indiana Map Framework Data ([www.indianamap.org](http://www.indianamap.org))

**Map Projection:** UTM Zone 16 N **Map Datum:** NAD83

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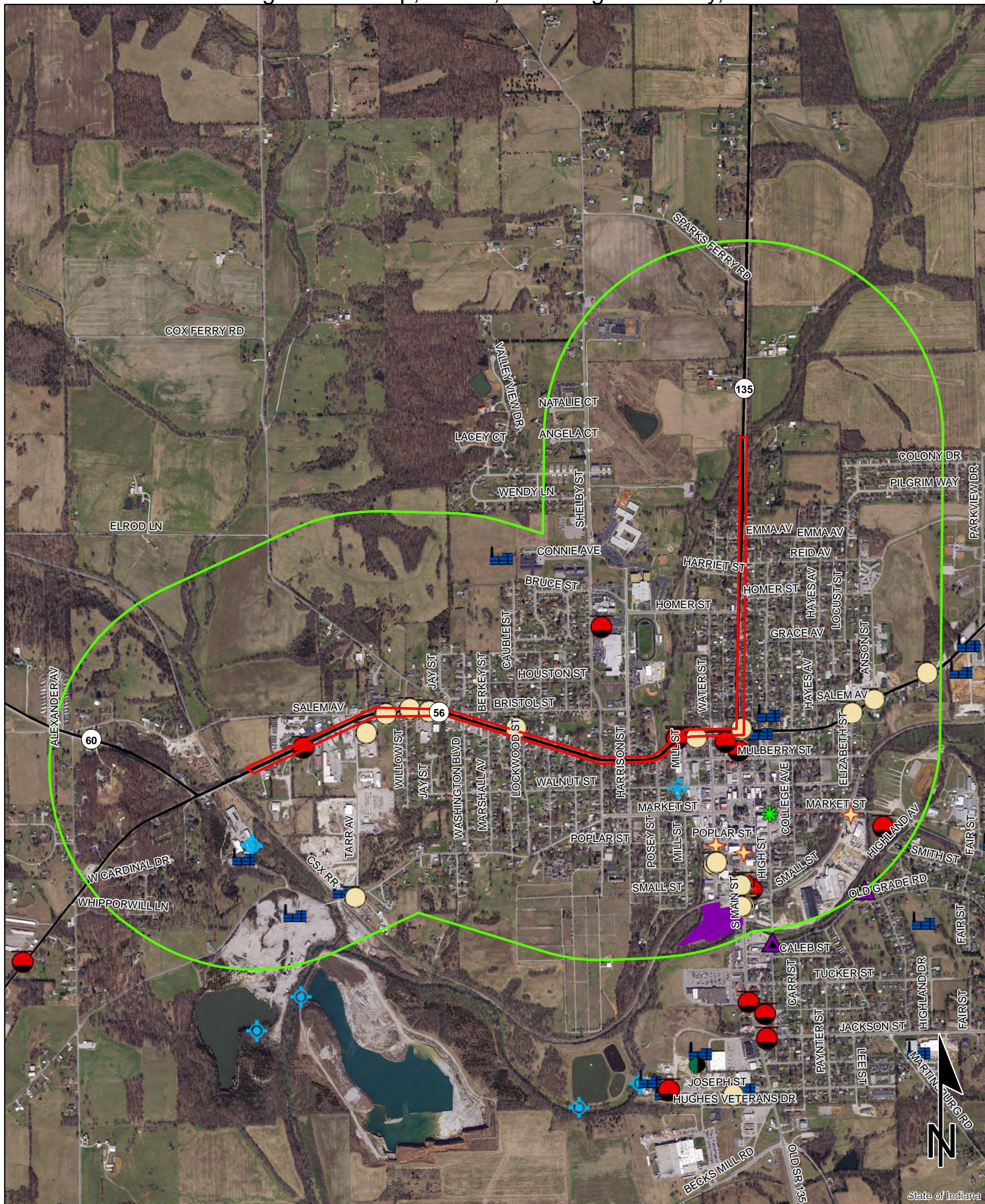
# Red Flag Investigation - Hazardous Material Concerns

S.R. 56, 0.9 mile east of west Junction of S.R. 60 to S.R. 135 (High Street) Des No. 1600873

S.R. 135, S.R. 56 to 0.72 mile north of S.R. 56 Des No. 1600875

Bridge Replacement, S.R. 56 over Brock Creek, 0.20 mile west of S.R. 135 Des. No. 1700168

Washington Township, Salem, Washington County, Indiana



0.25 0.125 0 0.25 Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

**Sources:**

**Non Orthophotography**

**Data** - Obtained from the State of Indiana Geographical Information Office Library

Information Office Library

**Orthophotography** - Obtained from Indiana Map Framework Data

([www.indianamap.org](http://www.indianamap.org))

**Map Projection:** UTM Zone 16 N **Map Datum:** NAD83

Brownfield	RCRA Generator/TSD	Institutional Controls
RCRA Corrective Action Sites	Restricted Waste Site	County Boundary
Confined Feeding Operation	Septage Waste Site	Project Area
Notice_of_Contamination	Solid Waste Landfill	Half Mile Radius
Construction/Demolition Site	State Cleanup Site	Toll
Infectious/Medical Waste Site	Superfund	Interstate
Leaking Underground Storage Tank	Tire Waste Site	State Route
Manufactured Gas Plant	Underground Storage Tank	US Route
NPDES Facilities	Voluntary Remediation Program	Local Road
NPDES Pipe Locations	Waste Transfer Station	
Open Dump Waste Site		



## Indiana County Endangered, Threatened and Rare Species List

### County: Washington

Species Name	Common Name	FED	STATE	GRANK	SRANK
<b>Platyhelminthes (Flatworms)</b>					
Sphalloplana weingartneri	Weingartner's Cave Flatworm		WL	G4	S4
<b>Diplopoda</b>					
Conotyia bollmani	Bollman's Cave Milliped		WL	G5	S4
Pseudotremia indianae	Blue River Cave Milliped		WL	G4	S4
<b>Crustacean: Malacostraca, Amphipods</b>					
Crangonyx packardi	Packard's Cave Amphipod		WL	G4	S3
<b>Crustacean: Malacostraca, Crayfish</b>					
Orconectes inermis inermis	A Troglobitic Crayfish		WL	G5T4	S4
<b>Crustacean: Malacostraca, Isopods</b>					
Miktoniscus barri	Barr's Terrestrial Isopod		WL	G2G4	SNR
<b>Crustacean: Copepoda</b>					
Diacyclops jeanneli	Jeannel's Cave Copepod		ST	G3G4	S2
<b>Crustacean: Ostracoda</b>					
Dactylocythere susanae	An Ostracod		WL	G2G4	S3
Pseudocandona jeanneli	Jeannel's Cave Ostracod		SE	G2	S1
Sagittocythere barri	Barr's Commensal Cave Ostracod		WL	G5	S3S4
<b>Mollusk: Bivalvia (Mussels)</b>					
Cyprogenia stegaria	Eastern Fanshell Pearlymussel	LE	SE	G1Q	S1
Epioblasma torulosa torulosa	Tubercled Blossom	LE	SE	G2TX	SX
Fusconaia subrotunda	Longsolid		SE	G3	SX
Lampsilis fasciola	Wavyrayed Lampmussel		SSC	G5	S3
Ligumia recta	Black Sandshell			G4G5	S2
Obovaria subrotunda	Round Hickorynut		SE	G4	S1
Pleurobema clava	Clubshell	LE	SE	G1G2	S1
Pleurobema pyramidatum	Pyramid Pigtoe		SE	G2G3	SX
Ptychobranhus fasciolaris	Kidneyshell		SSC	G4G5	S2
Simpsonaias ambigua	Salamander Mussel		SSC	G3	S2
Villosa lienosa	Little Spectaclecase		SSC	G5	S3
<b>Mollusk: Gastropoda</b>					
Carychium riparium	Floodplain Thorn			G2	SNR
Zonitoides kirbyi	Shadow Gloss			G2	SNR
<b>Ellipluran: Collembola</b>					
Arrhopalites ater	Black Medusa Cave Springtail		ST	G2	S2
Arrhopalites benitus	A Springtail		WL	G1	S1
Arrhopalites lewisi	Lewis' Cave Springtail		ST	GNR	S2
Folsomia prima	Primitive Springtail		WL	GNR	S4
Folsomides americanus	Small Springtail		SE	GNR	S1
Hypogastrura horrida	Bristly Springtail		WL	GNR	SNR

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Indiana Department of Natural Resources  
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Indiana County Endangered, Threatened and Rare Species List

County: Washington

Species Name	Common Name	FED	STATE	GRANK	SRANK
Isotoma anglicana	A Springtail		WL	GNR	SNR
Isotoma caeruleatra	Blue Springtail		WL	GNR	SNR
Isotoma nigrifrons	Dark Springtail		WL	GNR	SNR
Isotoma nixonii	Nixon's Springtail		WL	GNR	SNR
Isotoma torilidae			WL	GNR	SNR
Onychiurus casus	Fallen Springtail		WL	GNR	S4
Onychiurus reluctus	A Springtail		WL	GNR	S4
Pseudosinella collina	Hilly Springtail		SR	GNR	S2?
Pseudosinella fonsa	Fountain Cave Springtail		ST	G3G4	S2
Sinella alata	Springtail		WL	G5	S4
Sinella cavernarum	A Springtail		WL	G5	S4
<b>Insect: Coleoptera (Beetles)</b>					
Aleochara lucifuga	Rove beetle		WL	GNR	S4
Necrophilus pettiti	A Carrion Beetle		ST	GNR	S1?
Pseudanopthalmus stricticollis	Marengo Cave Ground Beetle			G4	S3
Pseudanopthalmus tenuis	Cave Beetle		WL	G4	S4
Pseudanopthalmus youngi	Young's cave ground beetle		SR	G3G4	S3
<b>Insect: Lepidoptera (Butterfly)</b>					
Cyllopsis gemma	Gemmed Satyr		SR	G4G5	S2
Erynnis martialis	Mottled Duskywing		ST	G3	S2S3
<b>Arachnida</b>					
Bathypantes weyeri	A Cave Spider			G4	SNR
Cicurina arcuata	A Funnel-web Weaver			GNR	S1
Hesperochernes mirabilis	Southeastern Cave Pseudoscorpion		WL	G5	S4
Kleptochthonius packardi	Packard's Cave Pseudoscorpion		SE	G2G3	S1S2
<b>Fish</b>					
Amblyopsis spelaea	Northern Cavefish		SE	G3G4	S1
Etheostoma maculatum	Spotted Darter		SSC	G2G3	S2S3
Etheostoma variatum	Variagate Darter		SE	G5	S1
Notropis ariommus	Popeye Shiner			G3	SX
<b>Amphibian</b>					
Acris blanchardi	Northern Cricket Frog		SSC	G5	S4
Cryptobranchus alleganiensis alleganiensis	Eastern Hellbender		SE	G3G4T3T4	S1
<b>Reptile</b>					
Clonophis kirtlandii	Kirtland's Snake		SE	G2	S2
Nerodia erythrogaster neglecta	Copperbelly Water Snake	PS:LT	SE	G5T3	S2
Opheodrys aestivus	Rough Green Snake		SSC	G5	S3
Terrapene carolina carolina	Eastern Box Turtle		SSC	G5T5	S3

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Indiana County Endangered, Threatened and Rare Species List

County: Washington

Species Name	Common Name	FED	STATE	GRANK	SRANK
<b>Bird</b>					
<i>Ammodramus henslowii</i>	Henslow's Sparrow		SE	G4	S3B
<i>Buteo lineatus</i>	Red-shouldered Hawk		SSC	G5	S3
<i>Cistothorus platensis</i>	Sedge Wren		SE	G5	S3B
<i>Dendroica cerulea</i>	Cerulean Warbler		SE	G4	S3B
<i>Haliaeetus leucocephalus</i>	Bald Eagle		SSC	G5	S2
<i>Helmitheros vermivorus</i>	Worm-eating Warbler		SSC	G5	S3B
<i>Tyto alba</i>	Barn Owl		SE	G5	S2
<i>Wilsonia citrina</i>	Hooded Warbler		SSC	G5	S3B
<b>Mammal</b>					
<i>Myotis austroriparius</i>	Southeastern Bat		SSC	G3G4	SH
<i>Myotis lucifugus</i>	Little Brown Bat		SSC	G3	S2
<i>Myotis septentrionalis</i>	Northern Myotis		SSC	G1G3	S2S3
<i>Myotis sodalis</i>	Indiana Bat or Social Myotis	LE	SE	G2	S1
<i>Perimyotis subflavus</i>	Eastern Pipistrelle		SSC	G3	S2S3
<i>Plecotus rafinesquii</i>	Rafinesque's Big-eared Bat		SSC	G3G4	SH
<i>Taxidea taxus</i>	American Badger		SSC	G5	S2
<b>Vascular Plant</b>					
<i>Aconitum uncinatum</i>	Blue Monkshood		SE	G4	S1
<i>Bacopa rotundifolia</i>	Roundleaf Water-hyssop		ST	G5	S1
<i>Calamagrostis porteri</i> ssp. <i>insperata</i>	Reed Bent Grass		ST	G4T3	S1
<i>Carex straminea</i>	Straw Sedge		ST	G5	S2
<i>Crataegus prona</i>	Illinois Hawthorn		SE	G4G5	S1
<i>Cuscuta cuspidata</i>	Cusp Dodder		SE	G5	S1
<i>Diervilla lonicera</i>	Northern Bush-honeysuckle		SR	G5	S2
<i>Eleocharis bifida</i>	Glades spikerush		SE	G3G4	S1
<i>Euphorbia obtusata</i>	Bluntleaf Spurge		SE	G5	S1
<i>Gonolobus obliquus</i>	Angle Pod		SR	G4?	S2
<i>Hexalectris spicata</i>	Crested Coralroot		SR	G5	S2
<i>Juncus secundus</i>	Secund Rush		SE	G5?	S1
<i>Lathyrus venosus</i>	Smooth Veiny Pea		ST	G5	S2
<i>Linum sulcatum</i>	Grooved Yellow Flax		SR	G5	S2
<i>Magnolia acuminata</i>	Cucumber Magnolia		SE	G5	S1
<i>Oenothera triloba</i>	Stemless Evening-primrose		SX	G4	SX
<i>Ophioglossum engelmannii</i>	Limestone Adder's-tongue		SR	G5	S2
<i>Penstemon deamii</i>	Deam Beardtongue		SR	G1	S1
<i>Polygala incarnata</i>	Pink Milkwort		SE	G5	S1
<i>Polypodium polypodioides</i>	Resurrection Fern		SR	G5	S2
<i>Scirpus purshianus</i>	Weakstalk Bulrush		SR	G4G5	S1
<i>Silene regia</i>	Royal Catchfly		ST	G3	S2

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## Indiana County Endangered, Threatened and Rare Species List

### County: Washington

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<b>Thalictrum pubescens</b>	<b>Tall Meadowrue</b>		<b>ST</b>	<b>G5</b>	<b>S2</b>
Tragia cordata	Heart-leaved Noseburn		WL	G4	S2
<b>Waldsteinia fragarioides</b>	<b>Barren Strawberry</b>		<b>SR</b>	<b>G5</b>	<b>S2</b>
<b>Woodwardia areolata</b>	<b>Netted Chainfern</b>		<b>SR</b>	<b>G5</b>	<b>S2</b>
<b>High Quality Natural Community</b>					
Barrens - bedrock limestone	Limestone Glade		SG	G4	S2S3
Barrens - bedrock siltstone	Siltstone Glade		SG	G2	S2
Barrens - chert	Chert Barrens		SG	G2	S1
Forest - upland dry-mesic	Dry-mesic Upland Forest		SG	G4	S4
Forest - upland mesic	Mesic Upland Forest		SG	G3?	S3
Primary - cave aquatic	Aquatic Cave		SG	GNR	SNR
Primary - cave terrestrial	Terrestrial Cave		SG	GNR	SNR
Primary - cliff limestone	Limestone Cliff		SG	GU	S1

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**APPENDIX F**  
**Water Resources**

# WATERS DETERMINATION REPORT

S.R. 56 AND S.R. 135  
SMALL TOWN RECONSTRUCTION PROJECT INCLUDING  
BRIDGE REPLACEMENT OVER BROCK CREEK  
SALEM, WASHINGTON TOWNSHIP, WASHINGTON  
COUNTY, INDIANA  
DES. NO. 1600873, 1600875, 1700168

**Prepared for:**  
The City of Salem

May 3, 2018



**Prepared by:**

**Metric Environmental, LLC**

**Complex Environment. Creative Solutions.**

6971 Hillside Court  
Indianapolis, IN 46256  
Telephone: 317.207.4286  
[www.metricenv.com](http://www.metricenv.com)

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S.R. 56 and S.R. 135

Small Town Reconstruction Project Including Bridge Replacement over Brock Creek

Salem, Washington Township, Washington County, Indiana

Des. No. 1600873, 1600875, and 1700168

Metric Project No. 17-0059-2



**WATERS OF THE U.S. DETERMINATION REPORT**  
**S.R. 56 and S.R. 135**  
**Small Town Reconstruction Project**  
**Including Bridge Replacement over Brock Creek**  
**Salem, Washington Township, Washington County, Indiana**  
**Des. No. 1600873, 1600875, 1700168**  
**Prepared By: Kathleen Sexton**  
**May 3, 2018**

**Date of Waters Field Investigation:** October 5, 2017

**Location:**

Sections 8, 13, 17, and 18; Township 2 North; Range 3 and 4 East  
Salem, IN United States Geological Survey (USGS) Topographic Quadrangle (**Exhibit 2A and 2B**)  
Salem, Washington Township, Washington County, Indiana

**National Wetlands Inventory (NWI) Information:**

One mapped NWI wetland, a Riverine, Lower Perennial, Unconsolidated Bottom, Permanently Flooded (R2UBH) wetland intersects the project study limits at Bridge No. 056-88-01478. During the desktop survey this R2UBH was identified as Brock Creek, which was confirmed during the field reconnaissance. An additional R2UBH wetland, identified as Highland Creek, is located approximately 157 ft southwest of the project study limits. The NWI map for this area is provided as **Exhibit 3**.

**National Hydrography Dataset (NHD) Information:**

There were eight mapped NHD flowlines within the project study limits. Only one of these, Brock Creek, was observed as a non-piped stream during the field reconnaissance. Two others were observed as piped streams throughout the entirety of the project study limits, and were identified as Unnamed Tributary (UNT) to Highland Creek (UNT 1) and UNT to Brock Creek (UNT 2). There was no evidence of the remaining five mapped NHD flowlines observed during the field reconnaissance, so it is likely that these are piped/tiled below the surface where they appear to intersect the project study limits. Locations of these streams and flowlines are provided on **Exhibits 3 and 4**.

**Karst Feature Information:**

Three cave entrance density areas are located within the 0.5 mi of the project study limits. One cave entrance density location intersects the project area. This data layer from *IndianaMap* shows the density (number of entrances per square kilometer) of mapped cave entrances in southern Indiana. Locations of individual cave entrances are not shown with this data. The cave entrance density within the project study limits was 1 per square kilometer. No karst features were observed during the field reconnaissance.



**FEMA Insurance Rate Map (FIRM):**

The mapped floodplain of Brock Creek, identified as Zone AE, an area subject to inundation by the 1% annual chance of flood, is included in the project study limits surrounding INDOT Bridge No. 056-88-01478. The mapped floodplain of Highland Creek is directly southwest of the project study limits western border but is not included in the limits. The FIRM map for this area is provided as **Exhibit 3**.

**Soils:**

According to the Natural Resources Conservation Service (NRCS) Soil Survey Geographic (SSURGO) Database for Washington County, Indiana, the project study limits contain three mapped soil units, none of which are listed as containing any hydric soil components. These soils are shown in the table below. The NRCS soil survey map is attached as **Exhibit 3**.

Symbol	Map Unit Name	Hydric Rating
CoB	Crider silt loam, 2 to 6 percent slopes	Not Hydric
CoC2	Crider silt loam, 6 to 12 percent slopes, eroded	Not Hydric
Hm	Haymond silt loam, frequently flooded	Not Hydric

**Attached Documents:**

- Maps of the project area (**Exhibits 1-3**)
- Photo Location Map (**Exhibit 4**)
- Site Photographs
- Wetland Determination Data Form(s)
- Preliminary Jurisdictional Determination Form

**Project Description:**

The proposed project is located in the central portion of Washington County, Indiana and will include the removal and replacement of pavement, sidewalks and curbs and establishment of new storm water drainage patterns along S.R. 56, from 0.12 mi east of West Junction of S.R. 60 to S.R. 135 (High Street) and along S.R. 135 from S.R. 56 to 0.72 mi north of S.R. 56, for a total of approximately 2.08 mi. Only resurfacing will be conducted on S.R. 56 from Water Street to High Street. It is anticipated that less than 10 acres of right-of-way will be required (Des. No. 1600873 and 1600875). The S.R. 56 bridge over Brock Creek (Bridge No. 056-88-01478) will be replaced. The bridge replacement will consist of the complete removal and replacement of the three span arch structure. The proposed new footprint will be similar in size to the existing structure (Des. No. 1700168).

**Field Reconnaissance:**

The wetland determination field visit was conducted on October 5, 2017 by Kathleen Sexton and Josh Myers of Metric Environmental, LLC. The project study limits consist of the area that has the potential to be impacted, based on the provided design scenario. This area was evaluated for

S.R. 56 and S.R. 135  
Small Town Reconstruction Project Including Bridge Replacement over Brock Creek  
Salem, Washington Township, Washington County, Indiana  
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Metric Project No. 17-0059-2



the presence of wetlands and Waters of the United States (U.S.). This investigation was conducted in accordance with the *1987 U.S. Army Corps of Engineers (USACE) Wetland Delineation Manual* and the *April 2012 Eastern Mountains and Piedmont Regional Supplement (version 2.0) Manual*.

A Location Map showing the project location is provided as **Exhibit 1** and USGS Salem, Indiana Quadrangle Topographic Maps are provided as **Exhibit 2A and 2B**. The project study limits extends approximately 1.36 mi east and west along S.R. 56 from 0.12 mi east of West Junction of S.R. 60, and approximately 0.72 mi north and south along S.R. 135. An aerial map of sampling points and water features is provided as **Exhibit 3**. A photo location map is provided as **Exhibit 4** and site photographs are attached.

The site was investigated for evidence of hydrophytic vegetation, hydric soil, and wetland hydrology to determine if the project impacts wetlands and other Waters of U.S. The sampling point (SP) locations were chosen in possible wetland areas within the project study limits. Any areas where sampling points weren't taken were investigated and determined to be upland areas, absent of wetlands. Field observations found these upland areas to have topography sloping upward to flat dry land, dominance of upland vegetation, mapped soil with no hydric components, and a lack of hydrology indicators. The uplands consisted of road right-of-way (ROW), urban development, and residential lawn. Two sampling points were taken and are identified as SP-1 and SP-2. The sampling points, recorded on the USACE Wetland Determination Data Form and shown on **Exhibit 3**, provided the following information:

**Sampling Plot Data Summary Table  
S.R. 56 and S.R. 135  
Small Town Reconstruction Project  
Including Bridge Replacement over Brock Creek  
Salem, Washington Township, Washington County, Indiana  
Des. No. 1600873, 1600875, and 1700168**

Plot #	Photo #s	Lat/Long	Hydrophytic Vegetation	Hydric Soils	Wetland Hydrology	Within Wetland
SP-1	96-98	38.608096 -86.104303	No	No	No	No
SP-2	103-105	38.607905 -86.10421	Yes	No	Yes	No

**Wetlands:**

There were no wetlands observed within the project study limits during the field reconnaissance.



### **Upland Sampling Points:**

Additional sampling points were taken in an area where wetlands were suspected, but these areas did not meet the criteria to qualify as wetland. Descriptions of these sampling points are provided below.

#### Sampling Point 1 (SP-1)

SP-1 was located north of S.R. 56 and east of Brock Creek, within the mapped floodplain of Brock Creek. The dominant vegetation present at this sampling point included Johnson grass (*Sorghum halapense*, FACU), groundivy (*Glechoma hederacea*, FACU), and reed canary grass (*Phalaris arundinacea*, FACW) in the herb stratum. This did not meet any of the hydrophytic vegetation indicators. To a depth of 20 in., the soil in the test pit was a silt loam and exhibited a matrix color of 10YR 4/3 (100%). This did not meet any hydric soil indicators. One secondary indicator of hydrology, geomorphic position (D2), was observed due to the sampling point being located within a mapped floodplain. This did not meet the criteria to pass for wetland hydrology. Since none the three wetland criteria were met, this area did not qualify as a wetland.

#### Sampling Point 2 (SP-2)

SP-2 was located south of S.R. 56 and east of Brock Creek, within the mapped floodplain of Brock Creek. The dominant vegetation present at this sampling point included reed canary grass (*Phalaris arundinacea*, FACW) in the herb stratum. This met the hydrophytic vegetation indicators of dominance test (100%) and prevalence index (2.46). To a depth of 20 in., the soil in the test pit was a silt loam and exhibited a matrix color of 10YR 4/3 (100%). This did not meet any hydric soil indicators. Two secondary indicators of hydrology, geomorphic position (D2) and FAC-Neutral Test (D5), were observed. The sampling point met for geomorphic position since it was located within a mapped floodplain. This met the criteria to pass for wetland hydrology. Since only two of the three wetland criteria were met, this area did not qualify as a wetland.

**Streams:**

Three streams were observed within the project study limits during the field reconnaissance.

**S.R. 56 and S.R. 135  
Small Town Reconstruction Project  
Including Bridge Replacement over Brock Creek  
Salem, Washington Township, Washington County, Indiana  
Des. No. 1600873, 1600875, and 1700168**

Stream Name	Photo #s	Lat/Long	OHWB Width (ft.)	OHWB Depth (ft.)	USGS Blue-line	Substrate	Riffles and Pools	Quality	Likely Water of the U.S.	Potential Stream Impact in Linear Feet (LFT)
<b>Brock Creek</b>	41, 42-47, 90, 91, 94-95, 100-101	38.607977 -86.104328	14.0	0.8	Yes (Perennial)	Bedrock, Silt	No	Poor	Yes	158 (0.051 ac)
<b>UNT 1</b>	5, 6, 7	38.607688 -86.123066	1.0	0.3	No (Ephemeral)	Artificial, Silt	N/A	Poor	Yes	79 (0.002 ac)
<b>UNT 2</b>	78-82	38.617549 -86.100471	1.5	0.3	No (Ephemeral)	Artificial, Silt	N/A	Poor	Yes	68 (0.002 ac)

**Brock Creek (125 LFT)**

Brock Creek flows from northwest to southeast and is approximately 158 linear feet (LFT) in length (0.051 ac) within the project study limits. Brock Creek flows southwest into West Fork Blue River, which flows south into Blue River, a Section 10 Traditional Navigable Water (TNW). Therefore, Brock Creek should be considered a jurisdictional Water of the U.S. This stream is associated with a solid blue line on the USGS topographic map, indicating it is perennial. This stream is classified by the NWI as a Riverine, Lower Perennial, Unconsolidated Bottom, Permanently Flooded (R2UBH) stream. Field observations classified this stream as a Riverine, Lower Perennial, Unconsolidated Bottom, Mud, Permanently Flooded (R2UB3H) stream. The stream exhibited OHWM characteristics with an established flow line on the bank. The OHWM was an average of 14.0 ft in width and 0.8 ft in depth within the project study limits. The stream substrate consisted of bedrock and silt. Undercut banks and overhanging vegetation were the in-stream cover present. Sinuosity was absent and the water velocity was slow, with a moderate gradient. No riffle/pool complexes were observed, and no aquatic organisms were found in the stream. According to the U.S.G.S. *Indiana StreamStats*, the drainage area upstream of the project study limits is 7.87 square miles (sq. mi).



### **UNT to Highland Creek (79 LFT) – UNT 1**

UNT 1 flows from southeast to northwest and is approximately 79 LFT in length (0.002 ac) within the project study limits. This stream flows northwest outside of the project study limits and into Highland Creek, which flows southwest into West Fork Blue River, which flows south into Blue River, a Section 10 TNW. Therefore, UNT 1 should be considered a jurisdictional Water of the U.S. This stream is carried under S.R. 56 via a culvert and piped throughout the entirety of the project study limits, and will likely not be impacted by the road reconstruction project. UNT 1 is not associated with a solid blue line on the USGS topographic map, indicating it is ephemeral. UNT 1 is not classified by the NWI, but it can be classified by the USACE designation as a Riverine, Ephemeral (R6) stream. The OHWM was an average of 1.0 ft in width and 0.3 ft in depth within the project study limits. The stream substrate consisted of artificial and silt. Overhanging vegetation was the in-stream cover present. Sinuosity was absent, with a moderate gradient, and the stream was not flowing at the time of the site visit. There was one scour hole observed near the culvert outlet south of S.R. 56, but no true pools or riffles were observed, and no aquatic organisms were found in the stream. There is no blue line associated with UNT 1 on USGS *Indiana StreamStats* so the drainage area could not be determined.

### **UNT to Brock Creek (68 LFT) – UNT 2**

UNT 2 flows from east to west and is approximately 68 LFT in length (0.002 ac) within the project study limits. This stream flows west outside of the project study limits and into Brock Creek, which flows southwest into West Fork Blue River, which flows south into Blue River, a Section 10 TNW. Therefore, UNT 2 should be considered a jurisdictional Water of the U.S. This stream is piped throughout the entirety of the project study limits and will likely not be impacted by the road reconstruction project. NHD data shows this UNT extending to the east of S.R. 56, but field observations found a vegetated channel that did not exhibit bed and bank characteristics (photo No. 78). Therefore, UNT 2 begins in the culvert and extends west of S.R. 56 where it becomes channelized, and an OHWM was observed. UNT 2 is not associated with a solid blue line on the USGS topographic map, indicating it is ephemeral. UNT 2 is not classified by the NWI, but it can be classified by the USACE designation as a Riverine, Ephemeral (R6) stream. The OHWM was an average of 1.5 ft in width and 0.3 ft in depth within the project study limits. The stream substrate consisted of artificial and silt. Overhanging vegetation was the in-stream cover present. Sinuosity was absent, with a high gradient, and the stream was not flowing at the time of the site visit. No riffle/pool complexes or aquatic organisms were observed in the stream. According to the U.S.G.S. *Indiana StreamStats*, the drainage area upstream of the project study limits is 0.048 sq. mi.

### **Roadside Ditches:**

Five roadside ditches (RSD) were identified within the project study limits during the field reconnaissance.

#### Roadside Ditch 1 (RSD 1)

RSD 1 is located on the north side of S.R. 56 and is approximately 1088 ft in length within the project study limits. This feature is a concrete lined drainage ditch that transitions to a vegetated drainage swale approximately 95 ft east of UNT 1. RSD 1 is dominated by upland vegetation and surrounded by mowed field and ROW. This feature conveys surface water runoff from S.R. 56 and the adjacent mowed field west into UNT 1. It is carried underneath several private driveways via culverts. No OHWM was observed so this feature is likely non-jurisdictional.

#### Roadside Ditch 2 (RSD 2)

RSD 2 is located on the north side of S.R. 56 and is approximately 863 ft in length within the project study limits. This feature is a vegetated drainage swale, consisting primarily of upland vegetation situated within a mowed ROW and adjacent to residential and urban land. RSD 2 is carried underneath several private driveways via culverts, and appears to convey surface water runoff west. No OHWM was observed so this feature is likely non-jurisdictional.

#### Roadside Ditch 3 (RSD 3)

RSD 3 is located on the south side of S.R. 56 and is approximately 46 ft in length within the project study limits. This feature is a vegetated drainage swale, consisting primarily of upland vegetation situated within a mowed ROW and adjacent to residential and urban land. RSD 3 appears to convey surface water runoff from S.R. 56 and residential lawn into storm sewers. No OHWM was observed so this feature is likely non-jurisdictional.

#### Roadside Ditch 4 (RSD 4)

RSD 4 is located on the north side of S.R. 56 and is approximately 54 ft in length within the project study limits. This feature is a vegetated drainage swale, consisting primarily of upland vegetation situated within a mowed ROW and adjacent to urban land, and appears to convey surface water runoff from S.R. 56 into Brock Creek. No OHWM was observed so this feature is likely non-jurisdictional.

#### Roadside Ditch 5 (RSD 5)

RSD 5 is located on the east side of S.R. 135 and is approximately 102 ft in length within the project study limits. This feature is a partially vegetated drainage ditch consisting primarily of upland vegetation that transitions to a concrete lined ditch as it flows north outside of the project study limits. RSD 5 is situated within mowed ROW and adjacent to mowed residential lawn, and appears to convey surface water runoff from S.R. 135 and adjacent mowed lawn north via a culvert beneath a private drive. No OHWM was observed so this feature is likely non-jurisdictional.

S.R. 56 and S.R. 135

Small Town Reconstruction Project Including Bridge Replacement over Brock Creek

Salem, Washington Township, Washington County, Indiana

Des. No. 1600873, 1600875, and 1700168

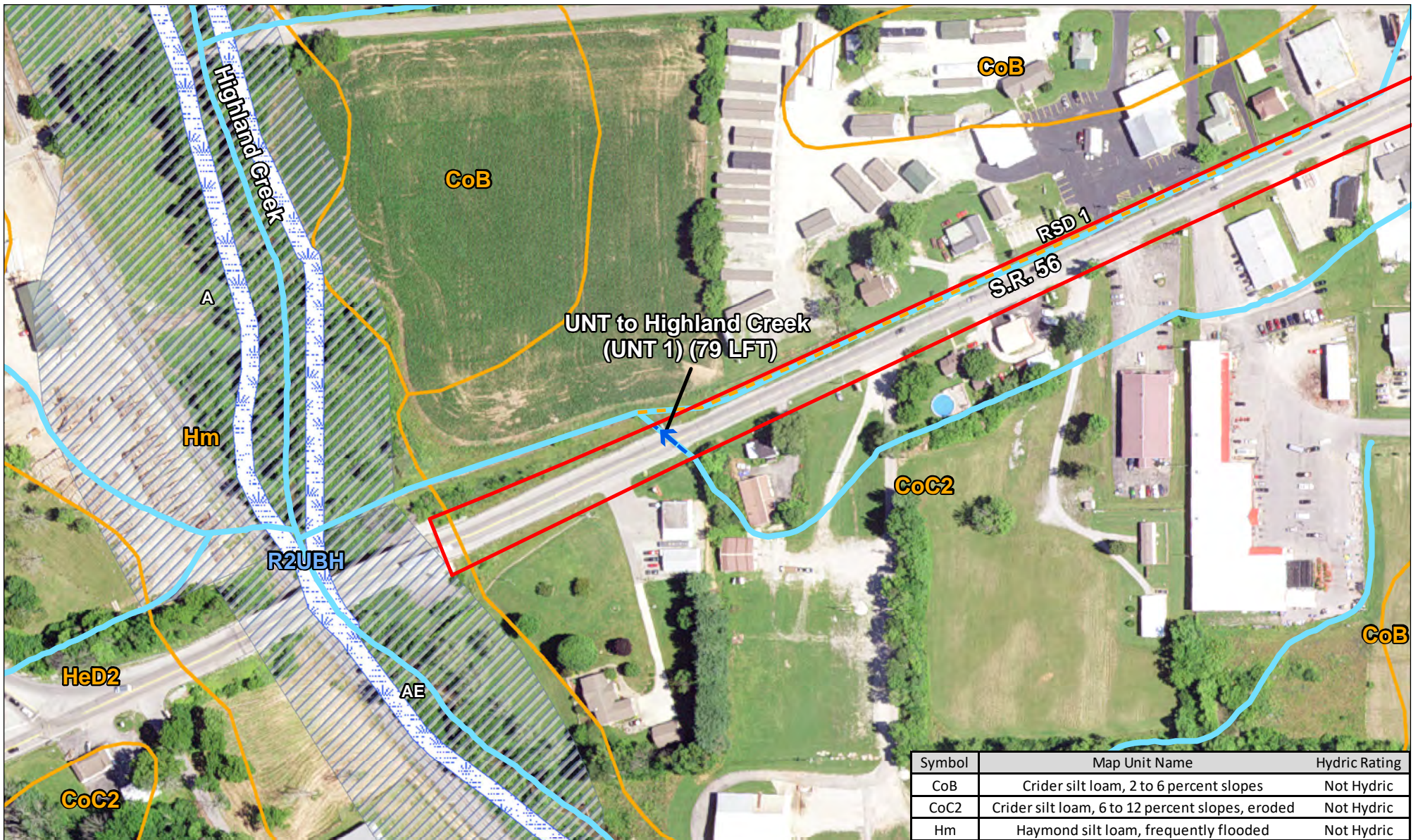
Metric Project No. 17-0059-2



**Conclusion:**

Three streams totaling 305 LFT and 0.055 ac were identified within the project study limits. Every effort should be taken to avoid or minimize impacts to these waterways. If impacts are necessary, mitigation may be required. The final determination of jurisdictional waters is ultimately made by the USACE. This report is our best judgment based on the guidelines set forth by USACE.

Duplicate attachments were intentionally removed. Please refer to Appendix B in the CE document.

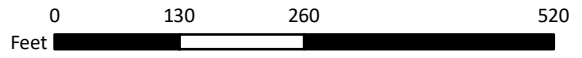


Symbol	Map Unit Name	Hydric Rating
CoB	Crider silt loam, 2 to 6 percent slopes	Not Hydric
CoC2	Crider silt loam, 6 to 12 percent slopes, eroded	Not Hydric
Hm	Haymond silt loam, frequently flooded	Not Hydric

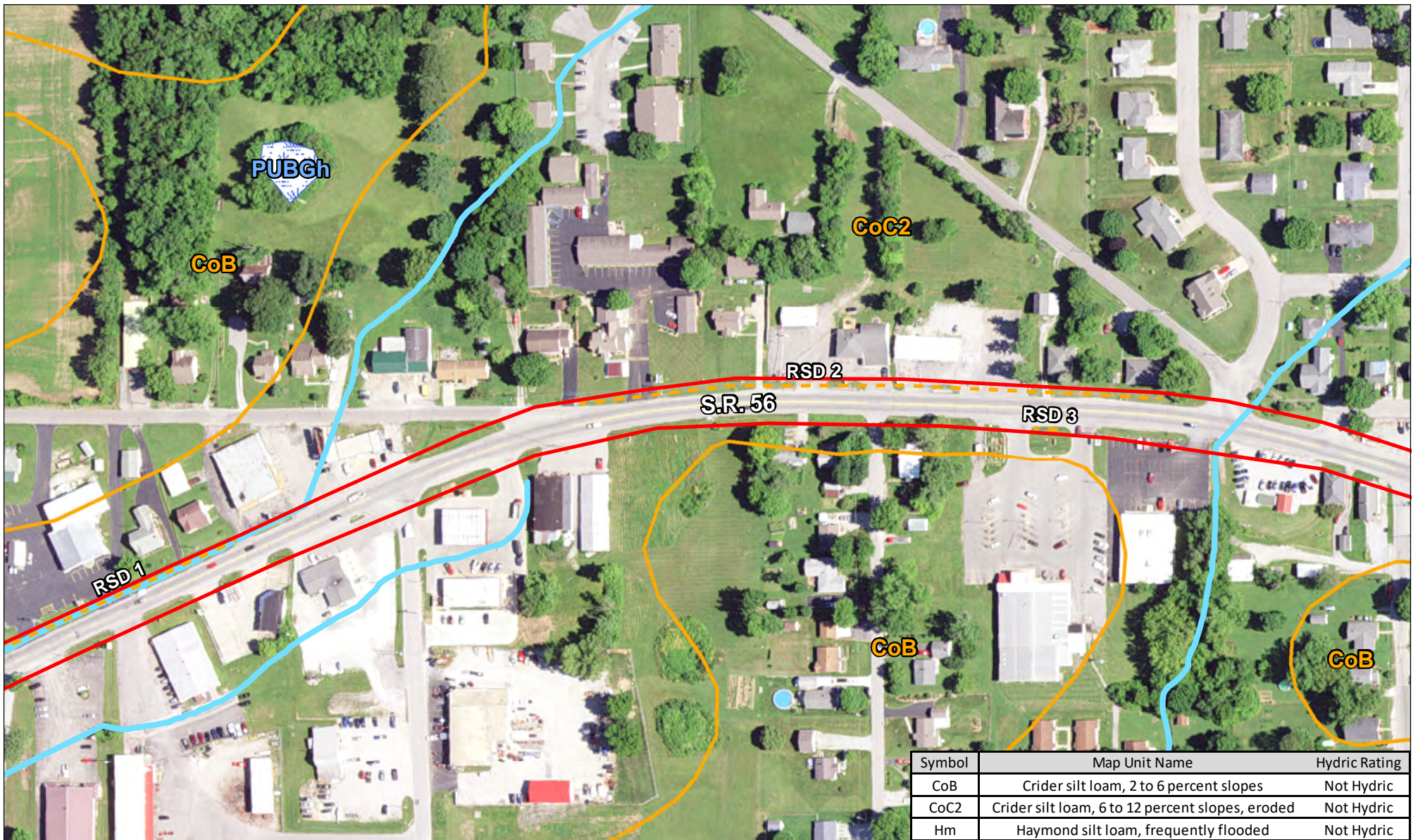
- Project Study Limits
- Sampling Point
- Stream
- - - Piped Stream
- NHD Stream
- NWI Wetlands
- Floodplain
- NRCS Soil Survey
- Cave Entrance Density Area

Exhibit 3 - NWI, NHD, FIRM, NRCS Soils, Karst, and Waters Delineation Map  
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 Salem, Washington County, Indiana  
 Des. No. 1600873, 1600875, and 1700168  
 Metric Project No. 17-0059-2

All locations approximate  
 Source: Indiana Spatial Data Portal (2016 Aerial)





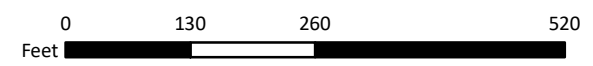


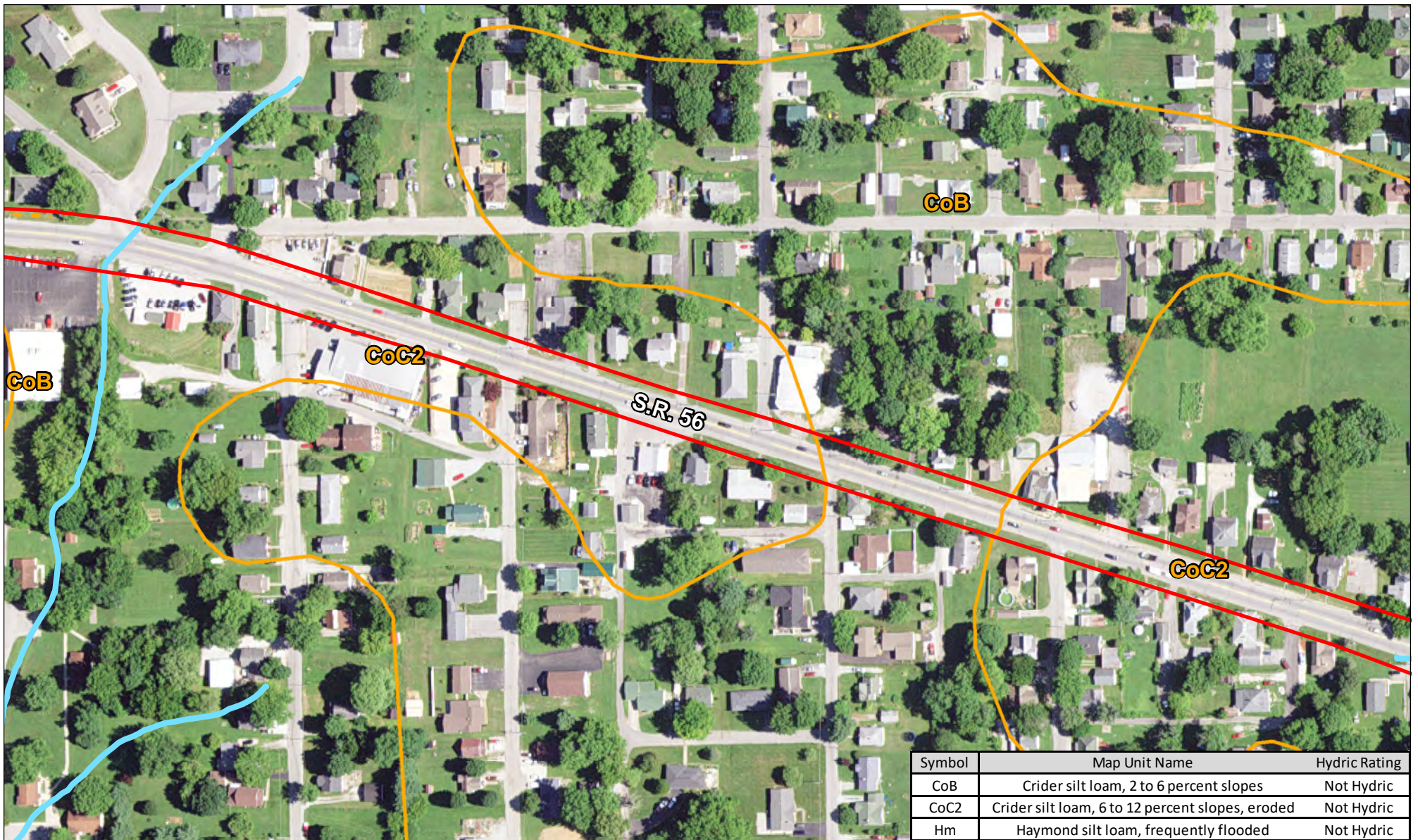
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All locations approximate  
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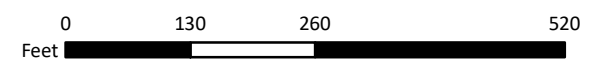


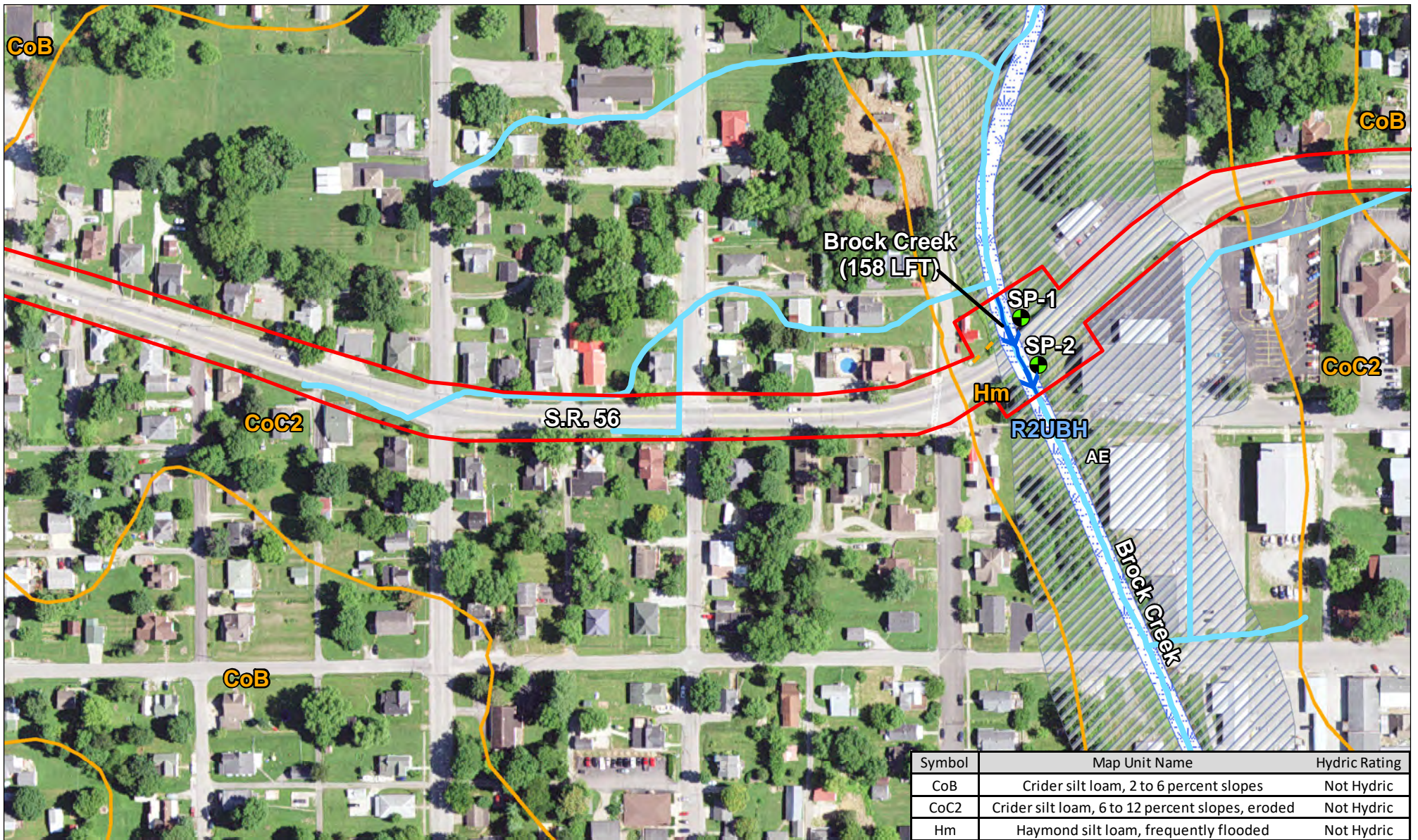
Symbol	Map Unit Name	Hydric Rating
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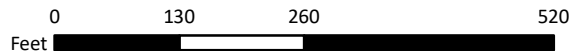


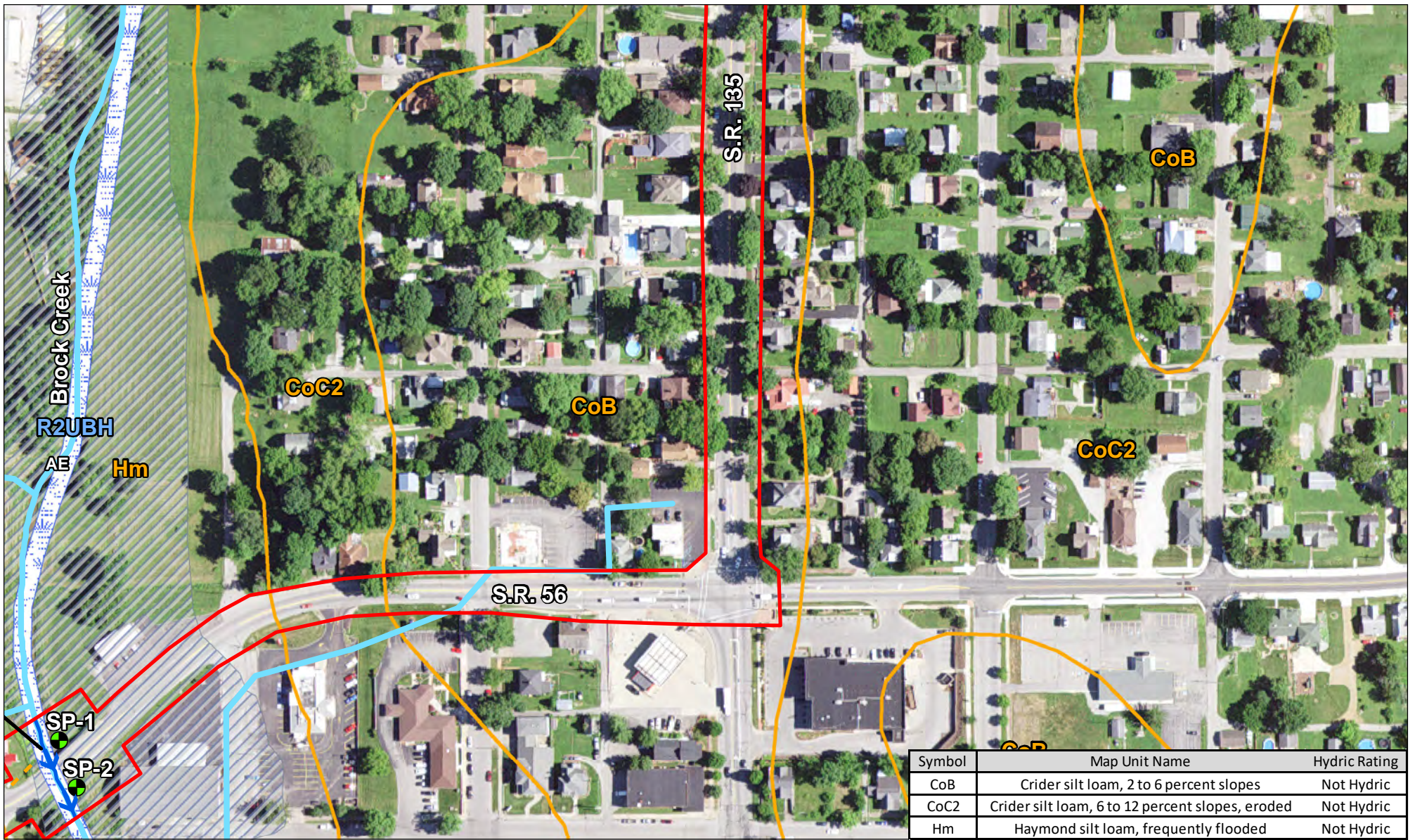


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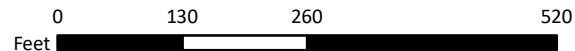


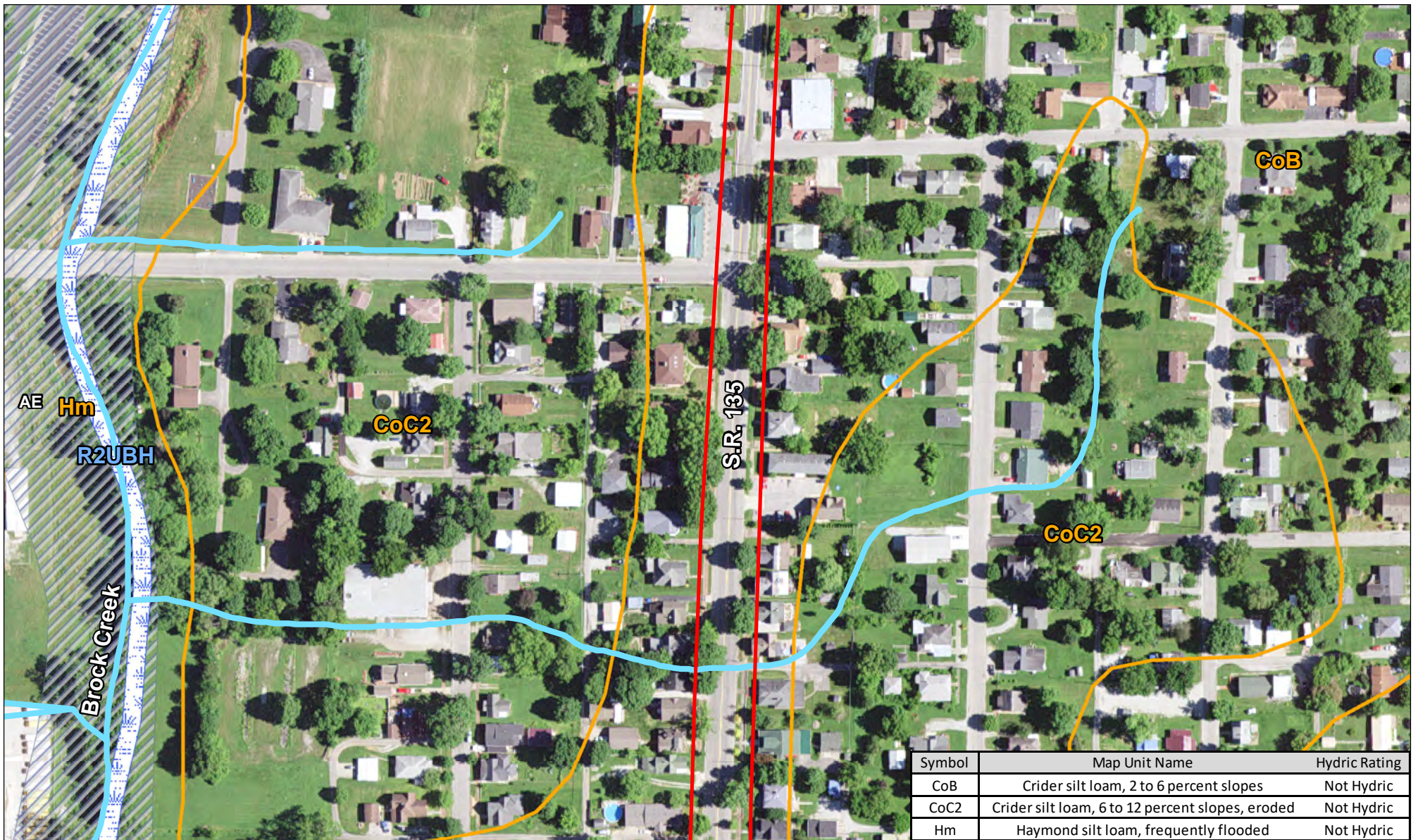
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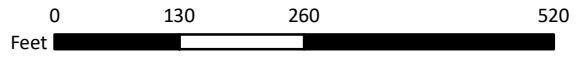


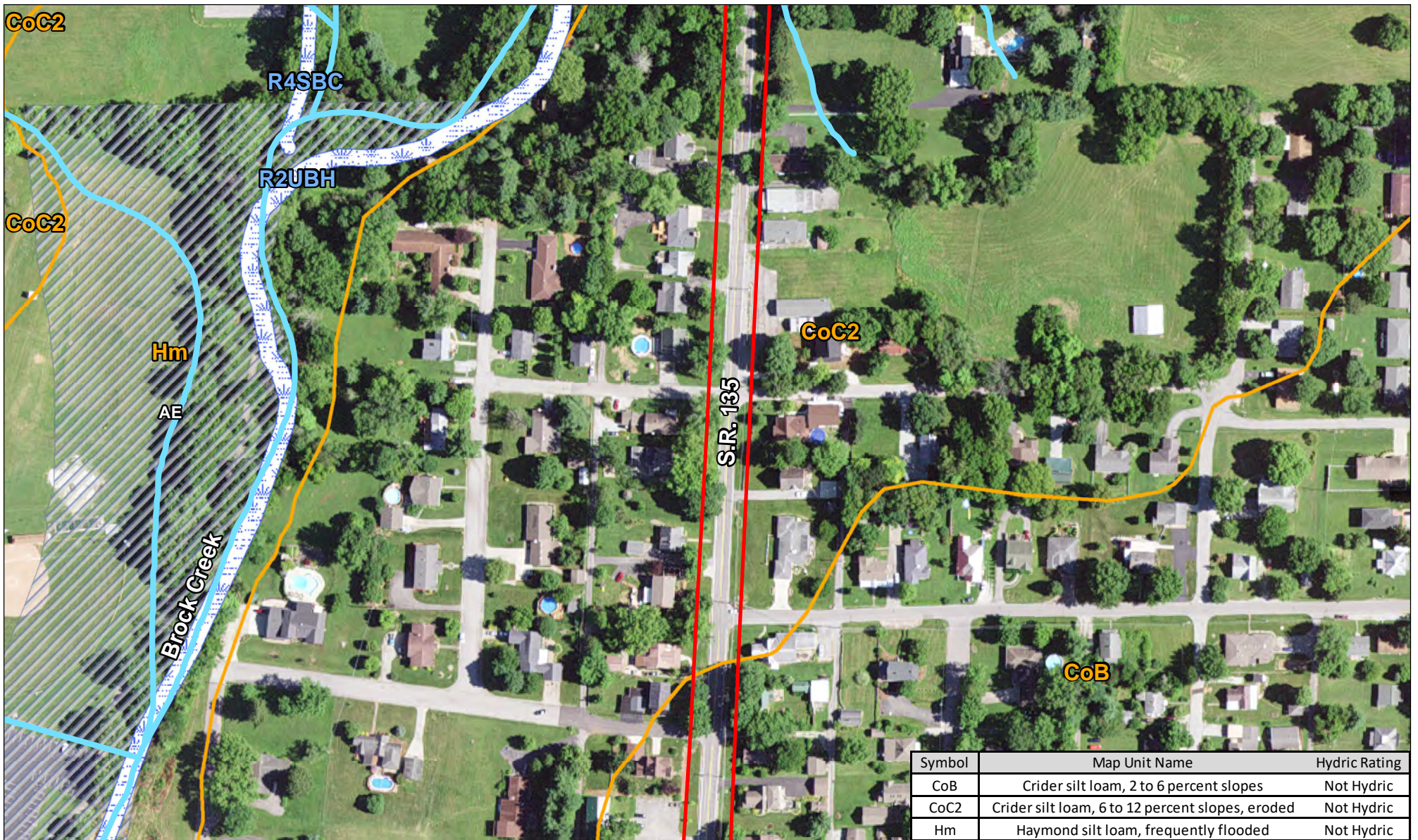
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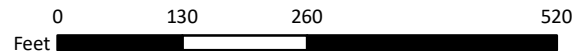


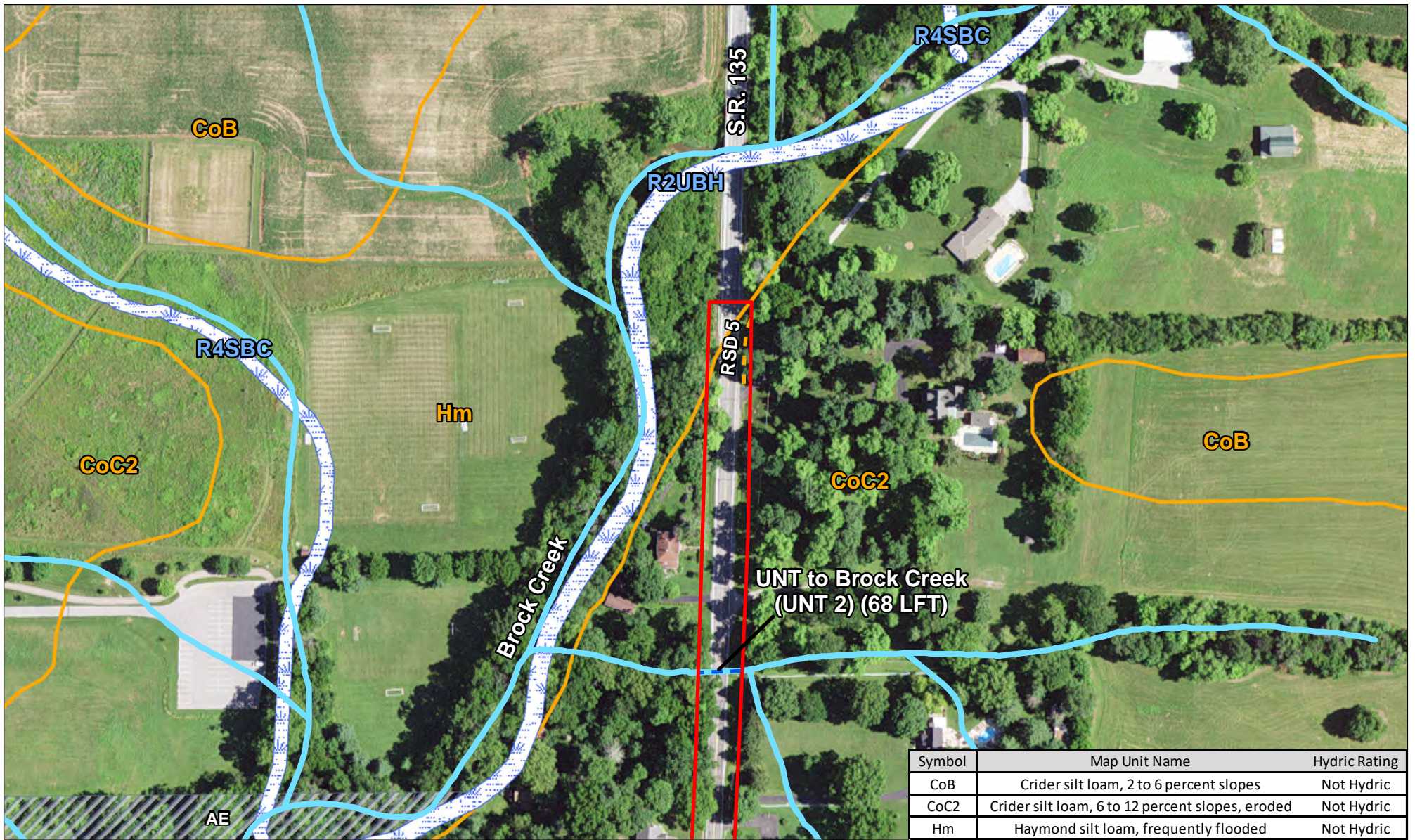


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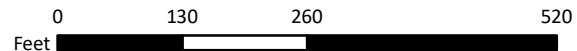




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All locations approximate  
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## WETLAND DETERMINATION DATA FORM – Eastern Mountains and Piedmont Region

Project/Site: S.R. 56 and S.R. 135 (Des. No. 1600873, 1600875, 1700168) City/County: Salem/Washington Co. Sampling Date: 10/5/2017  
 Applicant/Owner: INDOT State: IN Sampling Point: SP-1  
 Investigator(s): Kathleen Sexton, Josh Myers Section, Township, Range: S17; T2N; R4E  
 Landform (hillslope, terrace, etc.): Floodplain Local relief (concave, convex, none): None Slope (%): 0  
 Subregion (LRR or MLRA): LRR N Lat: 38.608096 Long: -86.104303 Datum: NAD 83  
 Soil Map Unit Name: Haymond silt loam (Hm) - Not hydric NWI classification: R2UBH

Are climatic / hydrologic conditions on the site typical for this time of year? Yes  No  (If no, explain in Remarks.)  
 Are Vegetation , Soil , or Hydrology  significantly disturbed? Are "Normal Circumstances" present? Yes  No   
 Are Vegetation , Soil , or Hydrology  naturally problematic? (If needed, explain any answers in Remarks.)

### SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.

Hydrophytic Vegetation Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Hydric Soil Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Wetland Hydrology Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>	Is the Sampled Area within a Wetland? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
Remarks:  Upland sampling point.	

### HYDROLOGY

<b>Wetland Hydrology Indicators:</b> <u>Primary Indicators (minimum of one is required; check all that apply)</u> <input type="checkbox"/> Surface Water (A1) <input type="checkbox"/> True Aquatic Plants (B14) <input type="checkbox"/> High Water Table (A2) <input type="checkbox"/> Hydrogen Sulfide Odor (C1) <input type="checkbox"/> Saturation (A3) <input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3) <input type="checkbox"/> Water Marks (B1) <input type="checkbox"/> Presence of Reduced Iron (C4) <input type="checkbox"/> Sediment Deposits (B2) <input type="checkbox"/> Recent Iron Reduction in Tilled Soils (C6) <input type="checkbox"/> Drift Deposits (B3) <input type="checkbox"/> Thin Muck Surface (C7) <input type="checkbox"/> Algal Mat or Crust (B4) <input type="checkbox"/> Other (Explain in Remarks) <input type="checkbox"/> Iron Deposits (B5) <input type="checkbox"/> Inundation Visible on Aerial Imagery (B7) <input type="checkbox"/> Water-Stained Leaves (B9) <input type="checkbox"/> Aquatic Fauna (B13)	<u>Secondary Indicators (minimum of two required)</u> <input type="checkbox"/> Surface Soil Cracks (B6) <input type="checkbox"/> Sparsely Vegetated Concave Surface (B8) <input type="checkbox"/> Drainage Patterns (B10) <input type="checkbox"/> Moss Trim Lines (B16) <input type="checkbox"/> Dry-Season Water Table (C2) <input type="checkbox"/> Crayfish Burrows (C8) <input type="checkbox"/> Saturation Visible on Aerial Imagery (C9) <input type="checkbox"/> Stunted or Stressed Plants (D1) <input checked="" type="checkbox"/> Geomorphic Position (D2) <input type="checkbox"/> Shallow Aquitard (D3) <input type="checkbox"/> Microtopographic Relief (D4) <input type="checkbox"/> FAC-Neutral Test (D5)
--	---

<b>Field Observations:</b> Surface Water Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____ Water Table Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____ Saturation Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____ (includes capillary fringe)	Wetland Hydrology Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
--	--

Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available:

Remarks:  
 Met geomorphic position because the sampling point was taken within a mapped floodplain.



**VEGETATION (Five Strata) – Use scientific names of plants.**

Sampling Point: SP-1

	Absolute % Cover	Dominant Species?	Indicator Status	
<b>Tree Stratum</b> (Plot size: <u>30 ft radius</u> )				
1. _____	_____	_____	_____	
2. _____	_____	_____	_____	
3. _____	_____	_____	_____	
4. _____	_____	_____	_____	
5. _____	_____	_____	_____	
6. _____	_____	_____	_____	
	0 _____ = Total Cover			
	50% of total cover: 0 _____		20% of total cover: 0 _____	
<b>Sapling Stratum</b> (Plot size: <u>15 ft radius</u> )				
1. _____	_____	_____	_____	
2. _____	_____	_____	_____	
3. _____	_____	_____	_____	
4. _____	_____	_____	_____	
5. _____	_____	_____	_____	
6. _____	_____	_____	_____	
	0 _____ = Total Cover			
	50% of total cover: 0 _____		20% of total cover: 0 _____	
<b>Shrub Stratum</b> (Plot size: <u>15 ft radius</u> )				
1. _____	_____	_____	_____	
2. _____	_____	_____	_____	
3. _____	_____	_____	_____	
4. _____	_____	_____	_____	
5. _____	_____	_____	_____	
6. _____	_____	_____	_____	
	0 _____ = Total Cover			
	50% of total cover: 0 _____		20% of total cover: 0 _____	
<b>Herb Stratum</b> (Plot size: <u>5 ft radius</u> )				
1. <u>Sorghum halapense</u>	50	Yes	FACU	
2. <u>Glechoma hederacea</u>	20	Yes	FACU	
3. <u>Phalaris arundinacea</u>	20	Yes	FACW	
4. <u>Persicaria pensylvanica</u>	10	No	FACW	
5. <u>Achillea millefolium</u>	10	No	FACU	
6. <u>Oxalis corniculata</u>	10	No	FACU	
7. <u>Commelina communis</u>	5	No	FAC	
8. <u>Leersia oryzoides</u>	5	No	OBL	
9. <u>Pteridium aquilinum</u>	5	No	FACU	
10. _____	_____	_____	_____	
11. _____	_____	_____	_____	
	135 _____ = Total Cover			
	50% of total cover: 67.5 _____		20% of total cover: 27 _____	
<b>Woody Vine Stratum</b> (Plot size: <u>30 ft radius</u> )				
1. _____	_____	_____	_____	
2. _____	_____	_____	_____	
3. _____	_____	_____	_____	
4. _____	_____	_____	_____	
5. _____	_____	_____	_____	
	0 _____ = Total Cover			
	50% of total cover: 0 _____		20% of total cover: 0 _____	
Remarks: (Include photo numbers here or on a separate sheet.)				

**Dominance Test worksheet:**

Number of Dominant Species That Are OBL, FACW, or FAC: 1 (A)

Total Number of Dominant Species Across All Strata: 3 (B)

Percent of Dominant Species That Are OBL, FACW, or FAC: 33% (A/B)

---

**Prevalence Index worksheet:**

	Total % Cover of:		Multiply by:	
OBL species	5%		x 1 =	0.1
FACW species	30%		x 2 =	0.6
FAC species	5%		x 3 =	0.2
FACU species	95%		x 4 =	3.8
UPL species	0%		x 5 =	0.0
Column Totals:	1.35	(A)	4.6	(B)

Prevalence Index = B/A = 3.41

---

**Hydrophytic Vegetation Indicators:**

1 - Rapid Test for Hydrophytic Vegetation

2 - Dominance Test is >50%

3 - Prevalence Index is ≤3.0<sup>1</sup>

4 - Morphological Adaptations<sup>1</sup> (Provide supporting data in Remarks or on a separate sheet)

Problematic Hydrophytic Vegetation<sup>1</sup> (Explain)

<sup>1</sup>Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic.

---

**Definitions of Five Vegetation Strata:**

**Tree** – Woody plants, excluding woody vines, approximately 20 ft (6 m) or more in height and 3 in. (7.6 cm) or larger in diameter at breast height (DBH).

**Sapling** – Woody plants, excluding woody vines, approximately 20 ft (6 m) or more in height and less than 3 in. (7.6 cm) DBH.

**Shrub** – Woody plants, excluding woody vines, approximately 3 to 20 ft (1 to 6 m) in height.

**Herb** – All herbaceous (non-woody) plants, including herbaceous vines, regardless of size, and woody plants, except woody vines, less than approximately 3 ft (1 m) in height.

**Woody vine** – All woody vines, regardless of height.

---

**Hydrophytic Vegetation Present?** Yes  No

**SOIL**

Sampling Point: SP-1

**Profile Description: (Describe to the depth needed to document the indicator or confirm the absence of indicators.)**

Depth (inches)	Matrix		Redox Features				Texture	Remarks
	Color (moist)	%	Color (moist)	%	Type <sup>1</sup>	Loc <sup>2</sup>		
0-20	10YR 4/3	100					SiL	

<sup>1</sup>Type: C=Concentration, D=Depletion, RM=Reduced Matrix, MS=Masked Sand Grains. <sup>2</sup>Location: PL=Pore Lining, M=Matrix.

**Hydric Soil Indicators:**

<input type="checkbox"/> Histosol (A1)	<input type="checkbox"/> Dark Surface (S7)	<p><b>Indicators for Problematic Hydric Soils<sup>3</sup>:</b></p> <input type="checkbox"/> 2 cm Muck (A10) (MLRA 147) <input type="checkbox"/> Coast Prairie Redox (A16) (MLRA 147, 148) <input type="checkbox"/> Piedmont Floodplain Soils (F19) (MLRA 136, 147) <input type="checkbox"/> Very Shallow Dark Surface (TF12) <input type="checkbox"/> Other (Explain in Remarks)
<input type="checkbox"/> Histic Epipedon (A2)	<input type="checkbox"/> Polyvalue Below Surface (S8) (MLRA 147, 148)	
<input type="checkbox"/> Black Histic (A3)	<input type="checkbox"/> Thin Dark Surface (S9) (MLRA 147, 148)	
<input type="checkbox"/> Hydrogen Sulfide (A4)	<input type="checkbox"/> Loamy Gleyed Matrix (F2)	
<input type="checkbox"/> Stratified Layers (A5)	<input type="checkbox"/> Depleted Matrix (F3)	
<input type="checkbox"/> 2 cm Muck (A10) (LRR N)	<input type="checkbox"/> Redox Dark Surface (F6)	
<input type="checkbox"/> Depleted Below Dark Surface (A11)	<input type="checkbox"/> Depleted Dark Surface (F7)	
<input type="checkbox"/> Thick Dark Surface (A12)	<input type="checkbox"/> Redox Depressions (F8)	
<input type="checkbox"/> Sandy Mucky Mineral (S1) (LRR N, MLRA 147, 148)	<input type="checkbox"/> Iron-Manganese Masses (F12) (LRR N, MLRA 136)	
<input type="checkbox"/> Sandy Gleyed Matrix (S4)	<input type="checkbox"/> Umbric Surface (F13) (MLRA 136, 122)	
<input type="checkbox"/> Sandy Redox (S5)	<input type="checkbox"/> Piedmont Floodplain Soils (F19) (MLRA 148)	
<input type="checkbox"/> Stripped Matrix (S6)	<input type="checkbox"/> Red Parent Material (F21) (MLRA 127, 147)	

<sup>3</sup>Indicators of hydrophytic vegetation and wetland hydrology must be present, unless disturbed or problematic.

**Restrictive Layer (if observed):**  
 Type: \_\_\_\_\_  
 Depth (inches): \_\_\_\_\_

Hydric Soil Present? Yes  No

Remarks:

**WETLAND DETERMINATION DATA FORM – Eastern Mountains and Piedmont Region**

Project/Site: S.R. 56 and S.R. 135 (Des. No. 1600873, 1600875, 1700168) City/County: Salem/Washington Co. Sampling Date: 10/5/2017  
 Applicant/Owner: INDOT State: IN Sampling Point: SP-2  
 Investigator(s): Kathleen Sexton, Josh Myers Section, Township, Range: S17; T2N; R4E  
 Landform (hillslope, terrace, etc.): Floodplain Local relief (concave, convex, none): None Slope (%): 0  
 Subregion (LRR or MLRA): LRR N Lat: 38.607905 Long: -86.10421 Datum: NAD 83  
 Soil Map Unit Name: Haymond silt loam (Hm) - Not hydric NWI classification: R2UBH

Are climatic / hydrologic conditions on the site typical for this time of year? Yes  No  (If no, explain in Remarks.)  
 Are Vegetation , Soil , or Hydrology  significantly disturbed? Are "Normal Circumstances" present? Yes  No   
 Are Vegetation , Soil , or Hydrology  naturally problematic? (If needed, explain any answers in Remarks.)

**SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.**

Hydrophytic Vegetation Present? Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Hydric Soil Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Wetland Hydrology Present? Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>	Is the Sampled Area within a Wetland? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
Remarks:  Upland sampling point.	

**HYDROLOGY**

<b>Wetland Hydrology Indicators:</b> <u>Primary Indicators (minimum of one is required; check all that apply)</u> <input type="checkbox"/> Surface Water (A1) <input type="checkbox"/> True Aquatic Plants (B14) <input type="checkbox"/> High Water Table (A2) <input type="checkbox"/> Hydrogen Sulfide Odor (C1) <input type="checkbox"/> Saturation (A3) <input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3) <input type="checkbox"/> Water Marks (B1) <input type="checkbox"/> Presence of Reduced Iron (C4) <input type="checkbox"/> Sediment Deposits (B2) <input type="checkbox"/> Recent Iron Reduction in Tilled Soils (C6) <input type="checkbox"/> Drift Deposits (B3) <input type="checkbox"/> Thin Muck Surface (C7) <input type="checkbox"/> Algal Mat or Crust (B4) <input type="checkbox"/> Other (Explain in Remarks) <input type="checkbox"/> Iron Deposits (B5) <input type="checkbox"/> Inundation Visible on Aerial Imagery (B7) <input type="checkbox"/> Water-Stained Leaves (B9) <input type="checkbox"/> Aquatic Fauna (B13)	<u>Secondary Indicators (minimum of two required)</u> <input type="checkbox"/> Surface Soil Cracks (B6) <input type="checkbox"/> Sparsely Vegetated Concave Surface (B8) <input type="checkbox"/> Drainage Patterns (B10) <input type="checkbox"/> Moss Trim Lines (B16) <input type="checkbox"/> Dry-Season Water Table (C2) <input type="checkbox"/> Crayfish Burrows (C8) <input type="checkbox"/> Saturation Visible on Aerial Imagery (C9) <input type="checkbox"/> Stunted or Stressed Plants (D1) <input checked="" type="checkbox"/> Geomorphic Position (D2) <input type="checkbox"/> Shallow Aquitard (D3) <input type="checkbox"/> Microtopographic Relief (D4) <input checked="" type="checkbox"/> FAC-Neutral Test (D5)
--	--

<b>Field Observations:</b> Surface Water Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____ Water Table Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____ Saturation Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____ (includes capillary fringe)	Wetland Hydrology Present? Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>
--	--

Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available:

Remarks:  
 Met geomorphic position because the sampling point was taken within a mapped floodplain.

**VEGETATION (Five Strata) – Use scientific names of plants.**

Sampling Point: SP-2

	Absolute % Cover	Dominant Species?	Indicator Status	
<b>Tree Stratum</b> (Plot size: <u>30 ft radius</u> )				
1. _____	_____	_____	_____	
2. _____	_____	_____	_____	
3. _____	_____	_____	_____	
4. _____	_____	_____	_____	
5. _____	_____	_____	_____	
6. _____	_____	_____	_____	
				0 _____ = Total Cover
				50% of total cover: 0 _____ 20% of total cover: 0 _____
<b>Sapling Stratum</b> (Plot size: <u>15 ft radius</u> )				
1. _____	_____	_____	_____	
2. _____	_____	_____	_____	
3. _____	_____	_____	_____	
4. _____	_____	_____	_____	
5. _____	_____	_____	_____	
6. _____	_____	_____	_____	
				0 _____ = Total Cover
				50% of total cover: 0 _____ 20% of total cover: 0 _____
<b>Shrub Stratum</b> (Plot size: <u>15 ft radius</u> )				
1. _____	_____	_____	_____	
2. _____	_____	_____	_____	
3. _____	_____	_____	_____	
4. _____	_____	_____	_____	
5. _____	_____	_____	_____	
6. _____	_____	_____	_____	
				0 _____ = Total Cover
				50% of total cover: 0 _____ 20% of total cover: 0 _____
<b>Herb Stratum</b> (Plot size: <u>5 ft radius</u> )				
1. <i>Phalaris arundinacea</i>	80	Yes	FACW	
2. <i>Echinocystis lobata</i>	20	No	FAC	
3. <i>Sorghum halapense</i>	15	No	FACU	
4. <i>Persicaria pensylvanica</i>	5	No	FAC	
5. _____	_____	_____	_____	
6. _____	_____	_____	_____	
7. _____	_____	_____	_____	
8. _____	_____	_____	_____	
9. _____	_____	_____	_____	
10. _____	_____	_____	_____	
11. _____	_____	_____	_____	
				120 _____ = Total Cover
				50% of total cover: 60 _____ 20% of total cover: 24 _____
<b>Woody Vine Stratum</b> (Plot size: <u>30 ft radius</u> )				
1. _____	_____	_____	_____	
2. _____	_____	_____	_____	
3. _____	_____	_____	_____	
4. _____	_____	_____	_____	
5. _____	_____	_____	_____	
				0 _____ = Total Cover
				50% of total cover: 0 _____ 20% of total cover: 0 _____
Remarks: (Include photo numbers here or on a separate sheet.)				

**Dominance Test worksheet:**

Number of Dominant Species That Are OBL, FACW, or FAC: 1 (A)

Total Number of Dominant Species Across All Strata: 1 (B)

Percent of Dominant Species That Are OBL, FACW, or FAC: 100% (A/B)

**Prevalence Index worksheet:**

	Total % Cover of:	Multiply by:
OBL species	0%	x 1 = 0.0
FACW species	80%	x 2 = 1.6
FAC species	25%	x 3 = 0.8
FACU species	15%	x 4 = 0.6
UPL species	0%	x 5 = 0.0
Column Totals:	<u>1.20</u> (A)	<u>3.0</u> (B)

Prevalence Index = B/A = 2.46

**Hydrophytic Vegetation Indicators:**

1 - Rapid Test for Hydrophytic Vegetation

2 - Dominance Test is >50%

3 - Prevalence Index is ≤3.0<sup>1</sup>

4 - Morphological Adaptations<sup>1</sup> (Provide supporting data in Remarks or on a separate sheet)

Problematic Hydrophytic Vegetation<sup>1</sup> (Explain)

<sup>1</sup>Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic.

**Definitions of Five Vegetation Strata:**

**Tree** – Woody plants, excluding woody vines, approximately 20 ft (6 m) or more in height and 3 in. (7.6 cm) or larger in diameter at breast height (DBH).

**Sapling** – Woody plants, excluding woody vines, approximately 20 ft (6 m) or more in height and less than 3 in. (7.6 cm) DBH.

**Shrub** – Woody plants, excluding woody vines, approximately 3 to 20 ft (1 to 6 m) in height.

**Herb** – All herbaceous (non-woody) plants, including herbaceous vines, regardless of size, and woody plants, except woody vines, less than approximately 3 ft (1 m) in height.

**Woody vine** – All woody vines, regardless of height.

**Hydrophytic Vegetation Present?** Yes  No

**SOIL**

Sampling Point: SP-2

**Profile Description: (Describe to the depth needed to document the indicator or confirm the absence of indicators.)**

Depth (inches)	Matrix		Redox Features				Texture	Remarks
	Color (moist)	%	Color (moist)	%	Type <sup>1</sup>	Loc <sup>2</sup>		
0-20	10YR 4/3	100					SiL	

<sup>1</sup>Type: C=Concentration, D=Depletion, RM=Reduced Matrix, MS=Masked Sand Grains.      <sup>2</sup>Location: PL=Pore Lining, M=Matrix.

<b>Hydric Soil Indicators:</b>		<b>Indicators for Problematic Hydric Soils<sup>3</sup>:</b>	
<input type="checkbox"/> Histosol (A1)	<input type="checkbox"/> Dark Surface (S7)	<input type="checkbox"/> 2 cm Muck (A10) (MLRA 147)	<sup>3</sup> Indicators of hydrophytic vegetation and wetland hydrology must be present, unless disturbed or problematic.
<input type="checkbox"/> Histic Epipedon (A2)	<input type="checkbox"/> Polyvalue Below Surface (S8) (MLRA 147, 148)	<input type="checkbox"/> Coast Prairie Redox (A16) (MLRA 147, 148)	
<input type="checkbox"/> Black Histic (A3)	<input type="checkbox"/> Thin Dark Surface (S9) (MLRA 147, 148)	<input type="checkbox"/> Piedmont Floodplain Soils (F19) (MLRA 136, 147)	
<input type="checkbox"/> Hydrogen Sulfide (A4)	<input type="checkbox"/> Loamy Gleyed Matrix (F2)	<input type="checkbox"/> Very Shallow Dark Surface (TF12)	
<input type="checkbox"/> Stratified Layers (A5)	<input type="checkbox"/> Depleted Matrix (F3)	<input type="checkbox"/> Other (Explain in Remarks)	
<input type="checkbox"/> 2 cm Muck (A10) (LRR N)	<input type="checkbox"/> Redox Dark Surface (F6)		
<input type="checkbox"/> Depleted Below Dark Surface (A11)	<input type="checkbox"/> Depleted Dark Surface (F7)		
<input type="checkbox"/> Thick Dark Surface (A12)	<input type="checkbox"/> Redox Depressions (F8)		
<input type="checkbox"/> Sandy Mucky Mineral (S1) (LRR N, MLRA 147, 148)	<input type="checkbox"/> Iron-Manganese Masses (F12) (LRR N, MLRA 136)		
<input type="checkbox"/> Sandy Gleyed Matrix (S4)	<input type="checkbox"/> Umbric Surface (F13) (MLRA 136, 122)		
<input type="checkbox"/> Sandy Redox (S5)	<input type="checkbox"/> Piedmont Floodplain Soils (F19) (MLRA 148)		
<input type="checkbox"/> Stripped Matrix (S6)	<input type="checkbox"/> Red Parent Material (F21) (MLRA 127, 147)		

<b>Restrictive Layer (if observed):</b> Type: _____ Depth (inches): _____	Hydric Soil Present?    Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
---	---

Remarks:

**Appendix 2 - PRELIMINARY JURISDICTIONAL DETERMINATION (PJD) FORM**

**BACKGROUND INFORMATION**

**A. REPORT COMPLETION DATE FOR PJD:** May 3, 2018

**B. NAME AND ADDRESS OF PERSON REQUESTING PJD:**

Kathleen Sexton  
Metric Environmental, LLC  
6971 Hillsdale Court  
Indianapolis, IN 46250  
317-207-4286  
Katies@metricenv.com

**C. DISTRICT OFFICE, FILE NAME, AND NUMBER:**

**D. PROJECT LOCATION(S) AND BACKGROUND INFORMATION:**

The proposed project is located in the central portion of Washington County, Indiana and will include the removal and replacement of pavement, sidewalks and curbs and establishment of new storm water drainage patterns along S.R. 56, from 0.12 mi east of West Junction of S.R. 60 to S.R. 135 (High Street) and along S.R. 135 from S.R. 56 to 0.72 mi north of S.R. 56, for a total of approximately 2.08 mi. Only resurfacing will be conducted on S.R. 56 from Water Street to High Street. It is anticipated that less than 10 acres of right-of-way will be required (Des. No. 1600873 and 1600875). The S.R. 56 bridge over Brock Creek (Bridge No. 056-88-01478) will be replaced. The bridge replacement will consist of the complete removal and replacement of the three span arch structure. The proposed new footprint will be similar in size to the existing structure (Des. No. 1700168).

**(USE THE TABLE BELOW TO DOCUMENT MULTIPLE AQUATIC RESOURCES AND/OR AQUATIC RESOURCES AT DIFFERENT SITES)**

State: IN County/parish/borough: Washington County City: Salem

Center coordinates of site (lat/long in degree decimal format):

Lat.: 38.608731°

Long.: -86.100817°

Universal Transverse Mercator: 16N, 578289.55 m E 4273741.86 m N

Name of nearest waterbody: Brock Creek

**E. REVIEW PERFORMED FOR SITE EVALUATION (CHECK ALL THAT APPLY):**

Office (Desk) Determination. Date:

Field Determination. Date(s):

**TABLE OF AQUATIC RESOURCES IN REVIEW AREA WHICH “MAY BE” SUBJECT TO REGULATORY JURISDICTION.**

<b>Site number</b>	<b>Latitude (decimal degrees)</b>	<b>Longitude (decimal degrees)</b>	<b>Estimated amount of aquatic resource in review area (acreage and linear feet, if applicable)</b>	<b>Type of aquatic resource (i.e., wetland vs. non-wetland waters)</b>	<b>Geographic authority to which the aquatic resource “may be” subject (i.e., Section 404 or Section 10/404)</b>
<b>Brock Creek</b>	38.607977	-86.104328	158 LFT (0.051 ac)	Non-wetland Waters	Section 404
<b>UNT 1</b>	38.607688	-86.123066	79 LFT (0.002 ac)	Non-wetland Waters	Section 404
<b>UNT 2</b>	38.617549	-86.100471	68 LFT (0.002 ac)	Non-wetland Waters	Section 404

- 1) The Corps of Engineers believes that there may be jurisdictional aquatic resources in the review area, and the requestor of this PJD is hereby advised of his or her option to request and obtain an approved JD (AJD) for that review area based on an informed decision after having discussed the various types of JDs and their characteristics and circumstances when they may be appropriate.
- 2) In any circumstance where a permit applicant obtains an individual permit, or a Nationwide General Permit (NWP) or other general permit verification requiring "pre-construction notification" (PCN), or requests verification for a non-reporting NWP or other general permit, and the permit applicant has not requested an AJD for the activity, the permit applicant is hereby made aware that: (1) the permit applicant has elected to seek a permit authorization based on a PJD, which does not make an official determination of jurisdictional aquatic resources; (2) the applicant has the option to request an AJD before accepting the terms and conditions of the permit authorization, and that basing a permit authorization on an AJD could possibly result in less compensatory mitigation being required or different special conditions; (3) the applicant has the right to request an individual permit rather than accepting the terms and conditions of the NWP or other general permit authorization; (4) the applicant can accept a permit authorization and thereby agree to comply with all the terms and conditions of that permit, including whatever mitigation requirements the Corps has determined to be necessary; (5) undertaking any activity in reliance upon the subject permit authorization without requesting an AJD constitutes the applicant's acceptance of the use of the PJD; (6) accepting a permit authorization (e.g., signing a proffered individual permit) or undertaking any activity in reliance on any form of Corps permit authorization based on a PJD constitutes agreement that all aquatic resources in the review area affected in any way by that activity will be treated as jurisdictional, and waives any challenge to such jurisdiction in any administrative or judicial compliance or enforcement action, or in any administrative appeal or in any Federal court; and (7) whether the applicant elects to use either an AJD or a PJD, the JD will be processed as soon as practicable. Further, an AJD, a proffered individual permit (and all terms and conditions contained therein), or individual permit denial can be administratively appealed pursuant to 33 C.F.R. Part 331. If, during an administrative appeal, it becomes appropriate to make an official determination whether geographic jurisdiction exists over aquatic resources in the review area, or to provide an official delineation of jurisdictional aquatic resources in the review area, the Corps will provide an AJD to accomplish that result, as soon as is practicable. This PJD finds that there "may be" waters of the U.S. and/or that there "may be" navigable waters of the U.S. on the subject review area, and identifies all aquatic features in the review area that could be affected by the proposed activity, based on the following information:




**SUPPORTING DATA. Data reviewed for PJD (check all that apply)**

Checked items should be included in subject file. Appropriately reference sources below where indicated for all checked items:

- Maps, plans, plots or plat submitted by or on behalf of the PJD requestor:
  - Map: \_\_\_\_\_ Dated 5/3/2018
  - Data sheets prepared/submitted by or on behalf of the PJD requestor.
    - Office concurs with data sheets/delineation report.
    - Office does not concur with data sheets/delineation report. Rationale: \_\_\_\_\_
- Data sheets prepared by the Corps: \_\_\_\_\_
- Corps navigable waters' study: \_\_\_\_\_
- U.S. Geological Survey Hydrologic Atlas: \_\_\_\_\_
  - USGS NHD data.
  - USGS 8 and 12 digit HUC maps.
- U.S. Geological Survey map(s). Cite scale & quad name: Salem, IN 7.5 min, 1994
  
- Natural Resources Conservation Service Soil Survey. Citation: SSURGO Washington County
  
- National wetlands inventory map(s). Cite name: http://www.fws.gov/wetlands/
- State/local wetland inventory map(s): \_\_\_\_\_
- FEMA/FIRM maps: ; Effective \_\_\_\_\_
  
- 100-year Floodplain Elevation is: \_\_\_\_\_.(National Geodetic Vertical Datum of 1929)
- Photographs:  Aerial (Name & Date): Indiana Aerial Photograph, 2012  
or  Other (Name & Date): Site Photographs, 10/5/17
- Previous determination(s). File no. and date of response letter: \_\_\_\_\_
- Other information (please specify): \_\_\_\_\_

**IMPORTANT NOTE: The information recorded on this form has not necessarily been verified by the Corps and should not be relied upon for later jurisdictional determinations.**

\_\_\_\_\_  
Signature and date of  
Regulatory staff member  
completing PJD

 05/03/2018  
\_\_\_\_\_  
Signature and date of  
person requesting PJD  
(REQUIRED, unless obtaining  
the signature is impracticable)<sup>1</sup>

<sup>1</sup> Districts may establish timeframes for requestor to return signed PJD forms. If the requestor does not respond within the established time frame, the district may presume concurrence and no additional follow up is necessary prior to finalizing an action.

**From:** Sperry, Steve  
**To:** [Alex Gray](#)  
**Cc:** [Rehder, Crystal](#); [Carlin, Whitney](#); [Greg Wendling](#)  
**Subject:** Waters Report Approval: Des. No. 1600873 1600875 1700168, S.R. 56\_S.R. 135 Town of Salem Rd Reconstruction project including Bridge Replacement over Brock Crk, Washington Co  
**Date:** Friday, May 4, 2018 10:21:07 AM  
**Attachments:** [image001.png](#)  
[image002.png](#)  
[image003.png](#)  
[image004.png](#)  
[image005.png](#)  
[1600873 Waters Rprt ApprvdFinal 20180504.pdf](#)

---

**Alex,**

Thank you for submitting the waters report for the above referenced project.

**Whitney**

The approved final report is attached and can also be found on Projectwise through this link: [1600873 Waters Rprt ApprvdFinal 20180504.pdf](#) . *It is the responsibility of the Project Manager to forward a copy of this report to the Project Designer.*

The information in this report should be used by the Project Designer to determine if waters of the U.S. will be impacted by the project. Avoidance and minimization of impacts must occur *before* mitigation will be considered. If mitigation is required, the Project Manager or Project Designer must coordinate with the Ecology and Waterway Permitting Office to discuss how adequate compensatory mitigation will be provided.

The Project Manager should notify the Ecology and Waterway Permitting Office if there is any change to the project footprint presented in this report. Such changes may require additional fieldwork and submittal of an updated waters report covering areas not previously investigated. *This report is only valid for a period of five years from the date of earliest fieldwork.* If the report expires prior to waterway permit application submittal, additional fieldwork and a revised waters report will be required.

It will not be sent to the United States Army Corps of Engineers (USACE) or the Indiana Department of Environmental Management (IDEM) until the waterways permit applications are submitted to these agencies.

Should you have any questions or need additional information please contact me.

Steve

**Stephen C. Sperry**

***Ecology and Permits Coordinator***

100 N. Senate Ave.

Indianapolis, IN 46204

**Office:** (317) 232-5206

**Email:** [ssperry@indot.in.gov](mailto:ssperry@indot.in.gov)



**APPENDIX G**  
**Public Involvement**



# INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

Land & Aerial Survey Office  
Division of Materials & Tests Building  
120 South Shortridge Road  
Indianapolis, Indiana 46219-6705

PHONE: (317) 610-7251  
FAX: (317) 356-9351

**Eric J. Holcomb, Governor**  
**Joe McGuinness, Commissioner**

October 13, 2017

Baird Mobile Homes, Inc.  
3495 US Hwy 441

Fruitland Park, FL 34731

## NOTICE OF SURVEY

Dear Property Owner:

The Indiana Department of Transportation (INDOT) will perform a survey for the proposed **Complete Rehab.** project on **SR135 and SR56 Through Salem IN**, Des No. **1600873** and **1700168** in **Washington** County, Indiana. A portion of this survey work may be performed on your property in order to provide design engineers information for project design. The survey work will include mapping the location of features such as trees, buildings, fences, drives, ground elevations, etc. The survey is needed for the proper planning and design of this highway project.

At this stage we generally do not know what effect, if any, our project may eventually have on your property. If we determine later that your property is involved, we will contact you with additional information.

Indiana Code 8-23-7-26 allows USI Consultants, Inc., as the authorized employees of INDOT, *Right of Entry* to the project site (including private property) upon proper notification. A copy of a Notice of Survey discussion sheet, as found on INDOT's website (<http://www.in.gov/indot/2888.htm>), is attached to this letter. Pursuant to Indiana Code 8-23-7-27, this letter serves as written notification that we will be performing the above noted survey in the vicinity of your property after **October 13, 2017**.

Employees of USI Consultants will show you their identification, if you are available, before coming onto your property.

If you own but are not the tenant of this property (i.e. rental, sharecrop), please inform us so that we may also contact the actual tenant of the property prior to commencement of our work. If you have any questions or concerns regarding our proposed survey work or schedule, please contact the USI Consultants Survey Operations Manager. This contact information is as follows:

Mark Schepers, P.S.  
USI Consultants, Inc.  
8415 E 56<sup>th</sup> St.  
Indianapolis, IN 46216  
(317) 544-4996

[www.in.gov/dot/](http://www.in.gov/dot/)  
**An Equal Opportunity Employer**



## INDIANA DEPARTMENT OF TRANSPORTATION

*Driving Indiana's Economic Growth*

Land & Aerial Survey Office  
Division of Materials & Tests Building  
120 South Shortridge Road  
Indianapolis, Indiana 46219-6705

PHONE: (317) 610-7251  
FAX: (317) 356-9351

**Eric J. Holcomb, Governor**  
**Joe McGuinness, Commissioner**

Under Indiana Code 8-23-7-28, you have a right to compensation for any damage that occurs to your land or water as a result of the entry or work performed during the entry. To obtain such compensation, you should contact the [Central Office](#) District Real Estate Manager; contact information is below. The District Real Estate Manager can provide you with a form to request compensation for damages. Once you fill out this form, you can return it to the District Real Estate Manager for consideration. If you are not satisfied with the compensation that INDOT determines is owed to you, Indiana Code 8-23-7-28 provides the following:

The amount of damages shall be assessed by the county agricultural extension educator of the county in which the land or water is located and two (2) disinterested residents of the county, one (1) appointed by the aggrieved party and one (1) appointed by the department. A written report of the assessment of damages shall be mailed to the aggrieved party and the department by first class United States mail. If either the department or the aggrieved party is not satisfied with the assessment of damages, either or both may file a petition, not later than fifteen (15) days after receiving the report, in the circuit or superior court of the county in which the land or water is located.

If you have questions regarding the rights and procedures outlined in this letter, please contact the [Seymour](#) Real Estate Manager. This contact information is as follows:

Nicole Curry  
185 Agrico Lane  
Seymour, IN 47274  
(812) 524-3970

Thank you in advance for your cooperation in this matter.

Sincerely,

A handwritten signature in black ink that reads "Greg Wendling". The signature is written in a cursive style.

---

Greg Wendling, P.E.  
Project Manager  
USI Consultants, Inc.  
8415 E 56<sup>th</sup> St.  
Indianapolis, IN 46216  
(317) 544-4996

[www.in.gov/dot/](http://www.in.gov/dot/)  
*An Equal Opportunity Employer*

ATTACH COPY OF ADVERTISEMENT HERE

Prescribed by State Board of Accounts

General Form No. 99P (Rev. 2009A)

Indiana Department of Transportation

To: **The Salem Leader**

Washington County, Indiana.

**PUBLISHER'S CLAIM**

LINE COUNT

Display Master (Must not exceed two actual lines, neither of which shall total more than four solid lines of the type in which the body of the advertisement is set) -- number of equivalent lines	.....
Head -- number of lines	.....
Body -- number of lines	<b>208</b> .....
Tail -- number of lines	.....
Total number of lines in notice	<b>208</b> .....

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<b>208</b> lines, ..... columns wide equals ..... equivalent lines at <b>.5585</b> cents per line	<b>\$116.18</b> .....
Additional charges for notices containing rule or tabular work (50 per cent of above amount)	.....
Charge for extra proofs of publication (\$1.00 for each proof in excess of two)	.....
<b>TOTAL AMOUNT OF CLAIM</b>	<b>\$116.18</b> .....

Width of single column in picas **7p10**. Size of type ..... **7** point.

Number of insertions **2**.....

Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width and type size, which was duly published in said paper **2**..... times. The dates of publication being as follows:

**Feb. 11 and 18**

Additionally, the statement checked below is true and correct:

- ..... Newspaper does not have a Web site.
- ..... Newspaper has a Web site and this public notice was posted on the same day as it was published in the newspaper.
- ..... Newspaper has a Web site, but due to technical problem or error, public notice was posted on .....
- ..... Newspaper has a Web site but refuses to post the public notice.

Date: **February 18, 2020**.....

NANCY GROSSMAN  
Title: PUBLISHER

LEGAL ADVERTISING

See table of legal rates in the applicable State Board of Accounts Bulletin

Claim No. \_\_\_\_\_ Warrant No. \_\_\_\_\_

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IN FAVOR OF

**The Salem Leader**  
**\$116.18**

ON ACCOUNT OF APPROPRIATION FOR

---

Appropriation No. \_\_\_\_\_

---

ALLOWED \_\_\_\_\_, \_\_\_\_\_

IN THE SUM OF \$ \_\_\_\_\_

I have examined the within claim and hereby certify as follows:

That it is in proper form.

That it is duly authenticated as required by law.

That it is based upon statutory authority.

That it is apparently

Correct

Incorrect

I certify that the within claim is true and correct; that the services there in itemized and for which charge is made were ordered by me and were necessary to the public business.

**LEGAL NOTICE OF PLANNED IMPROVEMENT**

The Indiana Department of Transportation (INDOT) is developing plans for proposed pavement replacement, drainage and pedestrian improvements along S.R. 56 from 0.1 mile east of the West Junction of S.R. 60 to 0.1 mile east of S.R. 135 (High Street). The project also involves pavement resurfacing and pedestrian improvements along S.R. 135 from S.R. 56 to 0.7 mile north of S.R. 56, also a bridge replacement project involving the bridge over Brock Creek in Salem, Washington County.

The purpose of this project is to extend the life of the existing pavement, address the cause of numerous accidents, inadequate drainage and pedestrian facilities and provide a hydraulically sufficient structure in order to perpetuate vehicular traffic over Brock Creek. The need is due to deteriorating pavement along S.R. 56 and S.R. 135. Drainage along the corridor is substandard in some locations with no ditching while other areas have non-transverse drop-offs causing frequent ponding. Existing sidewalk along S.R. 56 is discontinuous. Some of the curbs within the project limits are in poor condition and non-compliant with Americans with Disabilities (ADA) standards.

The proposed improvements include pavement replacement, pedestrian and storm water improvements along S.R. 56, resurfacing S.R. 135 including minor pedestrian improvements and a bridge reconstruction project on S.R. 56 over Brock Creek in Salem, Washington County. Specifically, S.R. 56 includes removing and replacing the roadway pavement, installing new loop detectors, constructing new sidewalks and ADA compliant curbs, establishing new enclosed storm water drainage patterns, new signs and pavement markings from 0.9 mile east of West Junction of Old Indiana 60/S.R. 56 to 0.8 mile east of S.R. 135 (High Street). Work on S.R. 135 includes milling and resurfacing of pavement, upgrading select curbs to meet ADA compliance and pavement markings. The bridge over Brock Creek will be replaced.

The proposed construction of this project will require 1.1 acres of new permanent right-of-way and 0.5 acre of temporary right-of-way.

The maintenance of traffic (MOT) plan will require lane closures during phased construction, flaggers and detours. For S.R. 56, a detour will be in place utilizing Salem Bypass, S.R. 135, U.S. 150 and S.R. 337. For S.R. 135, a detour will not be required during construction on S.R. 135. The contractor will utilize flaggers as needed. Access to homeowners and business will be maintained during construction. For the bridge over Brock Creek, the bridge project over Brock Creek will be conducted concurrently with the S.R. 56 roadway project and will utilize a detour. Local roads may be used by local traffic. Access to all properties will be maintained throughout construction. School corporations and emergency services will be notified of closures prior to construction. The project is proposed to begin construction in 2021 and continue through 2022.

The cost associated with the projects is approximately \$8 million which includes preliminary engineering, right-of-way and construction with both federal and state funds anticipated to be used. The Federal Highway Administration (FHWA) and INDOT have agreed this project falls within the guidelines of a Categorical Exclusion (CE) Level 2 environmental document. Preliminary design plans along with the CE are available for review at the following locations:

Salem Public Library,  
212 North Main Street,  
Salem, Indiana 47167

INDOT Seymour District Office,  
185 Agrico Lane,  
Seymour, Indiana 47274;  
Phone 855-463-6848 (855-INDOT4U)

INDOT Office of Public Involvement,  
Rm N642, 100 N. Senate Ave.,  
Indianapolis, IN 46204 2216,  
Phone # (317) 232-6601

All interested persons may request a public hearing be held and/or express their concerns by submitting comments to the attention of Rickie Clark, INDOT Office of Public Involvement, Room N642, 100 North Senate Ave., Indianapolis, IN 46204-2216 or [rclark@indot.in.gov](mailto:rclark@indot.in.gov) on or before Thursday, February 27, 2020.

In accordance with the Americans with Disabilities Act (ADA), persons and/or groups requiring project information be made available in alternative formats are encouraged to contact the INDOT Office of Public Involvement for the arrangement and coordination of services. Please contact Rickie Clark at 317-232-6601 or [rclark@indot.in.gov](mailto:rclark@indot.in.gov). In accordance with Title VI of the Civil Rights Act of 1964, persons and/or groups requiring project information be made available in another language are encouraged to contact the INDOT Office of Public Involvement.

This notice is published in compliance with: 1) Code of Federal Regulations, Title 23, Section 771 (CFR 771.111(h)(1)) stating, "Each State must have procedures approved by the FHWA to carry out a public involvement/public hearing program."; 2) 23 CFR 450.210(a)(1)(ix) stating, "Provide for the periodic review of the effectiveness of the public involvement process to ensure that the process provides full and open access to all interested parties and revise the process, as appropriate."; and 3) The INDOT Public Involvement Policies and Procedures approved by the Federal Highway Administration on August 16, 2012.





# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204

PHONE: (317) 234-0796  
FAX: (317) 233-4929

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

DES. # 1600873, 1600875, 1700168

## LEGAL NOTICE OF PLANNED IMPROVEMENT

The Indiana Department of Transportation (INDOT) is developing plans for proposed pavement replacement, drainage and pedestrian improvements along S.R. 56 from 0.1 mile east of the West Junction of S.R. 60 to 0.1 mile east of S.R. 135 (High Street). The project also involves pavement resurfacing and pedestrian improvements along S.R. 135 from S.R. 56 to 0.7 mile north of S.R. 56, also a bridge replacement project involving the bridge over Brock Creek in Salem, Washington County

The purpose of this project is to extend the life of the existing pavement, address the cause of numerous accidents, inadequate drainage and pedestrian facilities and provide a hydraulically sufficient structure in order to perpetuate vehicular traffic over Brock Creek. The need is due to deteriorating pavement along S.R. 56 and S.R. 135. Drainage along the corridor is substandard in some locations with no ditching while other areas have non-transversal drop-offs causing frequent ponding. Existing sidewalk along S.R. 56 is discontinuous. Some of the curbs within the project limits are in poor condition and non-compliant with Americans with Disabilities (ADA) standards.

The proposed improvements include pavement replacement, pedestrian and storm water improvements along S.R. 56, resurfacing S.R. 135 including minor pedestrian improvements and a bridge reconstruction project on S.R. 56 over Brock Creek in Salem, Washington County. Specifically, S.R. 56 includes removing and replacing the roadway pavement, installing new loop detectors, constructing new sidewalks and ADA compliant curbs, establishing new enclosed storm water drainage patterns, new signs and pavement markings from 0.9 mile east of West Junction of Old Indiana 60/S.R. 56 to 0.8 mile east of S.R. 135 (High Street). Work on S.R. 135 includes milling and resurfacing of pavement, upgrading select curbs to meet ADA compliance and pavement markings. The bridge over Brock Creek will be replaced.

The proposed construction of this project will require 1.1 acres of new permanent right-of-way and 0.5 acre of temporary right-of-way.

The maintenance of traffic (MOT) plan will require lane closures during phased construction, flaggers and detours. For S.R. 56, a detour will be in place utilizing Salem Bypass, S.R. 135, U.S. 150 and S.R. 337. For S.R. 135, a detour will not be required during construction on S.R. 135. The contractor will utilize flaggers as needed. Access to homeowners and business will be maintained during construction. For the bridge over Brock Creek, the bridge project over Brock Creek will be conducted concurrently with the S.R. 56 roadway project and will utilize a detour. Local roads may be used by local traffic. Access to all properties will be maintained throughout construction. School corporations and emergency services will be notified of closures prior to construction. The project is proposed to begin construction in 2021 and continue through 2022.

The cost associated with the projects is approximately \$8 million which includes preliminary engineering, right-of-way and construction with both federal and state funds anticipated to be used. The Federal Highway Administration (FHWA) and INDOT have agreed this project falls within the guidelines of a Categorical Exclusion (CE) Level 2 environmental document. Preliminary design plans along with the CE are available for review at the following locations:

Salem Public Library, 212 North Main Street, Salem, Indiana 4716

INDOT Seymour District Office, 185 Agrico Lane, Seymour, Indiana 47274; Phone 855-463-6848 (855-INDOT4U)

INDOT Office of Public Involvement, Rm N642, 100 N. Senate Ave., Indianapolis, IN 46204-2216, Phone # (317) 232-6601

**All interested persons may request a public hearing be held and/or express their concerns by submitting comments to the attention of Rickie Clark, INDOT Office of Public Involvement, Room N642, 100 North Senate Ave., Indianapolis, IN 46204-2216 or [rclark@indot.in.gov](mailto:rclark@indot.in.gov) on or before Thursday, February 27, 2020.**

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**Clark, Rickie**

---

**From:** Clark, Rickie  
**Sent:** Tuesday, February 11, 2020 2:44 PM  
**To:** rick.neilson@in.usda.gov; afrench2@indot.in.gov; Hector\_Santiago@nps.gov; DNR Environmental Review; Michael.E.Wurl@hud.gov; Gregory.A.McKay@usace.army.mil; digreen@washingtoncounty.in.gov; rvoyles@washington.in.gov; COMMISSIONER District 2; McWilliams, Robin; Joy Bierly; jusgreen@blueriver.net; tom.day@cityofsalemin.com; salem.police@cityofsalemin.com; ron.voyles@cityofsalemin.com; Combs, Claude; Washington Co. EMA Director; Welch, Jessica; sabrina.burdine@wcegp.org; J.Quyle@RadiusIndiana.com; rbills@washingtoncounty.in.gov; d3comm@washingtoncounty.in.gov; d1comm@washingtoncounty.in.gov; COMMISSIONER District 2; phasty@washingtoncounty.in.gov; joey.wheeler@cityofsalemin.com; Joy Bierly; ron.voyles@cityofsalemin.com; digreen@washingtoncounty.in.gov; cboling@washingtoncounty.in.gov  
**Subject:** INDOT proposes pavement replacement, bridge replacement, drainage and pedestrian facilities improvement in Salem, Washington County



## INDIANA DEPARTMENT OF TRANSPORTATION

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### LEGAL NOTICE OF PLANNED IMPROVEMENT - DES. # 1600873, 1600875, 1700168

The Indiana Department of Transportation (INDOT) is developing plans for proposed pavement replacement, drainage and pedestrian improvements along S.R. 56 from 0.1 mile east of the West Junction of S.R. 60 to 0.1 mile east of S.R. 135 (High Street). The project also involves pavement resurfacing and pedestrian improvements along S.R. 135 from S.R. 56 to 0.7 mile north of S.R. 56, also a bridge replacement project involving the bridge over Brock Creek in Salem, Washington County.



20200211140121...

The purpose of this project is to extend the life of the existing pavement, address the cause of numerous accidents, inadequate drainage and pedestrian facilities and provide a hydraulically sufficient structure in order to perpetuate vehicular traffic over Brock Creek. The need is due to deteriorating pavement along S.R. 56 and S.R. 135. Drainage along the corridor is substandard in some locations with no ditching while other areas have non-transversal drop-offs causing frequent ponding. Existing sidewalk along S.R. 56 is discontinuous. Some of the curbs within the project limits are in poor condition and non-compliant with Americans with Disabilities (ADA) standards.

The proposed improvements include pavement replacement, pedestrian and storm water improvements along S.R. 56, resurfacing S.R. 135 including minor pedestrian improvements and a bridge reconstruction project on S.R. 56 over Brock Creek in Salem, Washington County. Specifically, S.R. 56 includes removing and replacing the roadway pavement, installing new loop detectors, constructing new sidewalks and ADA compliant curbs, establishing new enclosed storm water drainage patterns, new signs and pavement markings from 0.9 mile east of West Junction of Old Indiana 60/S.R. 56 to 0.8 mile east of S.R. 135 (High Street). Work on S.R. 135 includes milling and resurfacing of pavement, upgrading select curbs to meet ADA compliance and pavement markings. The bridge over Brock Creek will be replaced.

The proposed construction of this project will require 1.1 acres of new permanent right-of-way and 0.5 acre of temporary right-of-way.

The maintenance of traffic (MOT) plan will require lane closures during phased construction, flaggers and detours. For S.R. 56, a detour will be in place utilizing Salem Bypass, S.R. 135, U.S. 150 and S.R. 337. For S.R. 135, a detour will not be required during construction on S.R. 135. The contractor will utilize flaggers as needed. Access to homeowners and business will be maintained during construction. For the bridge over Brock Creek, the bridge project over Brock Creek will be conducted concurrently with the S.R. 56 roadway project and will utilize a detour. Local roads may be used by local traffic. Access to all properties will be maintained throughout construction. School corporations and emergency services will be notified of closures prior to construction. The project is proposed to begin construction in 2021 and continue through 2022.

The cost associated with the projects is approximately \$8 million which includes preliminary engineering, right-of-way and construction with both federal and state funds anticipated to be used. The Federal Highway Administration (FHWA) and INDOT have agreed this project falls within the guidelines of a Categorical Exclusion (CE) Level 2 environmental document. Preliminary design plans along with the CE are available for review at the following locations:

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INDOT Office of Public Involvement, Rm N642, 100 N. Senate Ave., Indianapolis, IN 46204-2216, Phone # (317) 232-6601

**All interested persons may request a public hearing be held and/or express their concerns by submitting comments to the attention of Rickie Clark, INDOT Office of Public Involvement, Room N642, 100 North Senate Ave., Indianapolis, IN 46204-2216 or [rclark@indot.in.gov](mailto:rclark@indot.in.gov) on or before Thursday, February 27, 2020.**

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Rickie Clark MBA  
Indiana Department of Transportation  
Office of Public Involvement / Communications  
100 North Senate Avenue, Room N642  
Indianapolis, Indiana 46204  
**Phone:** (317) 232-6601  
**Email:** [rclark@indot.in.gov](mailto:rclark@indot.in.gov)



**Clark, Rickie**

---

**From:** Clark, Rickie  
**Sent:** Wednesday, February 26, 2020 10:01 AM  
**To:** rick.neilson@in.usda.gov; afrench2@indot.in.gov; Hector\_Santiago@nps.gov; DNR Environmental Review; Michael.E.Wurl@hud.gov; Gregory.A.McKay@usace.army.mil; digreen@washingtoncounty.in.gov; rvoyles@washington.in.gov; COMMISSIONER District 2; McWilliams, Robin; Joy Bierly; jusgreen@blueriver.net; tom.day@cityofsalemin.com; salem.police@cityofsalemin.com; ron.voyles@cityofsalemin.com; Combs, Claude; Washington Co. EMA Director; Welch, Jessica; sabrina.burdine@wcegp.org; J.Quyle@RadiusIndiana.com; rbills@washingtoncounty.in.gov; d3comm@washingtoncounty.in.gov; d1comm@washingtoncounty.in.gov; COMMISSIONER District 2; phasty@washingtoncounty.in.gov; joey.wheeler@cityofsalemin.com; Joy Bierly; ron.voyles@cityofsalemin.com; digreen@washingtoncounty.in.gov; cboling@washingtoncounty.in.gov  
**Subject:** INDOT proposes pavement replacement, bridge replacement, drainage and pedestrian facilities improvement in Salem, Washington County



## INDIANA DEPARTMENT OF TRANSPORTATION

### LEGAL NOTICE OF PLANNED IMPROVEMENT - DES. # 1600873, 1600875, 1700168

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20200211140121...

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The proposed improvements include pavement replacement, pedestrian and storm water improvements along S.R. 56, resurfacing S.R. 135 including minor pedestrian improvements and a bridge reconstruction project on S.R. 56 over Brock Creek in Salem, Washington County. Specifically, S.R. 56 includes removing and replacing the roadway pavement, installing new loop detectors, constructing new sidewalks and ADA compliant curbs, establishing new enclosed storm water drainage patterns, new signs and pavement markings from 0.9 mile east of West Junction of Old Indiana 60/S.R. 56 to 0.8 mile east of S.R. 135 (High Street). Work on S.R. 135 includes milling and resurfacing of pavement, upgrading select curbs to meet ADA compliance and pavement markings. The bridge over Brock Creek will be replaced.

The proposed construction of this project will require 1.1 acres of new permanent right-of-way and 0.5 acre of temporary right-of-way.

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Rickie Clark MBA  
Indiana Department of Transportation  
Office of Public Involvement / Communications  
100 North Senate Avenue, Room N642  
Indianapolis, Indiana 46204  
**Phone:** (317) 232-6601  
**Email:** [rclark@indot.in.gov](mailto:rclark@indot.in.gov)





Mailing list DEB #  
1600873, 1600875, 1700168

Vernon Hacker  
3792 W. SR 56  
Salem, IN 47167

Brave LLC  
307 Newton Street  
Jasper, IN 47547

Tony Wagoner  
600 Jay Street  
Salem, IN 47167

Brian Smith  
1013 Bristol Street  
Salem, IN 47167

Colton Howard  
1011 W. Bristol Street  
Salem, IN 47167

Eddie Gilstrap Motors  
207 S. Main Street  
Salem, IN 47167

David Allen  
P.O. Box 410  
Salem, IN 47167

Charles & Anita Carter  
1003 W. Mulberry Street  
Salem, IN 47167

William & Kellie Jones  
2395 W. Water Treatment Rd  
Salem, IN 47167

Voyles Family Trust  
907 W. Mulberry Street  
Salem, IN 47167

Cheryl Appell  
905 W. Mulberry Street  
Salem, IN 47167

Jim & Erin Sparkman  
903 W. Mulberry Street  
Salem, IN 47167

Ryan LLC  
1671 P.O. Box 06116  
Chicago, IL 60606

Trent Bontrager  
1007 E. Market Street  
Salem, IN 47167

Ervin & Wanda Day  
3784 E. Day Road  
Salem, IN 47167

John & Lois McLane  
910 W. Mulberry Street  
Salem, IN 47167

Anna & Robert Bary  
P.O. Box 23  
Depauw, IN 47115

Chaden Holdings LLC  
343 N. Howell Road  
Salem, IN 47167

Anna Pennington  
902 W. Mulberry Street  
Salem, IN 47167

Hacker Sawmill & Trucking  
3792 W. SR 56  
Salem, IN 47167

Bonnie Harmon  
807 W. Mulberry Street  
Salem, IN 47167

David Allen  
P.O. Box 485  
Salem, IN 47167

Jason & Erin Bontrager  
1206 E. Old SR 60  
Salem, IN 47167

Jason & Erin Bontrager  
1206 Old SR 60  
Salem, IN 47167

Roger & Olive Sweeney  
711 W. Mulberry Street  
Salem, IN 47167

Anita Maudlin  
707 W. Mulberry Street  
Salem, IN 47167

Marvin & Wanda Hoke  
705 W. Mulberry Street  
Salem, IN 47167

Campbell Rentals LLC  
208 N. Water Street  
Salem, IN 47167

Charles Godfrey  
701 W. Mulberry Street  
Salem, IN 47167

Brian Ratts  
609 W. Mulberry Street  
Salem, IN 47167

Vernon Hacker  
3792 W. SR 56  
Salem, IN 47167

Brave LLC  
307 Newton Street  
Jasper, IN 47547

Tony Wagoner  
600 Jay Street  
Salem, IN 47167

Brian Smith  
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Cheryl Appell  
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Salem, IN 47167

Jim & Erin Sparkman  
903 W. Mulberry Street  
Salem, IN 47167

Ryan LLC  
1671 P.O. Box 06116  
Chicago, IL 60606

Trent Bontrager  
1007 E. Market Street  
Salem, IN 47167

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Depauw, IN 47115

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1206 E. Old SR 60  
Salem, IN 47167

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Salem, IN 47167

Charles & Anita Carter  
1003 W. Mulberry Street  
Salem, IN 47167

William & Kellie Jones  
2395 W. Water Treatment Rd  
Salem, IN 47167

Voyles Family Trust  
907 W. Mulberry Street  
Salem, IN 47167

Cheryl Appell  
905 W. Mulberry Street  
Salem, IN 47167

Jim & Erin Sparkman  
903 W. Mulberry Street  
Salem, IN 47167

Ryan LLC  
1671 P.O. Box 06116  
Chicago, IL 60606

Trent Bontrager  
1007 E. Market Street  
Salem, IN 47167

Ervin & Wanda Day  
3784 E. Day Road  
Salem, IN 47167

John & Lois McLane  
910 W. Mulberry Street  
Salem, IN 47167

Anna & Robert Bary  
P.O. Box 23  
Depauw, IN 47115

Chaden Holdings LLC  
343 N. Howell Road  
Salem, IN 47167

Anna Pennington  
902 W. Mulberry Street  
Salem, IN 47167

Hacker Sawmill & Trucking  
3792 W. SR 56  
Salem, IN 47167

Bonnie Harmon  
807 W. Mulberry Street  
Salem, IN 47167

David Allen  
P.O. Box 485  
Salem, IN 47167

Jason & Erin Bontrager  
1206 E. Old SR 60  
Salem, IN 47167

Jason & Erin Bontrager  
1206 Old SR 60  
Salem, IN 47167

Roger & Olive Sweeney  
711 W. Mulberry Street  
Salem, IN 47167

Anita Maudlin  
707 W. Mulberry Street  
Salem, IN 47167

Marvin & Wanda Hoke  
705 W. Mulberry Street  
Salem, IN 47167

Campbell Rentals LLC  
208 N. Water Street  
Salem, IN 47167

Charles Godfrey  
701 W. Mulberry Street  
Salem, IN 47167

Brian Ratts  
609 W. Mulberry Street  
Salem, IN 47167

Baird Mobile Homes, Inc.  
3495 US Hwy 441  
Fruitland Park, FL 34731

Wilma Humphrey  
901 North Main Street  
Salem, IN 47167

Raymond E. Nicholson  
1309 W. Mulberry Street  
Salem, IN 47167

David Medlock  
1307 W. Mulberry Street  
Salem, IN 47167

John & Annette Agan  
1305 W. Mulberry Street  
Salem, IN 47167

Kenneth & Ramona  
1424 W. Mulberry Street  
Salem, IN 47167

Kenneth & Ramona  
1422 W. Mulberry Street  
Salem, IN 47167

Arthur Antoine  
1420 W. Mulberry Street  
Salem, IN 47167

Ronpart INC  
3825 N. Cox Ferry Road  
Salem, IN 47167

Clifford & Janice Webster  
1410 W. Mulberry Street  
Salem, IN 47167

Nichols Trust  
1404 W. Mulberry Street  
Salem, IN 47167

Billy Thompson  
1417 S. Jackson Street  
Salem, IN 47167

Steven Stewart  
10947 Lost River Road  
Campbellsburg, IN 47108

Bc Kom Enterprises LLC  
1312 W. Mulberry Street  
Salem, IN 47167

GSD Petroleum LLC  
9653 N. Granville Road  
Mequon, WI 53097

Robert & Elaine Nice  
4446 W. Cole Road  
Salem, IN 47167

Treasured Properties LLC  
90 E. Rose Lane  
Salem, IN 47167

Bennett Smedley  
1215 W. Mulberry Street  
Salem, IN 47167

Kristy Sanders  
304 S. High Street  
Salem, IN 47167

Terri Graves  
1211 W. Mulberry Street  
Salem, IN 47167

Hannah Hospitality LLC  
1209 W. Mulberry Street  
Salem, IN 47167

Janet Moore  
1205 W. Mulberry Street  
Salem, IN 47167

Marion Robertson  
1201 W. Mulberry Street  
Salem, IN 47167

R&D Salem Property LLC  
3864 N. Cox Ferry Road  
Salem, IN 47167

Ronald Journell  
1937 N. Sparks Ferry Road  
Salem, IN 47167

Casey's Enterprises LLC 2311  
P.O. Box 3001  
Ankeny, IA 50021

Kevin Powell  
28358 Brookview Terrace  
Santa Clarita, CA 91350

Gary L. Tarr Etal  
1866 S. Dog Trot Road  
Salem, IN 47167

Kevin & Luann Campbell  
208 N. Water Street  
Salem, IN 47167

McCabe & Steepleton  
1813 Redbud  
Huntingburg, IN 47542

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3495 US Hwy 441  
Fruitland Park, FL 34731

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Salem, IN 47167

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Campbellsburg, IN 47108

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Santa Clarita, CA 91350

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Salem, IN 47167

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208 N. Water Street  
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McCabe & Steepleton  
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Huntingburg, IN 47542

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Kevin & Luann Campbell  
208 N. Water Street  
Salem, IN 47167

McCabe & Steepleton  
1813 Redbud  
Huntingburg, IN 47542

**APPENDIX H**  
**Air Quality**



U.S. Department  
of Transportation  
**Federal Highway  
Administration**

**Indiana Division**

July 31, 2017

575 N. Pennsylvania St, Room 254  
Indianapolis, IN 46204  
317-226-7475  
317-226-7341

In Reply Refer To:  
HDA-IN

Mr. Trevor Mills  
Deputy Commissioner  
Engineering and Asset Management  
100 North Senate Avenue  
Indianapolis, IN 46204

Dear Mr. Mills:

We have completed our review of INDOT's Amendment #18-02 to the FY 2018-2021 Indiana Statewide Transportation Improvement Program (STIP) dated July 26, 2017. FHWA approves it for inclusion into the STIP.

Should you have any questions regarding this approval please contact Joyce Newland at 317-226-5353 or e-mail at [joyce.newland@dot.gov](mailto:joyce.newland@dot.gov).

Sincerely,

**JOYCE E  
NEWLAND**

Digitally signed by JOYCE E  
NEWLAND  
DN: c=US, o=U.S. Government,  
ou=DOT FHWAIndianapolisIN,  
ou=FWHA FHWAIndianapolisIN,  
cn=JOYCE E NEWLAND  
Date: 2017.07.31 11:19:18 -04'00'

For: Mayela Sosa  
Division Administrator

Enclosure

ecc: Michael McNeil, INDOT



Indiana Department of Transportation (INDOT)  
 State Preservation and Local Initiated Projects FY 2018 - 2021

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2018	2019	2020	2021
Washington County	38186 / 1500216	Init.	VA VARI	Bridge Inspections	Countywide Bridge Inspection and Inventory Program for Cycle Years 2018-2021	Seymour	0	STP		Local Funds	PE	\$0.00	\$39,143.52	\$19,156.80		\$19,986.72	
Indiana Department of Transportation	38602 / 1401870	Init.	SR 160	Small Structure Replacement	Over Unnamed Tributary of Springle Creek, 8.0 miles W of US 31	Seymour	0	STP		Bridge ROW	RW	\$16,000.00	\$4,000.00		\$20,000.00		
										Bridge Construction	CN	\$200,608.80	\$50,152.20			\$250,761.00	
Indiana Department of Transportation	39353 / 1593070	Init.	US 150	Small Structure Replacement	12.60 miles E of E Jct SR-56	Vincennes	0	STP		Bridge Construction	CN	\$195,200.00	\$48,800.00			\$244,000.00	
Indiana Department of Transportation	39774 / 1600672	Init.	SR 56	Small Structure Replacement	0.85 miles W of SR 39	Seymour	0	STP		Bridge Construction	CN	\$368,340.00	\$92,085.00				\$460,425.00
										Bridge Construction	PE	\$44,000.00	\$11,000.00				\$55,000.00
										Bridge ROW	RW	\$16,000.00	\$4,000.00			\$20,000.00	
										Bridge Consulting	PE	\$28,000.00	\$7,000.00				\$35,000.00
Indiana Department of Transportation	39788 / 1600711	Init.	SR 335	Box Culvert Replacement	5.66 miles N of US 150	Seymour	0	STP		Bridge Construction	CN	\$239,214.40	\$59,803.60				\$299,018.00
										Bridge Consulting	PE	\$40,000.00	\$10,000.00				\$50,000.00
										Bridge Construction	PE	\$68,000.00	\$17,000.00				\$85,000.00
										Bridge ROW	RW	\$32,000.00	\$8,000.00			\$40,000.00	
Indiana Department of Transportation	39788 / 1600711	M 08	SR 335	Box Culvert Replacement	5.66 miles N of US 150	Seymour	0	STP	\$474,018.00	Bridge ROW	RW	\$0.00	\$0.00		\$40,000.00	(\$40,000.00)	
Comments: Move RW phase from FY 2020 to FY 2019. No MPO.																	
Indiana Department of Transportation	39879 / 1600704	Init.	SR 160	Small Structure Pipe Lining	10.90 miles E of SR 60	Seymour	0	STP		Bridge Consulting	PE	\$36,000.00	\$9,000.00	\$20,000.00			\$25,000.00
										Bridge Construction	PE	\$20,000.00	\$5,000.00				\$25,000.00
										Bridge ROW	RW	\$24,000.00	\$6,000.00			\$30,000.00	
										Bridge Construction	CN	\$82,240.80	\$20,560.20				\$102,801.00
Indiana Department of Transportation	39905 / 1600873	Init.	SR 56	Pavement Replacement, Small Town	0.9 miles E of W Jct of SR 60 to .08 miles E of SR 135 (High St)	Seymour	1.45	STP		Road ROW	RW	\$52,000.00	\$13,000.00			\$65,000.00	

Indiana Department of Transportation (INDOT)  
 State Preservation and Local Initiated Projects FY 2018 - 2021

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2018	2019	2020	2021
Indiana Department of Transportation	39905 / 1600873	Init.	SR 56	Pavement Replacement, Small Town	0.9 miles E of W Jct of SR 60 to .08 miles E of SR 135 (High St)	Seymour	1.45	STP		Bridge Construction	CN	\$62,400.00	\$15,600.00				\$78,000.00
										Road Consulting	PE	\$249,600.00	\$62,400.00	\$312,000.00			
										Road Construction	CN	\$2,412,274.40	\$603,068.60				\$3,015,343.00
Indiana Department of Transportation	39905 / 1600875	Init.	SR 135	Pavement Replacement, Small Town	From SR 56 to 0.72 miles N of SR 56 (Salem NCL)	Seymour	.716	STP		Road Construction	PE	\$30,768.80	\$7,692.20				\$38,461.00
										Road ROW	RW	\$25,640.80	\$6,410.20			\$32,051.00	
										Road Construction	CN	\$1,177,607.20	\$294,401.80				\$1,472,009.00
										Road Consulting	PE	\$123,076.80	\$30,769.20	\$153,846.00			
Indiana Department of Transportation	39905 / 1600875	M 10	SR 135	Pavement Replacement, Small Town	From SR 56 to 0.72 miles N of SR 56 (Salem NCL)	Seymour	.716	STP	\$1,696,367.00	Road Consulting	PE	\$0.00	\$0.00	(\$153,846.00)	\$153,846.00		
Comments:Move PE phase from FY 2018 to FY 2019. No MPO.																	
Indiana Department of Transportation	40057 / 1601989	Init.	SR 135	Bridge Painting	3.76 miles S of SR 235 over Muscatatuck River	Seymour	0	STP		Bridge Construction	PE	\$4,000.00	\$1,000.00		\$5,000.00		
										Bridge Consulting	PE	\$44,000.00	\$11,000.00	\$15,000.00		\$40,000.00	
										Bridge Construction	CN	\$320,000.00	\$80,000.00			\$400,000.00	
Indiana Department of Transportation	40410 / 1600680	A 01	SR 60	Small Structure Pipe Lining	At 6.22 miles W of SR 56	Seymour	0	STP	\$485,163.00	Bridge Consulting	PE	\$120,000.00	\$30,000.00	\$150,000.00			
Comments:Amend PE phase in FY 2018 to the current STIP. No MPO.																	
Indiana Department of Transportation	40410 / 1700009	A 17	SR 56	Small Structure Replacement	5.46 miles E of SR 135	Seymour	0	STP	\$1,011,752.00	Bridge ROW	RW	\$8,000.00	\$2,000.00				\$10,000.00
										Bridge Consulting	PE	\$88,000.00	\$22,000.00	\$110,000.00			
Comments:Amend PE phase in FY 2018 and RW in 2021 to the current STIP. No MPO.																	
Indiana Department of Transportation	40433 / 1700168	A 02	SR 56	Br Repl, Conc. Beam Construction	00.20 mile W of SR 135 at Brock Creek	Seymour	0	STP	\$1,636,375.00	Bridge Consulting	PE	\$160,000.00	\$40,000.00	\$200,000.00			
Comments:Amend PE phase in FY 2018 to current STIP. No MPO.																	
Indiana Department of Transportation	40452 / 1602276	A 01	SR 39	Small Structure Replacement	0.15 miles N of SR 56	Seymour	0	STP	\$744,951.00	Bridge ROW	RW	\$8,000.00	\$2,000.00				\$10,000.00
										Bridge Consulting	PE	\$106,400.00	\$26,600.00	\$133,000.00			

Indiana Department of Transportation (INDOT)  
 State Preservation and Local Initiated Projects FY 2020 - 2024

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2020	2021	2022	2023	2024	
<b>Washington County</b>																			
Washington County	34558 / 1173265	Init.	IR 1006	Bridge Rehabilitation Or Repair	Bridge 105 on Becks Mill Road over Mill Creek in Washington County	Seymour	0	STPBG		Local Bridge Program	CN	\$1,016,400.00	\$0.00	\$1,016,400.00					
											Local Funds	CN	\$0.00	\$254,100.00	\$254,100.00				
Washington County	34558 / 1173265	M 02	IR 1006	Bridge Rehabilitation Or Repair	Bridge 105 on Becks Mill Road over Mill Creek in Washington County	Seymour	0	STBG	\$1,143,721.92	Local Bridge Program	CN	\$120,000.00	\$0.00	\$120,000.00					
											Local Funds	CN	\$0.00	\$30,000.00	\$30,000.00				
Comments:Adding CN funds in the amount of \$150,000 for FY 2020. No MPO.																			
Washington County	38186 / 1500216	Init.	VA VARI	Bridge Inspections	Countywide Bridge Inspection and Inventory Program for Cycle Years 2018-2021	Seymour	0	STPBG		Local Bridge Program	PE	\$118,439.20	\$0.00	\$23,946.40	\$76,537.60	\$17,955.20			
											Local Funds	PE	\$0.00	\$29,609.80	\$5,986.60	\$19,134.40	\$4,488.80		
Indiana Department of Transportation	38602 / 1401870	Init.	SR 160	Small Structure Replacement	Over Unnamed Tributary of Springle Creek, 8.0 miles W of US 31	Seymour	0	STPBG		Bridge Construction	CN	\$159,373.60	\$39,843.40	\$199,217.00					
Indiana Department of Transportation	39905 / 1600873	Init.	SR 56	Pavement Replacement, Small Town	0.09 miles E of W Jct of SR 60 to .08 miles E of SR 135 (High St)	Seymour	1.45	STPBG		Road ROW	RW	\$569,640.80	\$142,410.20	\$712,051.00					
											Road Construction	CN	\$5,286,760.00	\$1,321,690.00		\$389,700.00	\$6,218,750.00		
											District Other Construction	CN	\$280,000.00	\$70,000.00			\$350,000.00		
											Bridge ROW	RW	\$60,000.00	\$15,000.00		\$75,000.00			
											Bridge Construction	CN	\$1,137,891.20	\$284,472.80			\$1,422,364.00		
Indiana Department of Transportation	40057 / 1601989	Init.	SR 135	Bridge Painting	3.76 miles S of SR 235 over Muscatatuck River	Seymour	0	STPBG		Bridge Consulting	PE	\$92,000.00	\$23,000.00	\$115,000.00					
											Bridge Construction	CN	\$769,835.20	\$192,458.80	\$962,294.00				
Indiana Department of Transportation	40410 / 1700009	Init.	SR 56	Small Structure Pipe Lining	5.46 miles E of SR 135	Seymour	0	STPBG		Bridge ROW	RW	\$24,000.00	\$6,000.00		\$30,000.00				
											Bridge Construction	CN	\$1,094,667.20	\$273,666.80			\$1,368,334.00		
Indiana Department of Transportation	40453 / 1700173	Init.	SR 60	Br Repl. Comp. Cont. Conc. Construction	00.42 miles W of SR 335 over S Fork Blue River	Seymour	0	STPBG		Bridge ROW	RW	\$68,000.00	\$17,000.00		\$85,000.00				
											Bridge Construction	CN	\$2,947,751.20	\$736,937.80			\$3,684,689.00		

\*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

**APPENDIX I**  
**Additional Studies**

objectid	State	County	Grant ID Element	Type	grant Element Title	Grant Sponsor	Fiscal Year	Amount
47366	Indiana	WASHINGTON	186	C	CHRISTIAN CHURCH PLAYGROUND	SALEM PARK BOARD	1975	\$10,728.00
47465	Indiana	WASHINGTON	316	D	D/SALEM COMMUNITY PARK	SALEM PARK BOARD	1978	\$34,206.83
47489	Indiana	Washington	395	C	CAMPBELLSBURG TOWN PARK	CAMPBELLSBURG PARK AND RECREATION BOARD	1981	\$0.00
60798	Indiana	WASHINGTON	536	D	DELANEY CREEK PARK IMPROVEMENTS	WASHINGTON COUNTY PARK BOARD	2003	\$120,934.00

Accessed August 26, 2019

Accessed from <<https://www.lwcfcoalition.com/map-of-lwcf>>.



B03002

HISPANIC OR LATINO ORIGIN BY RACE

Universe: Total population

2013-2017 American Community Survey 5-Year Estimates

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.

	Washington County, Indiana		Census Tract 9675, Washington County, Indiana	
	Estimate	Margin of Error	Estimate	Margin of Error
Total:	27,807	*****	2,955	+/-294
Not Hispanic or Latino:	27,444	*****	2,923	+/-288
White alone	26,948	+/-22	2,842	+/-301
Black or African American alone	25	+/-25	1	+/-2
American Indian and Alaska Native alone	89	+/-110	0	+/-11
Asian alone	48	+/-36	0	+/-11
Native Hawaiian and Other Pacific Islander alone	0	+/-21	0	+/-11
Some other race alone	17	+/-22	0	+/-11
Two or more races:	317	+/-104	80	+/-82
Two races including Some other race	0	+/-21	0	+/-11
Two races excluding Some other race, and three or more races	317	+/-104	80	+/-82
Hispanic or Latino:	363	*****	32	+/-54
White alone	358	+/-10	32	+/-54
Black or African American alone	0	+/-21	0	+/-11
American Indian and Alaska Native alone	0	+/-21	0	+/-11
Asian alone	0	+/-21	0	+/-11
Native Hawaiian and Other Pacific Islander alone	0	+/-21	0	+/-11
Some other race alone	5	+/-10	0	+/-11
Two or more races:	0	+/-21	0	+/-11
Two races including Some other race	0	+/-21	0	+/-11
Two races excluding Some other race, and three or more races	0	+/-21	0	+/-11

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see Accuracy of the Data). The effect of nonsampling error is not represented in these tables.

While the 2013-2017 American Community Survey (ACS) data generally reflect the February 2013 Office of Management and Budget (OMB) definitions of metropolitan and micropolitan statistical areas; in certain instances the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB definitions due to differences in the effective dates of the geographic entities.

Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Source: U.S. Census Bureau, 2013-2017 American Community Survey 5-Year Estimates

#### Explanation of Symbols:

1. An '\*\*\*' entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.
2. An '-' entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution.
3. An '-' following a median estimate means the median falls in the lowest interval of an open-ended distribution.
4. An '+' following a median estimate means the median falls in the upper interval of an open-ended distribution.
5. An '\*\*\*\*' entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.
6. An '\*\*\*\*\*' entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.
7. An 'N' entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.
8. An '(X)' means that the estimate is not applicable or not available.



B17001

**POVERTY STATUS IN THE PAST 12 MONTHS BY SEX BY AGE**

Universe: Population for whom poverty status is determined  
2013-2017 American Community Survey 5-Year Estimates

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.

	Washington County, Indiana		Census Tract 9675, Washington County, Indiana	
	Estimate	Margin of Error	Estimate	Margin of Error
Total:	27,478	+/-119	2,862	+/-270
Income in the past 12 months below poverty level:	3,644	+/-665	432	+/-177
Male:	1,623	+/-327	194	+/-110
Under 5 years	188	+/-73	44	+/-39
5 years	60	+/-59	0	+/-11
6 to 11 years	192	+/-77	17	+/-26
12 to 14 years	131	+/-81	45	+/-67
15 years	43	+/-53	0	+/-11
16 and 17 years	36	+/-30	0	+/-11
18 to 24 years	157	+/-65	19	+/-33
25 to 34 years	168	+/-76	1	+/-2
35 to 44 years	120	+/-81	0	+/-11
45 to 54 years	207	+/-85	32	+/-30
55 to 64 years	205	+/-96	10	+/-16
65 to 74 years	81	+/-41	16	+/-20
75 years and over	35	+/-29	10	+/-17
Female:	2,021	+/-398	238	+/-96
Under 5 years	152	+/-72	0	+/-11
5 years	6	+/-11	6	+/-11
6 to 11 years	160	+/-69	31	+/-46
12 to 14 years	89	+/-60	13	+/-22
15 years	37	+/-44	0	+/-11
16 and 17 years	51	+/-38	0	+/-11
18 to 24 years	298	+/-128	0	+/-11
25 to 34 years	236	+/-96	21	+/-25
35 to 44 years	299	+/-101	65	+/-53
45 to 54 years	211	+/-76	10	+/-17
55 to 64 years	208	+/-87	49	+/-50
65 to 74 years	149	+/-60	19	+/-21
75 years and over	125	+/-53	24	+/-23
Income in the past 12 months at or above poverty level:	23,834	+/-694	2,430	+/-286
Male:	11,982	+/-376	1,104	+/-161
Under 5 years	589	+/-74	68	+/-59



	Washington County, Indiana		Census Tract 9675, Washington County, Indiana	
	Estimate	Margin of Error	Estimate	Margin of Error
5 years	53	+/-39	0	+/-11
6 to 11 years	927	+/-149	93	+/-53
12 to 14 years	456	+/-118	30	+/-28
15 years	242	+/-94	0	+/-11
16 and 17 years	285	+/-91	23	+/-35
18 to 24 years	1,008	+/-68	161	+/-59
25 to 34 years	1,348	+/-83	116	+/-52
35 to 44 years	1,649	+/-96	95	+/-60
45 to 54 years	1,839	+/-110	195	+/-66
55 to 64 years	1,752	+/-112	156	+/-56
65 to 74 years	1,150	+/-43	119	+/-54
75 years and over	684	+/-55	48	+/-35
Female:	11,852	+/-390	1,326	+/-195
Under 5 years	632	+/-75	66	+/-51
5 years	154	+/-76	0	+/-11
6 to 11 years	736	+/-126	76	+/-49
12 to 14 years	719	+/-126	45	+/-40
15 years	211	+/-83	41	+/-53
16 and 17 years	315	+/-74	0	+/-11
18 to 24 years	725	+/-129	142	+/-105
25 to 34 years	1,261	+/-97	169	+/-66
35 to 44 years	1,382	+/-104	136	+/-56
45 to 54 years	1,831	+/-75	167	+/-87
55 to 64 years	1,830	+/-74	192	+/-83
65 to 74 years	1,244	+/-74	102	+/-50
75 years and over	812	+/-72	190	+/-56

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see Accuracy of the Data). The effect of nonsampling error is not represented in these tables.

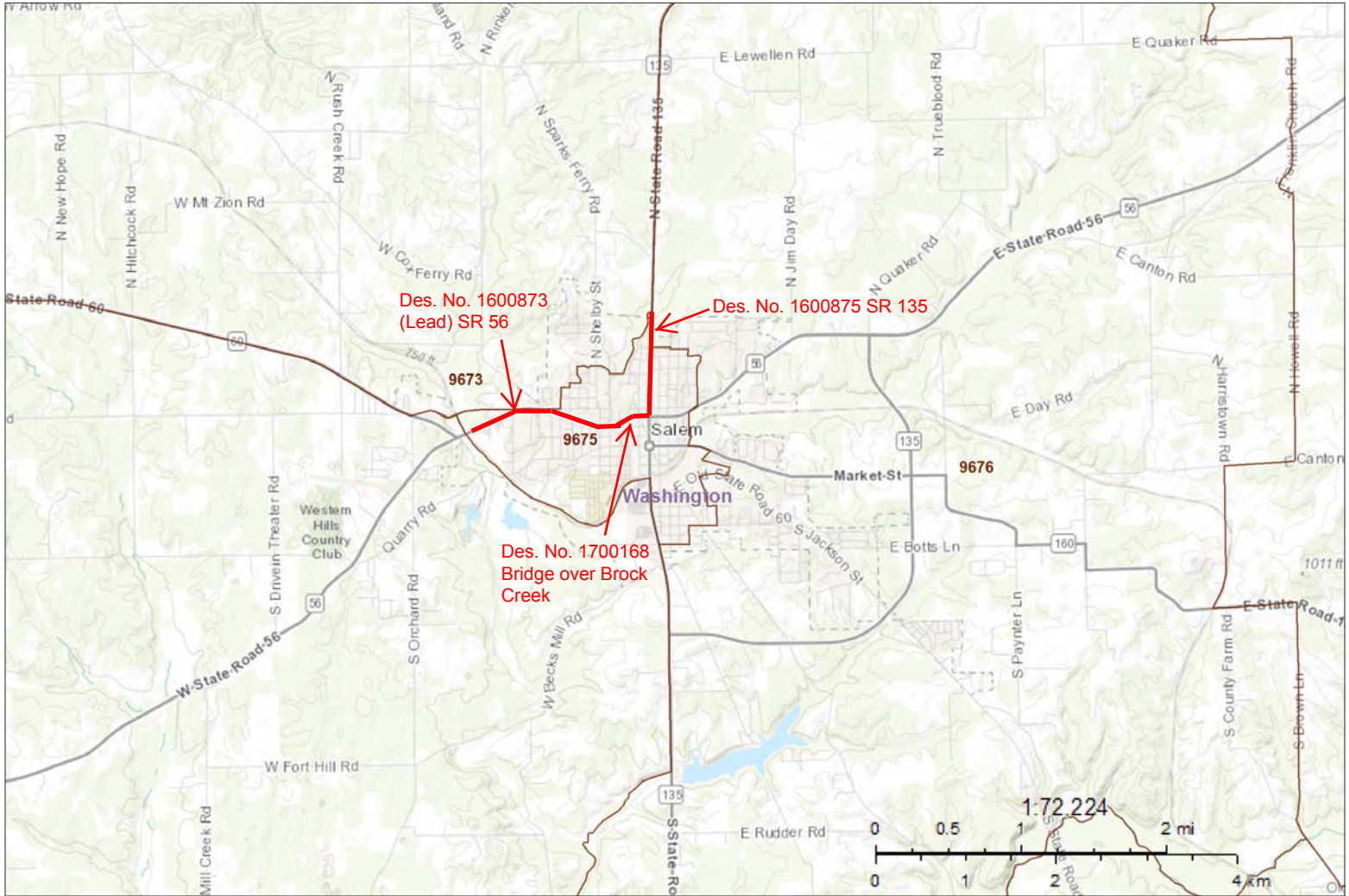
While the 2013-2017 American Community Survey (ACS) data generally reflect the February 2013 Office of Management and Budget (OMB) definitions of metropolitan and micropolitan statistical areas; in certain instances the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB definitions due to differences in the effective dates of the geographic entities.

Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Source: U.S. Census Bureau, 2013-2017 American Community Survey 5-Year Estimates

#### Explanation of Symbols:

1. An '\*\*\*' entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.
2. An '-' entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution.
3. An '-' following a median estimate means the median falls in the lowest interval of an open-ended distribution.
4. An '+' following a median estimate means the median falls in the upper interval of an open-ended distribution.
5. An '\*\*\*\*' entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.
6. An '\*\*\*\*\*' entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.
7. An 'N' entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.
8. An '(X)' means that the estimate is not applicable or not available.



**Legend**

**Your Selections**

No Legend

**Selection Results**

No Legend

**2018 Boundaries**

- County Washington
- Census Tract 9675

<b>Environmental Justice Analysis, 2013-2017 American Community Survey 5-Year Estimates</b>			
<b>Des. No. 1600873 (Lead) SR 56; Des. No. 1600875 SR 135; and Des. No. 1700168 SR 56 Bridge over Brock Creek</b>		<b>COC</b>	<b>AC-2</b>
		Washington County Indiana	Census Tract 9675 Washington County Indiana
	<b>LOW-INCOME</b>		
B17001001	Population for whom poverty status is determined: Total	27,478	2,862
B17001002	Population for whom poverty status is determined: Income in 2017 below poverty level	3,644	432
	<b>Percent Low-Income</b> (Income in 2017 below poverty level/Total population)	13.26%	15.09%
	<b>125 Percent of COC</b> (125 x COC Percent Low-Income)	16.58%	<b>AC &lt;125% COC</b>
	<b>Potential Low-Income EJ Impact?</b>		<b>No</b>
	<b>MINORITY</b>		
B03002001	Total Population: Total	27,807	2,955
B03002002	Total Population: Not Hispanic or Latino	27,444	2,923
B03002003	Total Population: Not Hispanic or Latino; White alone	26,948	2,842
B03002004	Total Population: Not Hispanic or Latino; Black or African American alone	25	1
B03002005	Total Population: Not Hispanic or Latino; American Indian and Alaska Native alone	89	0
B03002006	Total Population: Not Hispanic or Latino; Asian alone	48	0
B03002007	Total Population: Not Hispanic or Latino; Native Hawaiian and Other Pacific Islander alone	0	0
B03002008	Total Population: Not Hispanic or Latino; Some other race alone	17	0
B03002009	Total Population: Not Hispanic or Latino; Two or more races	317	80
B03002010	Total Population: Hispanic or Latino	363	32
B03002011	Total Population: Hispanic or Latino; White alone	358	32
B03002012	Total Population: Hispanic or Latino; Black or African American alone	0	0
B03002013	Total Population: Hispanic or Latino; American Indian and Alaska Native alone	0	0
B03002014	Total Population: Hispanic or Latino; Asian alone	0	0
B03002015	Total Population: Hispanic or Latino; Native Hawaiian and Other Pacific Islander alone	0	0
B03002016	Total Population: Hispanic or Latino; Some other race alone	5	0
B03002017	Total Population: Hispanic or Latino; Two or more races	0	0
	<b>Number Non-white/minority</b> (B03002001 - B03002003)	859	113
	<b>Percent Non-white/Minority</b> (Total population - white alone)/Total population	3.09%	3.82%
	<b>125 Percent of COC</b> (125 x COC Percent Non-white/Minority)	3.86%	<b>AC &lt;125% COC</b>
	<b>Potential Minority EJ Impact?</b>		<b>No</b>